

¹6.13 C5 – Fort Mall Redevelopment District

6.13.1 Purpose

This district is generally intended to provide regulations for the redevelopment of the Fort Mall site as envisioned in the Downtown Area Redevelopment Plan & Design Guidelines (Bylaw C14-08). A variety of land uses are supported in this district including residential, commercial, and mixed-use. Higher densities and scale greater than seen elsewhere in Fort Saskatchewan are supported, with high rise buildings accommodated in specific locations provided that the design ensures development relates to the adjacent areas and provides harmonious transitions. Open space and pedestrian connections will be provided to ensure ease of movement to and from the area, and opportunities for recreation. Special emphasis shall be given for the creation of a high quality public realm including urban plazas, outdoor amenity areas and interactive streetscapes.

¹ C22-14

6.13.2 C5 Permitted & Discretionary Uses:

(a) C5 Permitted Uses	
<ul style="list-style-type: none"> - ¹(Deleted) - Apartment Dwelling - Assisted Living Facility - Business Support Service - Commercial School - Community Service Facility - Custom Manufacturing Establishment - Day Care Facility* - Eating and Drinking Establishment - Eating and Drinking Establishment (limited) - Eating and Drinking Establishment (outdoor) - Emergency Response Service - Health Service - Home Office - Hotel - Indoor Entertainment Facility - Indoor Recreation Facility - Multi-attached Dwelling** 	<ul style="list-style-type: none"> - ²(Deleted) - Parking Facility - Personal Service - Pet Care Service - Place of Worship - Private Club - Professional, Financial, and Office Service - Public Facility - Retail Store (convenience) - Retail Store (general) - Retail Store (liquor) - Seasonal Garden Centre (temporary) - Show Home - Sign (Freestanding) - Sign (Identification) - Sign (portable) - Sign (Projecting) - Temporary Sales Centre - ³Accessory development to any use listed in subsection 6.13.2(a)

*Day Care Facility may not occur within an apartment dwelling

** Multi-attached Dwellings shall be limited to the Periphery Zone, as per Figure 6.13a

¹ C19-15

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(b) C5 Discretionary Uses	
<ul style="list-style-type: none"> - Casino - Government Service - Greenhouse - Late Night Club - Live Work Unit - Multi-attached Dwelling - Outdoor Recreation Facility - Pawn Shop 	<ul style="list-style-type: none"> - Temporary Outdoor Event - Vehicle Sales, Leasing or Rental Facility (limited) - Veterinarian Clinic - ¹(Deleted) - ²Accessory development to any use listed in subsection 6.13.2(b)

6.13.3 Site Development Regulations

	Interior or Corner Site	
a) Site Area	Minimum	300.0m ² (3229.2ft ²)
	Maximum	N/A
b) Site Width	Minimum	At the discretion of Development Authority
	Maximum	At the discretion of Development Authority
c) Front Setback	Minimum	<p>For buildings with commercial uses at ground floor: 1.4m to 3.4m (4.6ft to 11.2ft) in order to achieve a continuous pedestrian zone of 3.4m (11.2ft)</p> <p>For building with residential units at ground floor: minimum 3.0m (9.8ft) with display gardens.</p> <p>Non-residential uses at ground floor 0.0m (0.0ft) to 1.4m (4.6m) to achieve a continuous pedestrian zone of 3.4m (11.2ft)</p> <p>Residential uses at ground floor 3.0m (9.8ft) with display gardens</p> <p>Residential uses at ground floor abutting MR 1.0m (3.3ft) with display gardens in the MR</p>

¹ C19-15

² C19-15

Appendix B

d) Side Setback	Minimum	<p>At the discretion of the Development Authority unless located on a corner site.</p> <p>For corner sites: minimum 1.4m to 3.4m (4.6ft to 11.2ft) in order to achieve a continuous pedestrian zone of 3.4m (11.2ft)</p> <p>0.0m (0.0ft)</p>
e) Rear Setback	Minimum	<p>At the discretion of the Development Authority for sites not abutting a Residential Land Use District</p> <p>0.0m (0.0ft) when abutting a Non-Residential Land Use District</p> <p>4.5m (14.8ft) or one-half (1/2) the height of the building, whichever is greater, for sites abutting a Residential Land Use District</p>
f) Site Coverage	Maximum	70%
g) FAR	Maximum	<p>3.5</p> <p>4.0</p>
h) Unit Density	Maximum	<p>Low density—50 dwelling units per net developable hectare</p> <p>Medium density—90 dwelling units per net developable hectare</p> <p>High density—250 dwelling units per net developable hectare</p> <p>200 units per net developable hectare for sites less than 1500.0m²</p> <p>350 units per net developable hectare for sites greater than 1500.0m²</p>
i) Height		As per Section 6.13.4 of this Bylaw
j) Common Amenity Area	Minimum	<p>Apartment Dwellings</p> <p>4.5m² (48.4ft²) per dwelling unit</p> <p>All other Residential Dwellings</p> <p>At the discretion of the Development Authority. This can include indoor and outdoor amenities such as seating areas and roof top patios</p>

k) Private Amenity Area	Minimum	<p>7.5m² (81ft²) per dwelling unit. Minimum 25% of the required amenity area shall be provided as outdoor space</p> <p>Residential Dwellings at Grade and Above Grade 3.0m² (32.3ft²) per dwelling unit to be provided by balconies, decks, patios or rooftop amenity area***</p> <p>Residential Dwellings Below Grade To be provided through the common amenity area</p>
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*** Private Amenity Area shall only be provided by balconies in Apartment Dwellings

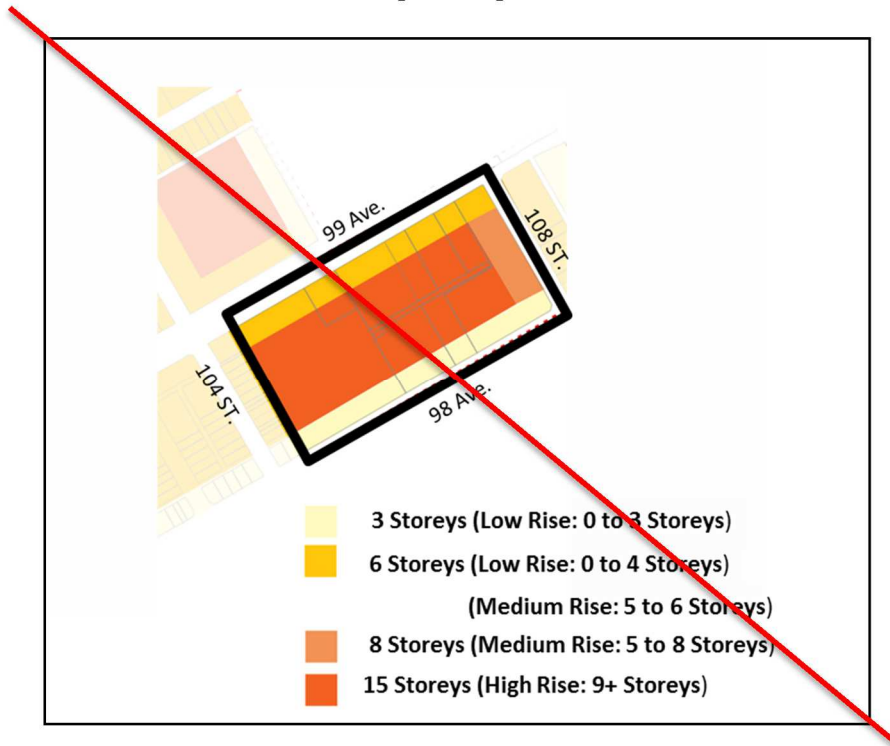
Urban Design Regulations

6.13.4 Urban Form

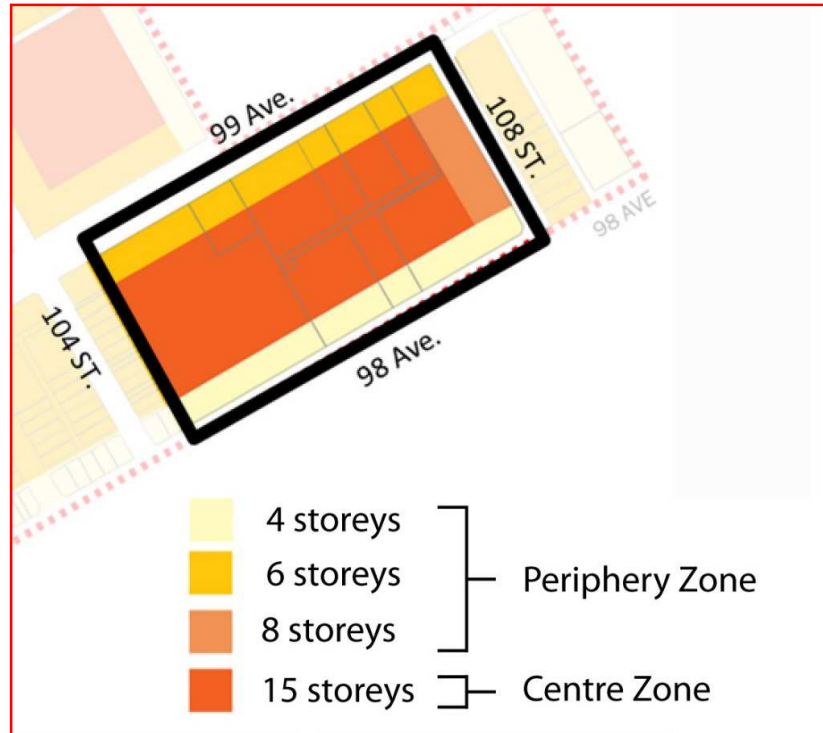
- a) Maximum building height shall be determined based upon:
 - i. ¹The location of the building in proximity to the public roadways, as per **Figure 6.13a**.
 - ii. ~~For mid-rise and high-rise buildings, the maximum building height shall also be limited by the application of 45-degree angular plane (**Figure 6.13b**).~~
Along 98 Avenue, new development shall have a minimum height of 2 storeys when located in the Periphery Zone, and a minimum height of 4 storeys in the Centre Zone, as per **Figure 6.13a**.
 - iii. Building heights shall be transitioned through appropriate stepbacks as per **Figure 6.13a**.

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¹Figure 6.13a: Fort Mall Site Maximum Heights Diagram

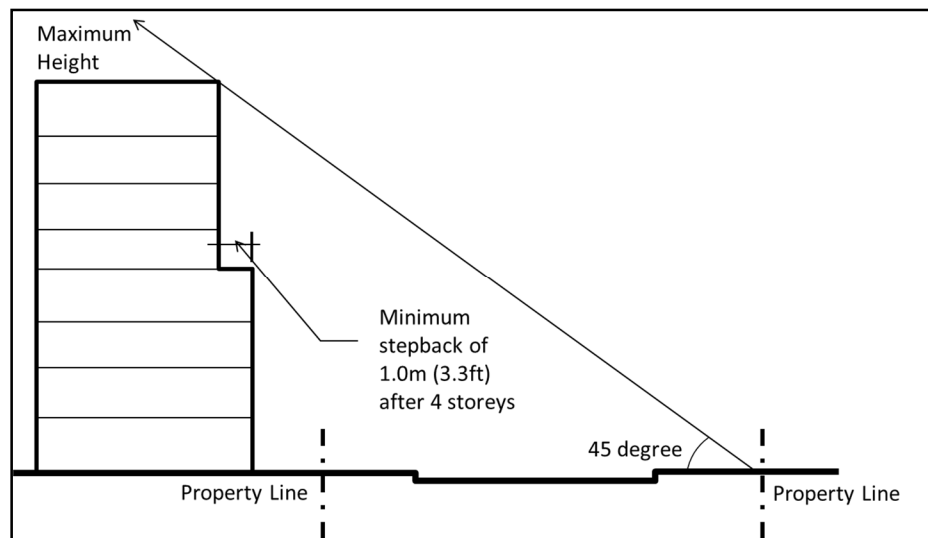


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- b) The maximum building height for buildings greater than 5 storeys shall be limited by the application of a 45 degree angular plane, as per **Figure 6.13b**.

Figure 6.13b: 45 Degree Angular Plane Method for Determining Height Maximums



- c) ~~Vents, mechanical rooms and equipment, elevator penthouses etc. shall be integrated into the architectural treatment of building roof or screened with materials and finishes compatible with the building.~~

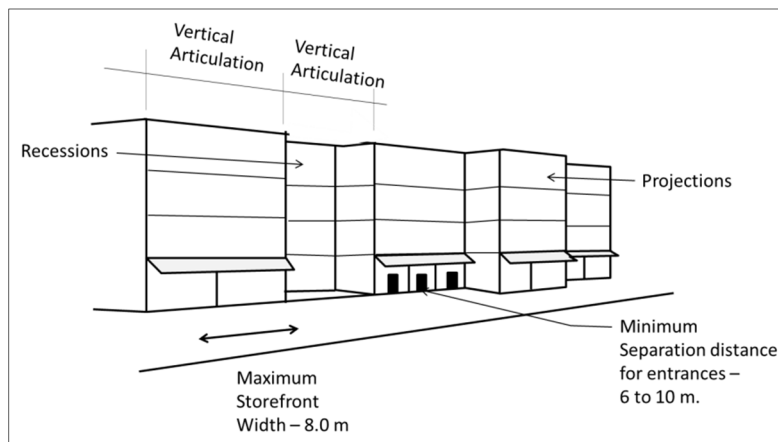
The maximum parapet height for all new buildings shall not exceed 1.5m (4.9ft).

- d) Vents, mechanical rooms and equipment, elevator penthouses, etc. shall be integrated into the architectural treatment of building roof or screened with materials and finishes compatible with the building.

6.13.5 Street Character and Pedestrian Realm

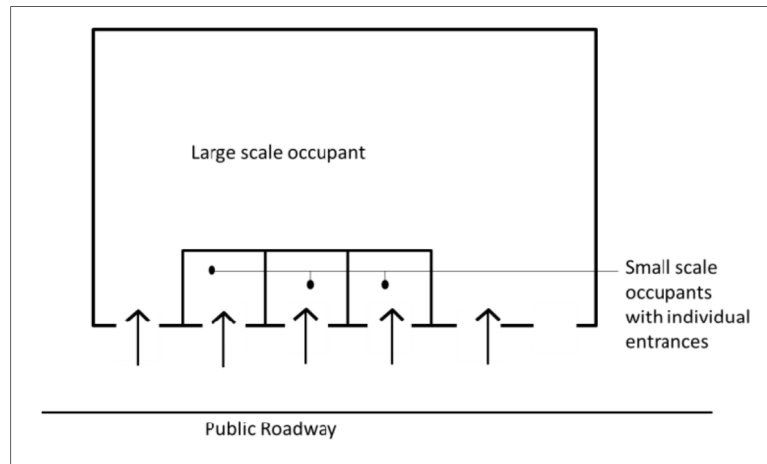
- a) For buildings where the ground floor is occupied by non-residential tenancy, the front setback shall be hard surfaced with a consistent treatment and theme from the City sidewalk to the satisfaction of the Development Authority.
- b) To avoid monotony in architecture, all buildings shall be required to provide a vertical articulation in the streetwall fronting public roads using a variety of colours, materials, projections as well as recessions in the building façade, as per **Figure 6.13c**.

Figure 6.13c: Vertical Articulation Specifications



- c) Individual retail store frontages along 99 Avenue at ground floor shall not exceed 8.0m (26.3ft) in width, as per **Figure 6.13c**.
- d) Where feasible, entrances for commercial and office uses shall be located at intervals of 6.0m to 10.0m (19.7ft to 32.8ft) along building façades fronting public roadway.
- e) For new construction, large scale commercial uses at ground floor shall be required to provide small scale individualized tenancy fronting the public roadway, as per **Figure 6.13d**.

Figure 6.13d: Small Scale Occupancy in Large Scale Commercial Uses



6.13.6 Open Space and Linkages

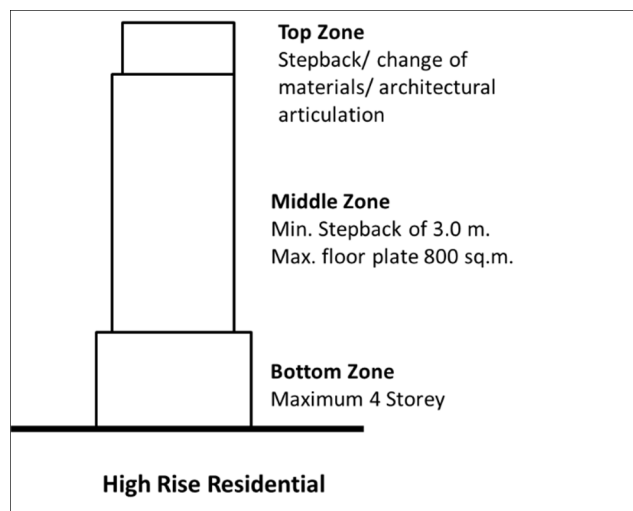
- a) A minimum 15% of the Fort Mall site area shall be dedicated as publically-accessible open space that is connected to the wider city level open space network.
- b) Open space shall be developed and landscaped in accordance with Section 4.8 to 4.11 of this Bylaw. In addition, street furniture such as benches, waste receptacles, garden lighting, etc. may be required to enhance the open space to the satisfaction of Development Authority.
- c) New private or public roads in this district shall be designed in a manner to re-establish the typical block pattern and the street grid found in Fort Saskatchewan's downtown.

6.13.7 Building Massing and Architectural Character

- a) Buildings at the intersection of the following streets shall be required to incorporate special architectural treatment to mark entrances to the downtown and key focal points:
 - i. 99 Avenue and 106 Street; and
 - ii. 99 Avenue and 108 Street.
- ~~b) Mid-rise and high-rise buildings shall provide three distinct vertical zones as per the diagram below, and meet the following step back requirements:~~
 Buildings more than 5 storeys shall provide three distinct vertical zones, as per **Figure 6.13e**, and meet the following step back requirements:

- i. The base zone shall be a minimum of two storeys and a maximum four storeys, and shall be integrated with townhouses, apartments or commercial retail units;
- ii. The middle zone shall provide a minimum setback of 3.0m (9.8ft) and a maximum floor plate of 800m² (8611ft²); and
- iii. The top zone shall be required for high rise buildings and shall include the top three stories. The top zone shall provide either an additional setback or a change in material/colour or special architectural treatment to the satisfaction of the Development Authority.

Figure 6.13e: Vertical Zones in Mid-rise and High-rise Buildings



- c) A minimum separation distance of 25.0m (82.0ft) measured perpendicularly to building face shall be provided between the shafts (middle zones) of two high rise towers.
- d) Building façade on corner sites shall address both public roadways.
- e) New developments shall be encouraged to incorporate public art into building façades.
- f) Large blank façades with opaque surfaces shall be minimised to the satisfaction of the Development Authority.

6.13.8 Pedestrian Entrances

- a) Ground floor entrances for commercial/office uses shall be level with grade of the adjacent sidewalk.

- b) Ground floor entrances for residential units fronting public road shall provide a 1.0m (3.3ft) grade separation from adjacent sidewalk to provide visual privacy for residential units.
- c) Entrances to commercial uses at ground floor and residential uses above ground level shall be architecturally differentiated from each other.

6.13.9 Ground Floor Treatment

a) The land uses along ground floors of all buildings shall be as per **Figure 6.13f**, whereas:

- i. Ground floor uses along 99 Avenue shall be limited to commercial or residential development; and
- ii. Ground floor uses along 98 Avenue shall be limited to residential development.
- ~~iii. Surface and structure parking areas shall be located at the rear of the building and screened from public view.~~

Figure 6.13f: Ground Floor Frontage Use Designations

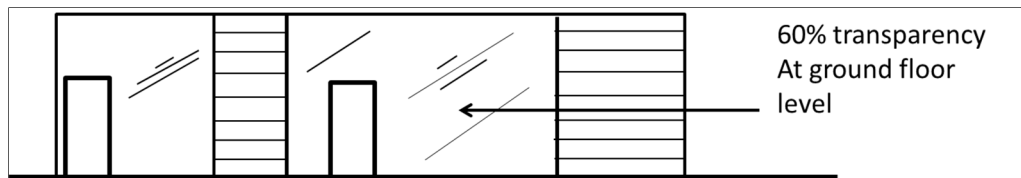


- ~~b) The ground floor of each commercial development shall be required to provide a minimum of 60% transparency measured along the width of the associated parcel.~~

~~Tempered or tinted glass that prohibits visibility shall be considered as opaque surface;~~
and

Facade improvement or facades for newly constructed buildings with non-residential uses located on the ground floor facing a public street or public area shall provide a minimum 60% transparency on the ground floor level to encourage pedestrian interactions and safety, as per **Figure 6.13g**.

Figure 6.13g: Transparency in Ground Level Commercial Developments

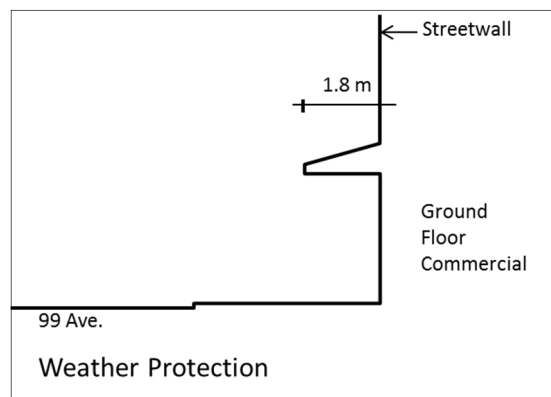


- c) Principal entrances of dwelling units or commercial retail units provided at the ground floor level shall provide direct access to the adjacent public sidewalk.

6.13.10 Canopies and Weather Protection

- a) A continuous weather protection of minimum 1.8m (5.9ft) width at the ground floor of all building façades fronting 99 Avenue shall be encouraged, as per **Figure 6.13h**.

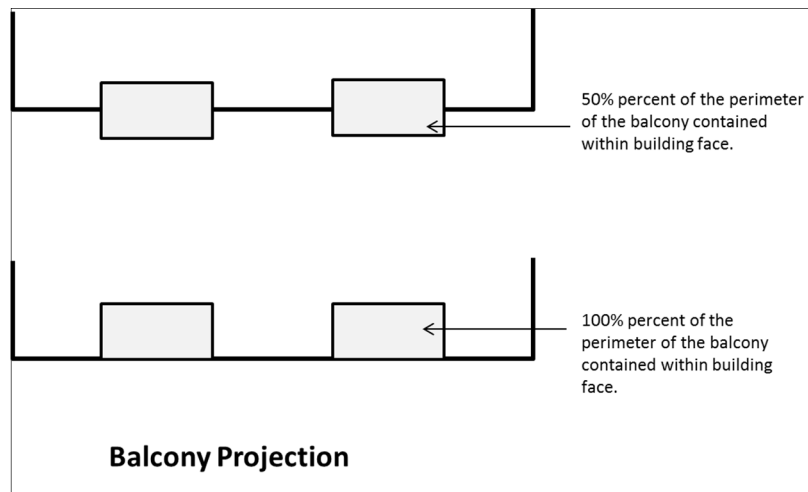
Figure 6.13h: Canopies and Weather Protection



6.13.11 Building Projections

- a) Balconies on the streetwall shall be partly or fully recessed from the building face with ~~a minimum~~ approximately 50% of their perimeter contained by exterior walls of the building, as per **Figure 6.13i**.

Figure 6.13i: Balcony Projection



- b) Balcony projections may project beyond the floor plate restrictions or the front streetwall up to a maximum of 1.0m (3.3ft) but shall in no case project beyond the property line.

6.13.12 General Parking Requirements

- a) On-site parking should be provided at the rear or sides of buildings, within underground parkade or above-ground parking structures. Surface parking areas should not be developed adjacent to any public roadway other than a lane, unless a suitable interface with the abutting street is provided to the satisfaction of the Development Authority.
- b) Corner sites may have surface parking areas located on the side of the building, facing the flanking roadway when screened from public view.
- c) The Development Authority may consider granting additional Floor Area Ratio, if the applicant agrees to provide underground parking stalls to meet all parking requirements of the project.
- d) Uses and developments not specified in an approved Parking Impact Assessment shall meet the Minimum Parking Requirements for Downtown, as per Table 11.e.

- e) Structured parking facilities shall generally be provided at locations internal to the site. If such parking facilities are located fronting a public roadway, then the following design considerations shall be utilized:
 - i. Ground floor shall include retail uses with multiple entrances;
 - ii. Entrance to the parking facility shall be designed with special architectural treatment to maintain the integrity of retail frontage; and
 - iii. The facade of the upper storeys of the parking facility shall be designed to reflect residential or commercial building character.

6.13.12 ~~Parking~~, Circulation, Accesses, Loading and Waste Collection

- ~~a) On-site parking should be provided at the rear or sides of buildings, within underground parkade or above-ground parking structures. Surface parking areas should not be developed adjacent to any public roadway other than a lane, unless a suitable interface with the abutting street is provided to the satisfaction of the Development Authority.~~
- b) All vehicular access to parking and on-site service areas, parking facilities, waste storage/collection areas as well as loading facilities shall be screened from public roadways using enhanced landscape treatment or special architectural features.
- c) Where possible, vehicular entrances to underground parking facilities and passenger drop-off areas shall be provided from the rear of buildings.
- d) Internal roadway network shall be designed to improve walkability and reduce shortcutting by vehicular traffic.
- e) ~~Drive-through service should be limited.~~
- ~~f) Structured parking facilities shall generally be provided at locations internal to the site. If such parking facilities are located fronting a public roadway, then the following design considerations shall be utilised:

 - i. Ground floor shall include retail uses with multiple entrances;
 - ii. Entrance to the parking facility shall be designed with special architectural treatment to maintain the integrity of retail frontage; and
 - iii. The façade of the upper storeys of the parking facility shall be designed to reflect residential or commercial building character.~~

- ~~g) The Development Authority may consider granting additional Floor Area Ratio, if the applicant agrees to provide underground parking stalls to meet all parking requirements of the project.~~
- h) Garbage and recycling containers shall provide a minimum setback of 1.0m (3.3ft) from a property line and be screened using appropriate architectural or landscaping treatment to the satisfaction of Development Authority.
- ~~i) Designated areas for storage, temporary truck parking, waste collection, compaction, and loading shall have a minimum setback of 7.5m (24.6ft) from public roadway and a minimum separation of 25.0m (82.0ft) from residential buildings.~~

6.13.13 Signage

- a) Buildings on corner sites shall provide signage on both building façades.
- b) Projecting signs may project beyond the streetwall by a maximum of 1.0m (3.3ft) and should be restricted to ground floor only.

6.13.14 Additional Development Regulations for C5

- a) All development and uses within this Land Use District are subject to the applicable provisions of Part 4 - General Regulations for all Land Use Districts, Sections 6.1 to 6.7 of Part 6 - Commercial Land Use Districts, Part 11 - Parking and Loading, and Part 12 – Signs. ~~and are subject to all provisions from the Downtown Area Redevelopment Plan (DARP) or other Statutory Plans adopted by Council;~~
- b) Except for off-street parking, loading areas and approved patios, all business activities shall be carried out entirely within completely enclosed buildings or structures. Sidewalk sales, tent sales, or farmers markets shall be considered in the approved open space areas such as parking lots or plazas in accordance with the regulations for Temporary Outdoor Events; and
- c) The siting and appearance of all buildings or improvements, and the landscaping of the site shall be to the satisfaction of the Development Authority in order that there shall be general conformity with adjacent buildings, and that there may be adequate protection afforded to the amenities of adjacent buildings and sites. The form and character of buildings shall complement adjacent residential character of the neighbourhood.

PART 13 – DEFINITIONS

General Definitions

DISPLAY GARDENS means an area dedicated to planting that provides privacy for residential uses, and improves streetscape aesthetics.

FLOOR AREA RATIO (FAR) means the numerical value of the gross floor area on all levels of all buildings on a lot, divided by the area of the lot.

¹**HIGH DENSITY RESIDENTIAL** means residential development at a density of over 70 dwelling units per net developable hectare ~~for developments outside of the C5 – Fort Mall Redevelopment District. High Density Residential within the C5 – Fort Mall Redevelopment District means residential development at a density of 91-250 dwelling units per net developable hectare and is 9 to 15 storeys, except when located in the Downtown or C5 Districts.~~

¹²**LOW DENSITY RESIDENTIAL** means residential development at a density up to 35 dwelling units per net developable hectare ~~for developments outside of the C5 – Fort Mall Redevelopment District. Low Density Residential within the C5 – Fort Mall Redevelopment District means residential development at a density not to exceed 50 dwelling units per net developable hectare and is 0 to 4 storeys, except when located in the Downtown or C5 Districts.~~

¹**MEDIUM DENSITY RESIDENTIAL** means residential development at a density of 36-70 dwelling units per net developable hectare ~~for developments outside of the C5 – Fort Mall Redevelopment District. Medium Density Residential within the C5 – Fort Mall Redevelopment District means residential development at a density of 51-90 dwelling units per net developable hectare and is 5 to 8 storeys, except when located in the Downtown or C5 Districts.~~

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² C19-14