



CITY OF FORT SASKATCHEWAN AGENDA

Regular Council Meeting Tuesday, March 10, 2015 - 6:00 PM Council Chambers – City Hall

1. Call to Order

2. Approval of Minutes

- 2.1 Approval of Minutes of February 24, 2015 Regular Council Meeting (attachment)

3. Delegations

Those individuals in attendance at the meeting will be provided with an opportunity to address Council regarding an item on the agenda, with the exception of those items for which a Public Hearing is required or has been held. Each individual will be allowed a maximum of five (5) minutes.

4. Presentation

- 4.1 AltaLink Update

Ed Toupin,
Mgr.,
Municipal &
Community
Relations,
AltaLink
(attachment)

5. Public Hearing

Bylaw C5-15 to Amend Land Use Bylaw C10-13 by Redistricting a Portion of SE 1/4 Section of 19-54-22-W4M and a Portion of Lot 1, Block 1, Plan 022 6974 from UR - Urban Reserve District to RC - Comprehensively Planned Residential District, R3 - Small Lot Residential District, and DC(A)-07 - Direct Control - Residential (Southfort Meadows Stage 5) - Southfort Meadows Stage 4 and 5

Janel
Smith-
Duguid
(verbal)

6. Business Arising from Public Hearing

Janel Smith-
Duguid

(attachment)

6.1 Bylaw C5-15 to Amend Land Use Bylaw C10-13 by Redistricting a Portion of SE 1/4 Section of 19-54-22-W4M and a Portion of Lot 1, Block 1, Plan 022 6974 from UR - Urban Reserve District to RC - Comprehensively Planned Residential District R3 - Small Lot Residential District, and DC(A)-07 - Direct Control - Residential (Southfort Meadows Stage 5) - Southfort Meadows Stage 4 and 5 - 2nd and 3rd reading

Janel Smith-Duguid
(attachment)

7. Unfinished Business

8. New Business

8.1 Fort Saskatchewan Food Gatherers Society - Request for Land Lease

Troy
Fleming /
Patty
Gignac /
Marlene
Leroux,
Fort Sask.
Food
Gatherers
Society

9. Bylaws

9.1 Bylaw C8-15 - Amend Land Use Bylaw C10-13 - Introduction of the RHR - High Rise Residential District - 1st reading

Craig
Thomas
(attachment)

9.2 Bylaw C9-15 - Adopt the Josephberg Road North Industrial Area Structure Plan - 1st reading

Claire
Negrin
(attachment)

10. Mayor and Councillors Boards/Committees Update

11. Administrative Inquiries

12. Notice of Motion

13. Adjournment



**CITY OF FORT SASKATCHEWAN
MINUTES
REGULAR COUNCIL
Tuesday, February 24, 2015 - 6:00 PM
Council Chambers – City Hall**

Present:

Members of Council:

Mayor Gale Katchur
Councillor Birgit Blizzard
Councillor Sheldon Bossert
Councillor Frank Garritsen
Councillor Stew Hennig
Councillor Arjun Randhawa
Councillor Ed Sperling

Administration:

Kelly Kloss, City Manager
Troy Fleming, General Manager, Infrastructure & Community Services
Brenda Rauckman, General Manager, Corporate & Protective Services
Brenda Molter, Director, Legislative Services
Wendy Kinsella, Director, Communications and Marketing
Barb Shuman, Director, Recreation
Matthew Siddons, Current Planner
Reade Beaudoin, Digital Media Coordinator
Sheryl Exley, Legislative Officer
Dorothy MacMillan, Legislative Officer

1. Call to Order

Mayor Katchur called the regular Council Meeting of February 24, 2015 to order at 6:00 p.m.

2. Approval of Minutes

2.1 Approval of Minutes of February 10, 2015 Regular Council Meeting

R43-15

MOVED BY Councillor Blizzard that the minutes of the February 10, 2015 regular Council Meeting be adopted, as amended.

In Favour: Gale Katchur, Frank Garritsen, Stew Hennig, Arjun Randhawa, Birgit Blizzard, Sheldon Bossert, Ed Sperling

CARRIED UNANIMOUSLY

3. Delegations

None.

4. Unfinished Business

4.1 Recreation Access Program

Presented by: Barb Shuman, Director, Recreation

R44-15

MOVED BY Councillor Randhawa that Council approve the Recreation Access Program, in principle, with funding to implement the program being considered as part of the 2016 Budget deliberations.

In Favour: Gale Katchur, Frank Garritsen, Stew Hennig, Arjun Randhawa, Birgit Blizzard, Sheldon Bossert, Ed Sperling

CARRIED UNANIMOUSLY

5. New Business

5.1 Beaver Hills Initiative (BHI) – Letter of Support

Presented by Matthew Siddons, Current Planner, Glen Lawrence, Chair, BHI, Dr. Guy Swinnerton, University of Alberta Professor

R45-15

MOVED BY Councillor Garritsen that Council provide a letter of support to the Beaver Hills Initiative - Biosphere Reserve nomination.

In Favour: Gale Katchur, Frank Garritsen, Stew Hennig, Arjun Randhawa, Birgit Blizzard, Sheldon Bossert, Ed Sperling

CARRIED UNANIMOUSLY

5.2 City Naming Registry

Presented by: Brenda Molter, Director, Legislative Services

R46-15

MOVED BY Councillor Blizzard that Council approve the inclusion of the name, Jim Sheasgreen, to the City Naming Registry.

In Favour: Gale Katchur, Frank Garritsen, Stew Hennig, Arjun Randhawa, Birgit Blizzard, Sheldon Bossert, Ed Sperling

CARRIED UNANIMOUSLY

5.3 Fort Saskatchewan Policing Committee Appointment

Presented by: Brenda Molter, Director, Legislative Services

R47-15

MOVED BY Councillor Bossert that Council approve the appointment of Cameron McCormick to the Fort Saskatchewan Policing Committee for the balance of a three-year term commencing February 25, 2015 and expiring on December 31, 2016.

In Favour: Gale Katchur, Frank Garritsen, Stew Hennig, Arjun Randhawa, Birgit Blizzard, Sheldon Bossert, Ed Sperling

CARRIED UNANIMOUSLY

6. Bylaws

None.

7. Mayor and Councillors Boards/Committees

Members of Council provided an update on current and upcoming activities for Boards/Committees.

8. Administrative Inquiries

Members of Council were given the opportunity to ask questions and provide concerns and comments.

9. Notice of Motion

None.

10. Adjournment

R48-15

MOVED BY Councillor Hennig that the regular Council Meeting of February 24, 2015 adjourn at 7:18 p.m.

In Favour: Gale Katchur, Frank Garritsen, Stew Hennig, Arjun Randhawa, Birgit Blizzard, Sheldon Bossert, Ed Sperling

CARRIED UNANIMOUSLY

Mayor

Director, Legislative Services

CITY OF FORT SASKATCHEWAN

AltaLink Update

Purpose:

Mr. Ed Toupin, Manager, Municipal and Community Relations, AltaLink will be in attendance to provide an update to members of Council and Administration on AltaLink and its electrical transmission projects in Fort Saskatchewan and area.

Action Required:


That Mr. Ed Toupin be thanked for his presentation.


Attachment:

Appendix A - AltaLink's PowerPoint Presentation.

File No.:

Prepared by:	Sheryl Exley Legislative Officer	Date: March 2, 2015
Approved by:	Brenda Molter Director, Legislative Services	Date: March 2, 2015
Approved by:	Brenda Rauckman General Manager, Corporate & Protective Services	Date: March 3, 2015
Reviewed by:	Troy Fleming Acting City Manager	Date: March 3, 2015
Submitted to:	City Council	Date: March 10, 2015





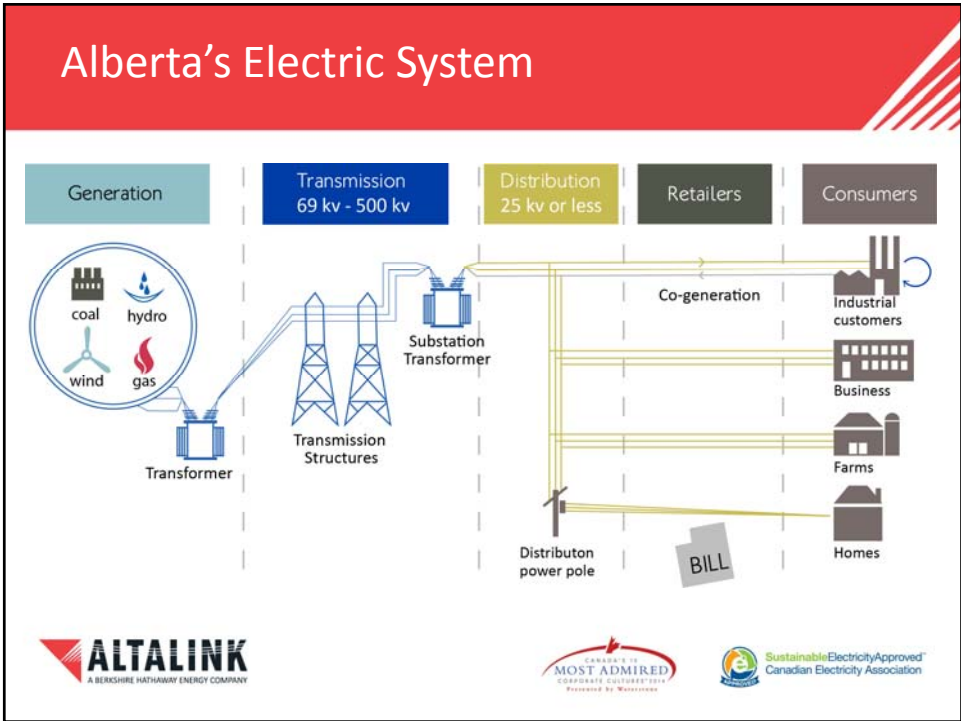


**AltaLink – Supporting Alberta’s Growth
City of Fort Saskatchewan**

March 2015

Ed Toupin





Three key industry players in Alberta's Transmission Industry

Alberta Electric System Operator (AESO)

System Planners

Alberta Utilities Commission (AUC)

Regulatory Body

Transmission Facilities Owners (TFO)

Owners and Operators



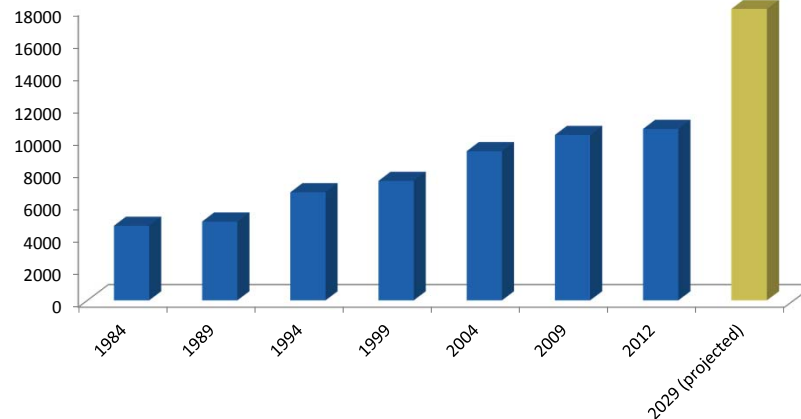
Who is AltaLink?

- Established in 2002
- 100% focused on electricity transmission
- Our numbers:
 - 12,000 km of transmission lines
 - 300 substations
 - Serve 85% of Provincial Pop.



Reliable power is critical to Alberta

Alberta Peak Electricity Demand (MW)



Source: Alberta Electric System Operator



Distribution of Electrical Load

- Industry is over 60%
 - Large industrial Plants
 - Smaller local industrial businesses
- Commercial is 20%
- Residential is 15%
- Agricultural is 5%
 - Farms and Ranches



Projects in Fort Sask. Area

- Bannerman Sub Transmission Lines
 - 2 -240 KV lines to new refinery
- Substation Upgrades
 - Shell Scotford sub
 - Amelia sub
- ATCO Electric activities
 - Gas Generation project
 - EATL completion



Maintaining the Transmission System

- Planned Maintenance
- Emergency Maintenance



AltaLink- a partner in your community

- Technology Scholarships
 - 12 - \$1000 Annually
- First Nations Scholarships
 - 4 - \$2000 Annually
- United Way Campaigns
- AltaLink and Alberta 4-H
 - Supporting all 420 clubs



Thank you.

For more information
please visit:

ed.toupin@altalink.ca



CITY OF FORT SASKATCHEWAN

Bylaw C5-15 to Amend Land Use Bylaw C10-13 by Redistricting a Portion of SE ¼ Section of 19-54-22-W4M and a Portion of Lot 1, Block 1, Plan 022 6974 from UR - Urban Reserve District to RC – Comprehensively Planned Residential District, R3 – Small Lot Residential District, and DC(A)-07 - Direct Control – Residential (Southfort Meadows Stage 5)

Motions:

1. That Council amend Bylaw C5-15 by replacing Subsection 9.12.7(a) – Combined Side Yard Regulations with the following regulation: “The reduced setback shall only be applied to the block in its entirety or a minimum of seven contiguous single detached lots.”
2. That Council give second reading to Bylaw C5-15 to amend Land Use Bylaw C10-13 by redistricting a Portion of SE ¼ Section of 19-54-22-W4M and a Portion of Lot 1, Block 1, Plan 022 6974 from UR - Urban Reserve District to RC – Comprehensively Planned Residential District, R3 – Small Lot Residential District, and DC(A)-07 - Direct Control – Residential (Southfort Meadows Stage 5), as amended.
3. That Council give third reading to Bylaw C5-15 to amend Land Use Bylaw C10-13 by redistricting a Portion of SE ¼ Section of 19-54-22-W4M and a Portion of Lot 1, Block 1, Plan 022 6974 from UR - Urban Reserve District to RC – Comprehensively Planned Residential District, R3 – Small Lot Residential District, and DC(A)-07 - Direct Control – Residential (Southfort Meadows Stage 5).

Purpose:

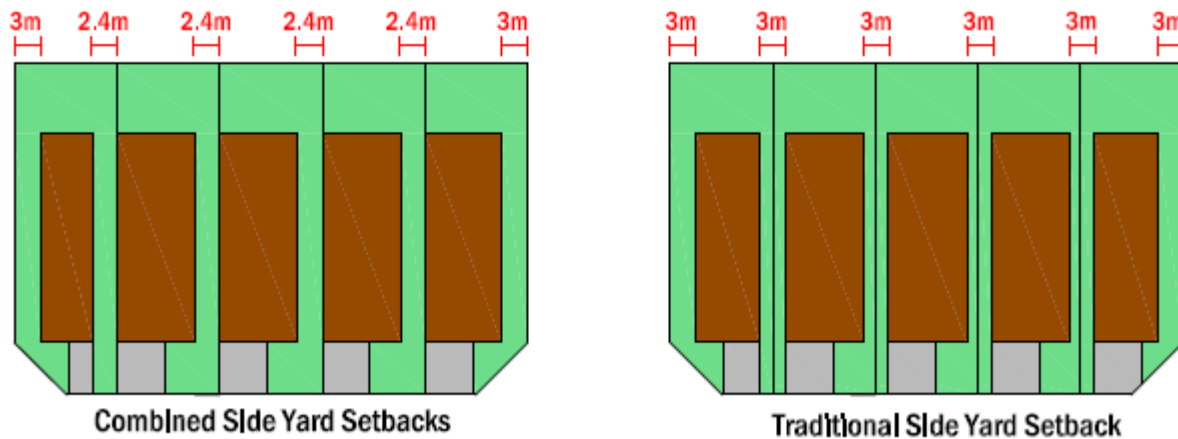
The purpose of Bylaw C5-15 is to redistrict lands for Southfort Meadows Stage 4 and Stage 5. Three residential zoning requests have been made, including a Direct Control (Administration) zoning. The purpose of the Direct Control zoning is to introduce a pilot project to test the feasibility of regulations designed to encourage innovative design concepts and offer a greater diversity of housing types.

The intent of the Combined Setback Pilot Project is to convert two seemingly unusable setback areas into one useable setback area. The combined setback would be applied when:

- The opposite side yard achieves a setback of 2.4m;
- The combined setbacks apply to an entire block or a minimum of eight contiguous lots;
- An easement has been registered on title of the adjacent property permitting access for maintenance and drainage; and
- The architectural controls have been enhanced to create a signature street.

This project is considered a pilot project given the unique features being proposed, including combined setbacks and enhanced architectural designs. Pilot projects such as this represent an opportunity for the City of Fort Saskatchewan to test a unique housing product to the area and offer a diversity of housing product to current and new residents.

As per the Land Use Bylaw, a total of 3.0m separation between houses is required, provided through two separate setbacks. With the pilot project, a total of 2.4m separation is required, provided by one setback, as demonstrated in the following conceptual diagram:



The developer estimates a \$7,500 cost savings will be created for the builder through reduced lot costs. The developer intends to require builders to transfer the reduced cost into improved external architectural features, such as façade design, materials, textures, colours, rooflines, windows, doors, and/or decorative detailing. The value contributed to side yard setback remains consistent, as demonstrated in the following equations:

Traditional side yard equation:

$$1.5\text{m (land dedication)} + 1.5\text{m (land dedication)} = 3.0\text{m}$$

Pilot project side yard equation:

$$2.4\text{m (land dedication)} + 0.6\text{m (improved architecture)} = 3.0\text{m}$$

The traditional dedication to side yard setback remains consistent in terms of cost. However, the utilization of the cost has been modified. Therefore, the question before Council relates to the prioritization of ensuring all new lots physically provide the 3.0m side yard setback versus providing flexibility in pursuit of innovation and improved streetscapes.

Administration does not typically provide input into architectural controls, but through Subsection 9.12.7(c) of the Direct Control zoning under amending Land Use Bylaw C5-15, the architectural controls are to be developed collaboratively between the developer and the Subdivision Authority. At the time of permit application, should the Development Authority consider the approved architectural controls not in adherence, the development permit could be refused as per Subsection 9.12.7(d) of the proposed Land Use Bylaw amendment C5-15. Through these regulations, Administration has the ability to influence architectural standards in accordance with Council's direction, helping to ensure implementation of a signature street.

Background:

An application was received from WSP Canada Inc. (formerly Focus) on behalf of Southfort Development Corp. This subject area is located south of Southfort Estates and east of the existing Southfort Meadows neighbourhood.

Bylaw C5-15 received first reading at the February 10, 2015 regular Council meeting. At that time, Council provided Administration with the following question:

What assurances does Council have that the Direct Control zoning can be limited to the subject area?

With all redistricting, Council serves as the legislative body that ultimately assesses and assigns the appropriate districting. Through this process, Council is obligated to consider previously approved statutory plans, such as the *Municipal Development Plan* and *Area Structure Plans*. Council is also required to hold a fair procedure for determining the appropriate district. With consideration for statutory plans and evidence provided through the procedure, it is ultimately Council's decision to determine the appropriate zoning.

Frederick Laux's Planning Law and Practice in Alberta describes the application of site-specific Direct Control Districting as follows:

.. "The district may be applied in those circumstances where, although the proposed development could be accommodated by applying a conventional district to the subject lands, the unusual nature of the project, site or neighbourhood are such that the use of a conventional district would be 'inappropriate or inadequate'." (page 6-39)

In this instance, Administration considers the unusual nature of the project warrants the use of Direct Control zoning. All future applications would be assessed on a case by case basis.

Comparable regulations have been introduced in Edmonton, Leduc, and Spruce Grove. The regulations in Edmonton require a 1.5m setback, as opposed to the 2.4m setback. As such, the majority of complications experienced in Edmonton relate to the increase in density.

At full build-out, the proposed redistricting is estimated to provide approximately 110 dwelling units. The estimated density for Southfort Ridge Stages 4 and 5 is 25 du/ndrha, which achieves the Capital Region Growth Plan density targets of 25 du/ndrha – 30 du/ndrha.

Southfort Development Corp.'s model expands across multiple quarter sections. Previously, starter products have been concentrated to Southfort Ridge, move-up product located within the Meadows, and large move-up product concentrated to the Estates. The final stage of Southfort Ridge received approvals in 2014. Therefore, the intent is to now provide move-up and starter product within the Meadows.

Council-approved planning documents support a diversity of housing products to create diverse neighbourhoods. However, the transition from the Estates (7du/ndrha) to the proposed product (27du/ndrha) is significant. The Direct Control zoning allows single detached and semi-detached dwellings. To ease the transition while supporting an increase in density and diversity, the regulations limit lots abutting the Estates to single detached dwellings. Semi-detached dwellings would be permitted in the center of the block where the storm pond serves as a large buffer.

A minor amendment is being requested to Bylaw C5-15 since first reading. Subsection 9.12.7(a) previously required the combined setbacks to be applied to no less than eight contiguous lots to ensure consistency. Through subdivision design it was determined a minimum requirement of seven contiguous lots was more conducive. Therefore, Bylaw C5-15 and the regulation will be amended from eight to seven contiguous single detached lots.

Plans/Standards/Legislations:

The Municipal Development Plan (MDP), Community Sustainability Plan (CSP), and Southfort Area Structure Plan (SF-ASP) provide policy direction in relation to this application. An analysis of the application against relevant policies is provided as Appendix "B".

The above-mentioned plans support a diversity of housing products and housing innovation. The proposed regulations, combined with the requirement for improved architectural standards, will create a product currently not available within Fort Saskatchewan. Should the pilot project be permitted, five different housing products will be visible from the intersection of Meadowview Drive and Greenfield Link. This level of integration within a developing community is unique in the Edmonton region.

The proposed redistricting places a higher emphasis on the streetscape and improved architectural controls, which is a common goal of the above noted plans. The plans also place a high emphasis on access to passive and active recreation opportunities, which has been accommodated throughout the community as shown in the Outline Plan provided as Appendix "E". The proposed plan excels at integrating housing forms and lot sizes, improving architectural styles to enhance the public realm, and test a pilot project for innovation. Opportunity for improvement exists for integration of sustainability practices, and limiting driveway access to a collector road (although concerns regarding the impact have been mitigated).

As per the *Municipal Government Act*, adjacent landowners were notified by mail and advertisements were published in a local newspaper for two consecutive weeks. At the time this report was completed, no submissions regarding the proposed bylaw were received.

Financial Implications:

The City of Fort Saskatchewan would be responsible for regular maintenance of the roads and public lands servicing the developed area as part of usual municipal operations (waste pickup, fire, policing, snow removal, utilities, roads and the related hardware, future infrastructure, etc.).

Alternatives:

1. That Council give second and third reading to Bylaw C5-15 to amend Land Use Bylaw C10-13 by redistricting a Portion of SE ¼ Section of 19-54-22-W4M and a Portion of Lot 1, Block 1, Plan 022 6974 from UR - Urban Reserve District to RC – Comprehensively Planned Residential District, R3 – Small Lot Residential District, and DC(A)-07 - Direct Control – Residential (Southfort Meadows Stage 5).
2. That Council not proceed with second and third reading to Bylaw C5-15, thus leaving the lands in their current legal designation, and advise how they wish to proceed.

Attachments:

1. Bylaw C5-15
2. Schedule "A" – Map Amendment to Appendix A of Bylaw C10-13
3. Appendix "A" – Air Photo
4. Appendix "B" – Relevant Policies
5. Appendix "C" – Draft DC(A)-07 Land Use Bylaw District
6. Appendix "D" – Requested existing Land Use Bylaw Districts (RC & R3)
7. Appendix "E" – Southfort Ridge/Meadows Outline Plan

File No.: Bylaw C5-15

Prepared by:	Janel Smith-Duguid Director, Planning & Development	Date: February 19, 2015
Approved by:	Troy Fleming General Manager, Infrastructure & Community Services	Date: February 23, 2015
Reviewed by:	Kelly Kloss City Manager	Date: February 23, 2015
Submitted to:	City Council	Date: March 10, 2015



CITY OF FORT SASKATCHEWAN

A BYLAW OF THE CITY OF FORT SASKATCHEWAN IN THE PROVINCE OF ALBERTA TO AMEND BYLAW C10-13, LAND USE BYLAW

BYLAW C5-15

WHEREAS the *Municipal Government Act*, R.S.A.,2000, c.M-26 as amended or repealed and replaced from time to time, provides that a municipality has the power to amend the Land Use Bylaw;

NOW THEREFORE, the Council of the City of Fort Saskatchewan, in the Province of Alberta, duly assembled, enacts as follows:

1. That Appendix A, Land Use District Map, covering Part of SE ¼ Section of 19-54-22-W4M and Portion of Lot 1, Block 1, Plan 022 6974 be amended to redistrict from UR - Urban Reserve District to RC – Comprehensively Planned Residential District, R3 – Small Lot Residential District, and DC(A)-07 - Direct Control – Residential (Southfort Meadows Stage 5) as shown on the attached Schedule “A”.
2. This Bylaw is cited as the Amendment to Land Use Bylaw C10-13, as amended, repealed, and/or replaced from time to time.
3. That Schedule “A” of Bylaw C10-13 be amended as follows:
 - (a) by adding the following Direct Control Land Use District:

“9.12 DC(A)-07 DIRECT CONTROL - Residential (Southfort Meadows Stage 5)

9.12.1 Purpose

The general purpose of this Land Use District is to establish site-specific direct control provisions to facilitate a Pilot Project to assess the feasibility of combining side yards. Dwelling units may be located up to one side property line, provided the regulations are met or exceeded. Where the combined side yard is applied, architectural controls shall require the front façade to be designed and constructed to a high architectural quality, providing variety and visual interest within the development that differentiates from adjacent developments. This District is intended to provide innovative development to maximize housing variety while creating a signature streetscape.

9.12.2 Area of Application

Portion of Southeast ¼ Section of 19-54-22-W4M and a portion of Lot 1, Block 1, Plan 022 6974, as shown in **Figure 17**.

9.12.3 DC(A)-07 Uses:

- Accessory development
- Bed and breakfast
- Community garden
- Day care facility (limited)
- Group home (limited)
- Home business
- Home office
- Show home
- Semi-detached dwelling
- Single detached dwelling
- Swimming pool
- Temporary sales centre

9.12.4 DC(A)-07 Site Subdivision Regulations for Single Detached Dwellings

	Interior Site	Corner Site
Site Area	353.6m ² (3,791.0ft ²) minimum	394.4m ² (4,237.6ft ²) minimum
Site Width	10.4m (34.0ft) minimum	11.6m (38.0ft) minimum
Site Depth	34.0m (111.5ft) minimum	

9.12.5 DC(A)-07 Site Subdivision Regulations for Semi-Detached Dwellings

	Interior Site	Corner Site
Site Area	299.2m ² (3,222.4ft ²) minimum	350.3m ² (3,768.7ft ²) minimum
Site Width	8.8m (28.9ft) minimum	10.3m (33.8ft) minimum
Site Depth	34.0m (111.5ft) minimum	

9.12.6 DC(A)-07 Site Development Regulations for Single Detached and Semi-Detached Dwellings

	Interior Site or Corner Site	
Front Yard Setback	Front	6.0m (19.7ft) minimum 7.0m (23.0ft) maximum
	Flanking	3.0m (9.8ft) minimum 4.5m (14.8ft) maximum
Rear Yard Setback	8.0m (26.2ft) minimum	
Side Yard Setback	1.5m (4.9ft) minimum. Combined side yards may be considered as per Section 9.12.6.	
Principal Building Height	Two and one half (2 ½) storeys not to exceed 10.0m (32.8ft) maximum	
Site Coverage	45% maximum for principal building over one storey, excluding decks	
	50% maximum for principal building of one storey, excluding decks	
	50% maximum for all buildings and structures where principal building is over one storey	
	55% maximum for all buildings where principal building is one storey	
Density	Maximum of one dwelling unit per site	

9.12.7 Combined Side Yard Regulations:

- a) The reduced setback shall only be applied to the block in its entirety or a minimum of eight contiguous single detached lots.
- b) For single detached dwellings, one side yard setback may be reduced to 0.0m (0.0ft) where:
 - i. The other side yard achieves a minimum setback of 2.4m;
 - ii. The 0.0m (0.0ft) setback is located next to the minimum 2.4m (7.9ft) side yard setback of the abutting lot; and
 - iii. A Right-of-Way Easement with a minimum width of 1.5m is registered against Title of the adjacent site that provides for:

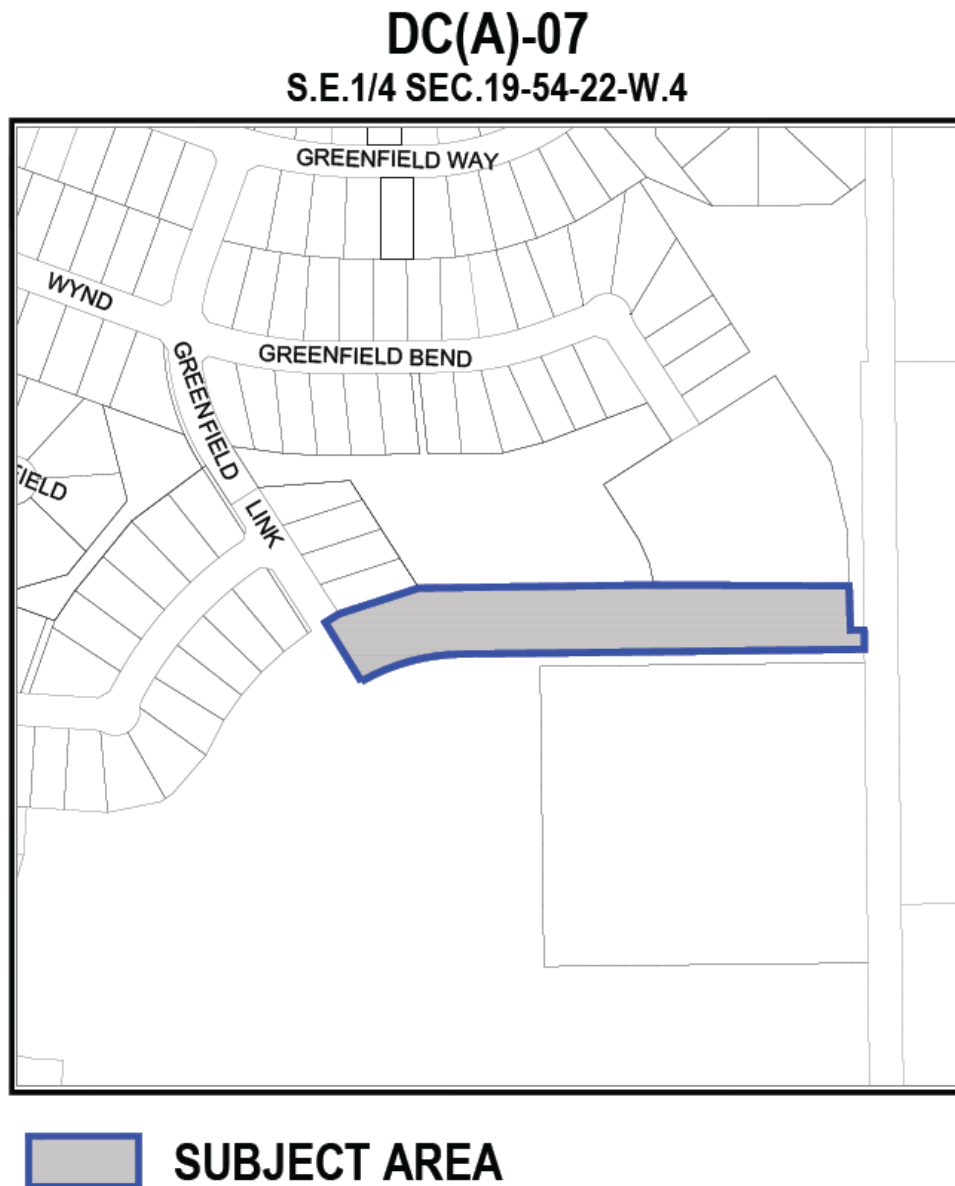
- a. The encroachment of eaves from the subject dwelling into the easement a maximum distance of 0.6m (2.0ft);
 - b. The encroachment of the footing of the subject dwelling into the easement a maximum distance of 0.6m (2.0ft);
 - c. A drainage swale, constructed to the satisfaction of the City of Fort Saskatchewan; and
 - d. Permission to access the easement area for maintenance of both properties.
- c) To translate the savings in land costs facilitated by combined setbacks into improved quality and aesthetics, architectural controls for the exterior of the principal building shall incorporate equivalently priced upgrade requirements. The architectural controls shall be developed collaboratively between the subdivision applicant and the Subdivision Authority. Upgraded elements may include, but are not limited to: façade design, materials, textures, colours, rooflines, windows, doors, and decorative detailing.
- d) All applications for Development Permits within the DC(A)-07 District shall demonstrate façade design and architectural elements in accordance with the approved architectural controls, to the satisfaction of the Development Authority.

9.12.8 Additional Development Regulations for DC(A)-07:

- a) All development and uses within this Land Use District are subject to the applicable provisions of Part 4 – General Regulations for all Land Use Districts, Sections 5.1 to 5.13 of Part 5 – Residential Land Use Districts, Part 11 – Parking and Loading, and Part 12 – Signs.
- b) The Development Authority shall have the ability to vary the DC(A)-07 District regulations in accordance with Section 3.3 of this Bylaw.
- c) Where a lot abuts a lot zoned Residential Estate Lot District, semi-detached dwellings shall be prohibited.
- d) No less than 50% of the side yard shall be landscaped with soft landscaping elements such as trees, shrubs, plants, lawns, and ornamental plantings.

(b) by adding the following map:

- (i) **“FIGURE 17 - Applicable Area DC(A)-07 – Residential (Southfort Meadows Stage 5)”**



4. If any portion of this Bylaw is declared invalid by a court of competent jurisdiction, the invalid portion must be severed and the remainder of the Bylaw is deemed valid.

5. This Bylaw becomes effective upon third and final reading.

READ a first time this 10th day of February 2015.

READ a second time this _____ day of _____ 2015.

READ a third time and passed this day of 2015.

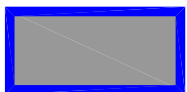
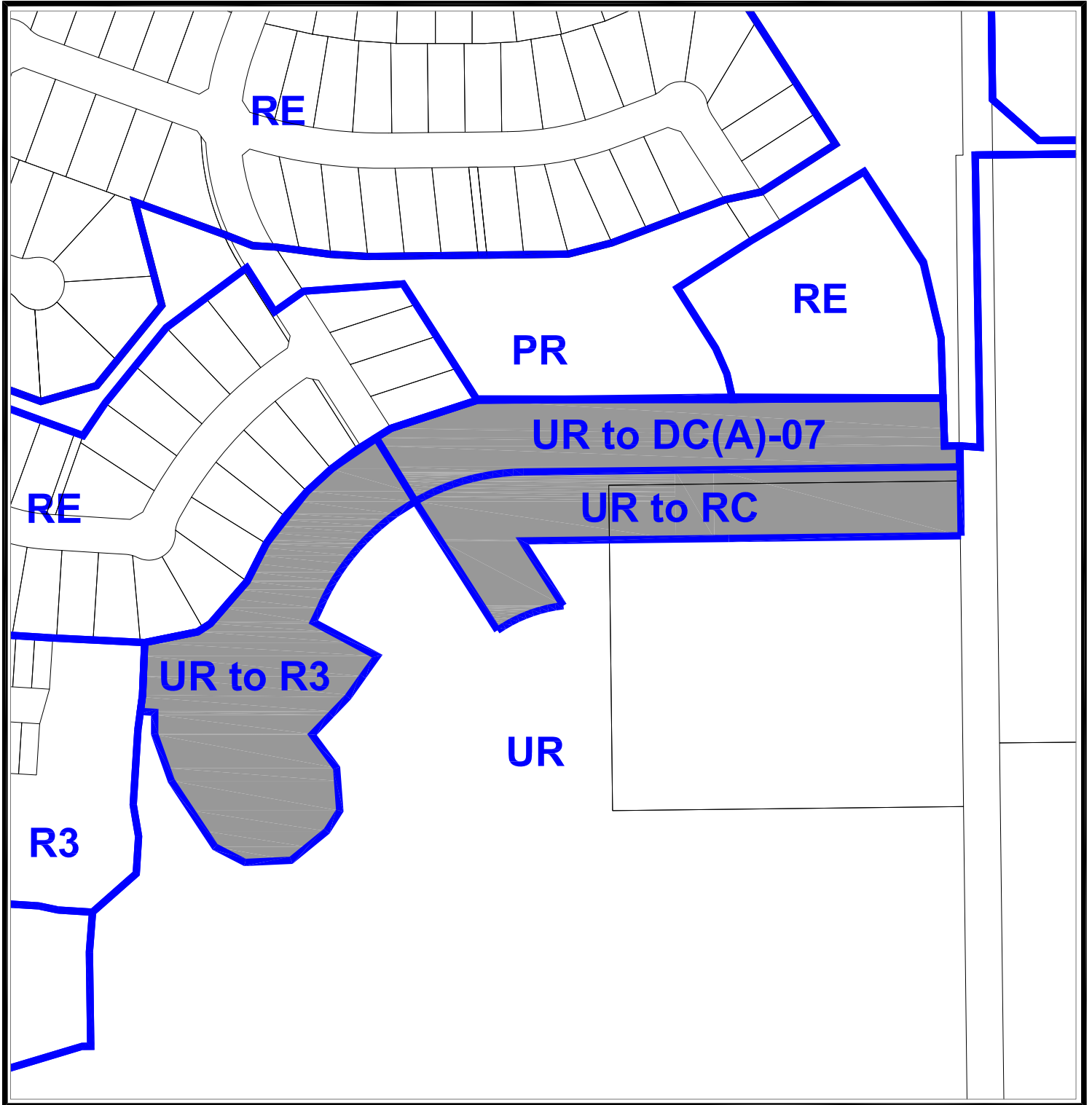
MAYOR

DIRECTOR, LEGISLATIVE SERVICES

DATE SIGNED: _____

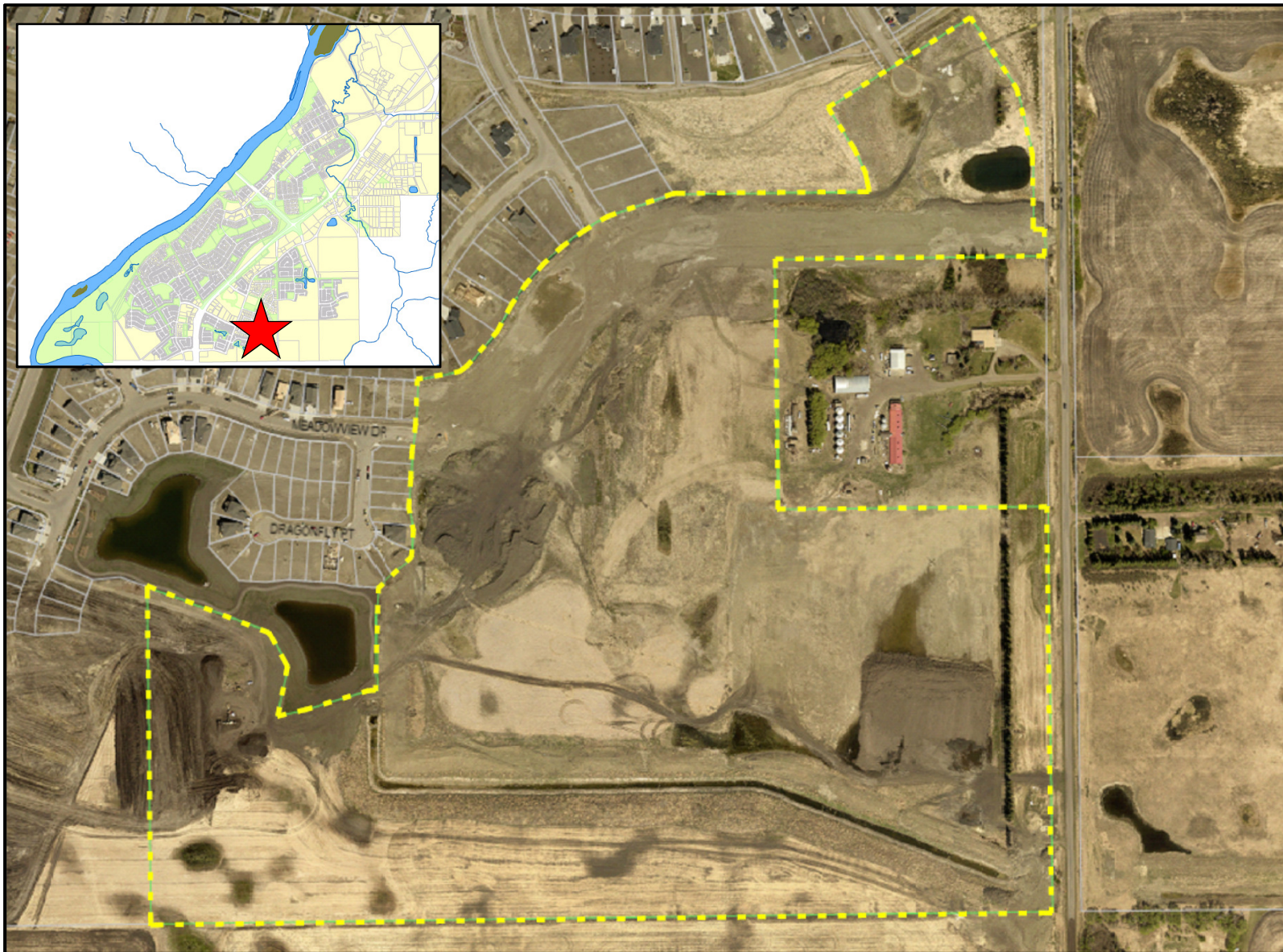
BYLAW C5-15

Schedule "A"



SUBJECT AREA

APPENDIX A



DISCLAIMER: The information shown is for reference only. The City of Fort Saskatchewan disclaims all responsibility for the accuracy, completeness, timeliness and merchantability of information shown. Use this information at your own risk

Related Policy to Bylaw C5-15
Southfort Meadows Stage 4 & 5 Redistricting

Municipal Development Plan – Bylaw C16-10
Designation: Developing Community Area

6.0 Developing Community Area		
6.4.5	Require that new greenfield development follows an efficient and logical pattern of phasing.	Satisfactory
6.4.7	Reinforce the development of complete neighbourhood units by encouraging a range of dwelling unit types and densities, along with supporting services, in each residential neighbourhood within the Developing Community Area.	Satisfactory
7.0 Community Design		
7.1.4	Recognizing the role that streets and parks play as a key feature of the public realm, encourage building and housing development that face public streets and parks, rather than turning its back on the public realm.	Satisfactory
7.1.7	In undeveloped portions of Westpark and Southfort, promote small block sizes, encouraging the development of 60 to 100 metre block widths in order to encourage walking and cycling.	Opportune for improvement
7.1.8	In undeveloped portions of Westpark and Southfort, encourage the use of block shapes that can facilitate changes in future land uses.	Opportune for improvement
8.0 Mobility		
8.14	When undertaking transportation planning, ensure that streets are designed for all users, with adequate facilities for pedestrians, cyclists, and users of public transit, as well as consideration for the effective operation of Public Works and Emergency Services.	Satisfactory
9.0 Housing		
9.1.1	Encourage a range of housing types within all areas of Fort Saskatchewan, with close access to neighbourhood services and amenities.	Satisfactory
9.1.3	Increase the overall density of housing in existing urban areas to reduce the requirement for additional infrastructure servicing and to meet the density targets established in the Capital Region Plan.	Satisfactory
9.2.3	Achieve neighbourhood targets of 20% of new housing units to be developed as affordable or attainable housing.	Opportune for improvement
10.0 Parks & the Natural Environment		
10.1.1	Provide all neighbourhoods with access to passive and active recreational opportunities.	Excels
12.0 Sense of Community		
12.1.1	Promote land use patterns and mobility connections that foster community interactions.	Satisfactory

Community Sustainability Plan – R173-14

UR – Urban Resources		
UR1	Rezone city to allow mixed use development, higher densities and a higher percentage of land (approximately 10-15% more) being dedicated to green spaces.	Satisfactory
UR4	Develop the City around neighbourhood nodes so people can walk to their nearest node for daily needs and amenities.	Opportune for improvement
UR6	Emphasize the pedestrian experience in all urban design ensuring people have places to socialize and connect.	Satisfactory
UR14	Promote all new building development to follow a green building rating system (e.g., LEED®) or develop a municipal green building code to be adhered to by developers.	Opportune for improvement
UR19	Restrict maximum lot size and minimum density to promote higher density.	Satisfactory
UR36	Promote alternate building form and materials that have low environmental impact for building and construction (e.g., straw bale, rammed earth, and sod buildings).	Opportune for improvement
NE – Natural Environment		
NE2	Incorporate the natural environment, greenways and parks in the overall design of the City.	Satisfactory
CC – Compassionate Community and Sense of Community		
CC5	Adapt land use policies to allow for a greater range of housing options including mixed use and high density developments	Satisfactory
CC25	Create a sense of community where people make an effort to know their neighbours.	Satisfactory

Southfort Area Structure Plan – Bylaw C7-13

Designation: Low Density Residential

4.1 Overall		
Prior to acceptance of redistricting applications, an Outline Plan and an Engineering Design Brief for the parcel in its entirety shall be provided by the developer, demonstrating how the proposed development will integrate with adjacent development and align with the objectives and policies of this Area Structure Plan.		Satisfactory
When determined necessary, the City shall host design forums prior to accepting redistricting and subdivision applications to facilitate a round table discussions between various City Departments and the developer. The developer shall demonstrate how the proposed development will achieve the planning and operations goals of the City.		Satisfactory
Neighbourhood character and quality shall be enhanced through public realm improvement such as boulevards, entrance features, street furniture, public art, and other landscape features.		Excels

4.2 Residential	
Provide a variety of lot sizes and housing forms to provide choice and accommodate all residents in the community.	Excels
Urban design shall be used to create a cohesive neighbourhood with a variety of architectural styles that encourages interaction with the public realm by orienting buildings towards public streets and parks.	Excels
The City may consider entering a Pilot Project to grant variances to a group of residential lots in a greenfield development in exchange for innovative neighbourhood design and diverse housing options. Notification of the proposed Pilot Project shall be circulated to adjacent property owners with an opportunity for residents to comment, which will be taken into consideration before entering an agreement.	Excels
4.6 Green Spaces	
Provide neighbourhoods with access to passive and active recreational opportunities.	Excels
6.4 Transportation	
Prohibit front driveway access to arterial and major collector roadways. Front drive access from minor collector roads may be permitted on a limited basis.	Opportunity for improvement
Where possible and considered necessary by the City, dwellings with front access shall face dwellings with rear access to allow one side of the street to be used for off-site parking and snow storage, especially in developments that are primarily semi-detached and multi-attached dwelling.	Satisfactory

9.12 DC(A)-07 DIRECT CONTROL – Residential (Southfort Meadows Stage 5)

9.12.1 Purpose

The general purpose of this Land Use District is to establish site-specific direct control provisions to facilitate a Pilot Project to assess the feasibility of combining side yards. Dwelling units may be located up to one side property line, provided the regulations are met or exceeded. Where the combined side yard is applied, architectural controls shall require the front façade to be designed and constructed to a high architectural quality, providing variety and visual interest within the development that differentiates from adjacent developments. This District is intended to provide innovative development to maximize housing variety while creating a signature streetscape.

9.12.2 Area of Application

Portion of Southeast ¼ Section of 19-54-22-W4M and a portion of Lot 1, Block 1, Plan 022 6974, as shown in **Figure 17**.

9.12.3 DC(A)-07 Uses:

- Accessory development
- Bed and breakfast
- Community garden
- Day care facility (limited)
- Group home (limited)
- Home business
- Home office
- Show home
- Semi-detached dwelling
- Single detached dwelling
- Swimming pool
- Temporary sales centre

9.12.4 DC(A)-07 Site Subdivision Regulations for Single Detached Dwellings

	Interior Site	Corner Site
Site Area	353.6m ² (3,791.0ft ²) minimum	394.4m ² (4,237.0ft ²) minimum
Site Width	10.4m (34.0ft) minimum	11.6m (38.0ft) minimum
Site Depth	34.0m (111.5ft) minimum	

9.12.5 DC(A)-07 Site Subdivision Regulations for Semi-Detached Dwellings

	Interior Site	Corner Site
Site Area	299.2m ² (3,222.4ft ²) minimum	350.3m ² (3,768.7ft ²) minimum
Site Width	8.8m (28.9ft) minimum	10.3m (33.8ft) minimum
Site Depth	34.0m (111.5ft) minimum	

9.12.6 DC(A)-07 Site Development Regulations for Single Detached and Semi-Detached Dwellings

	Interior or Corner Site	
Front Yard Setback	Front	6.0m (19.7ft) minimum 7.0m (23.0ft) maximum
	Flanking	3.0m (9.8ft) minimum 4.5m (14.8ft) maximum
Rear Yard Setback	8.0m (26.2ft) minimum	
Side Yard Setback	1.5m (4.9ft) minimum. Combined side yards may be considered as per Section 9.12.6.	
Principal Building Height	Two and one half (2 ½) storeys not to exceed 10.0m (32.8ft) maximum	
Site Coverage	45% maximum for principal building over one storey, excluding decks	
	50% maximum for principal building of one storey, excluding decks	
	50% maximum for all buildings and structures where principal building is over one storey	
Density	55% maximum for all buildings where principal building is one storey	
	Maximum of one dwelling unit per site	

9.12.7 Combined Side Yard Regulations:

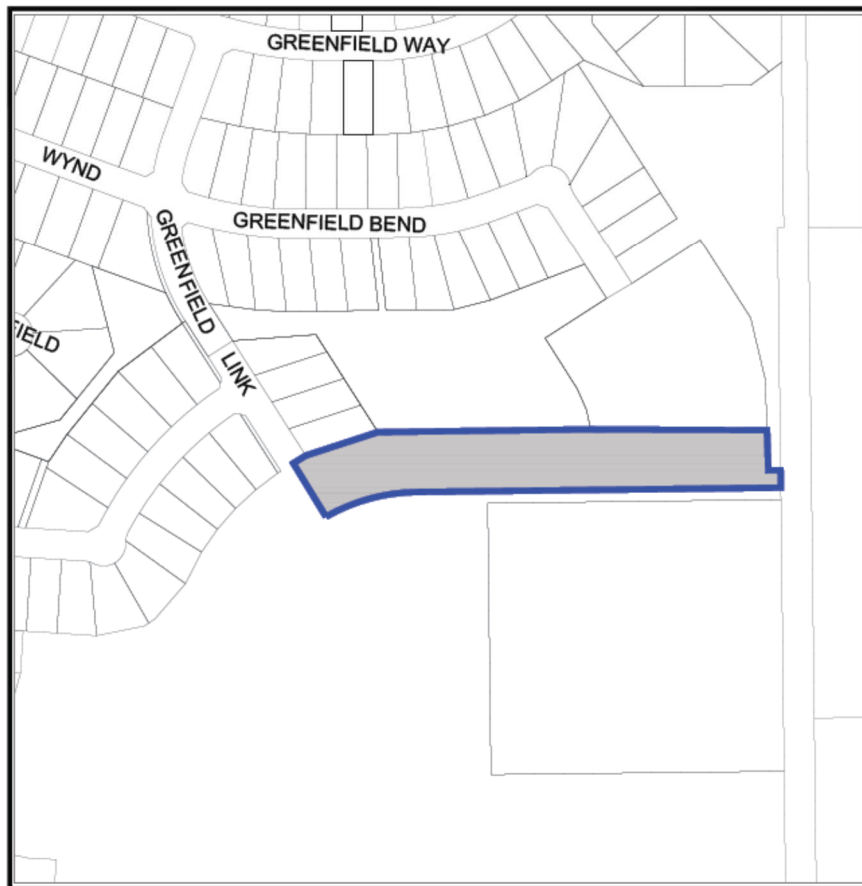
- a) The reduced setback shall only be applied to the block in its entirety or a minimum of eight contiguous single detached lots.
- b) For single detached dwellings, one side yard setback may be reduced to 0.0m (0.0ft) where:
 - i. The other side yard achieves a minimum setback of 2.4m;
 - ii. The 0.0m (0.0ft) setback is located next to the minimum 2.4m (7.9ft) side yard setback of the abutting lot; and
 - iii. A Right-of-Way Easement with a minimum width of 1.5m is registered against Title of the adjacent site that provides for:
 - a. The encroachment of eaves from the subject dwelling into the easement a maximum distance of 0.6m (2.0ft);
 - b. The encroachment of the footing of the subject dwelling into the easement a maximum distance of 0.6m (2.0ft);
 - c. A drainage swale, constructed to the satisfaction of the City of Fort Saskatchewan; and
 - d. Permission to access the easement area for maintenance of both properties.
- c) To translate the savings in land costs facilitated by combined setbacks into improved quality and aesthetics, architectural controls for the exterior of the principal building shall incorporate equivalently priced upgrade requirements. The architectural controls shall be developed collaboratively between the subdivision applicant and the Subdivision Authority. Upgraded elements may include, but are not limited to: façade design, materials, textures, colours, rooflines, windows, doors, and decorative detailing.
- d) All applications for Development Permits within the DC(A)-07 District shall demonstrate façade design and architectural elements in accordance with the approved architectural controls, to the satisfaction of the Development Authority.

9.12.8 Additional Development Regulations for DC(A)-07:

- a) All development and uses within this Land Use District are subject to the applicable provisions of Part 4 – General Regulations for all Land Use Districts, Sections 5.1 to 5.13 of Part 5 – Residential Land Use Districts, Part 11 – Parking and Loading, and Part 12 – Signs.

- b) The Development Authority shall have the ability to vary the DC(A)-07 District regulations in accordance with Section 3.3 of this Bylaw.
- c) Where a lot abuts a lot zoned Residential Estate Lot District, semi-detached dwellings shall be prohibited.
- d) No less than 50% of the side yard shall be landscaped with soft landscaping elements such as trees, shrubs, plants, lawns, and ornamental plantings.

DC(A)-07
S.E. 1/4 SEC. 19-54-22-W.4



SUBJECT AREA

5.21 RC – Comprehensively Planned Residential District

¹³5.21.1 RC Purpose

This District is generally intended to accommodate a range of dwelling forms and flexibility in the design and development of a neighbourhood not exceeding a maximum density of 35 dwelling units per net residential hectare. Development in this District may require a Neighbourhood Design Concept setting out the mix of land uses, lot sizes, housing types, streetscapes, relationships of buildings and architectural form and character.

5.21.2 RC Permitted and Discretionary Uses

RC Permitted Uses:

- Accessory development
- Duplex dwelling
- Home office
- Identification Sign
- Multi-attached dwelling
- Secondary suite dwelling*
- Semi-detached dwelling
- Single detached dwelling

RC Discretionary Uses:

- Bed and breakfast
- Community garden
- Day care facility (limited)
- Group home (limited)
- Home business
- Show home
- Temporary sales centre

*Subject to regulations in Section 4.35 Secondary Suites.

5.21.3 RC Site Subdivision Regulations for Single Detached Dwellings and Duplex Dwellings

	Interior Site	Corner Site
Site Area	309.4m ² (3,330.4ft ²) minimum	342.4m ² (3,685.6ft ²) minimum
Site Width	9.1m (29.9ft) minimum	10.6m (34.8ft) minimum
Site Depth	34.0m (111.5ft) minimum	

¹³ C19-14

5.21.4 RC Site Subdivision Regulations for Semi-Detached Dwellings

	Interior Site	Corner Site
Site Area	238.0m ² (2,561.8ft ²) minimum	271.0m ² (2,917.0ft ²) minimum
Site Width	7.3m (24.0ft) minimum	9.1m (30.0ft) minimum
Site Depth	34.0m (111.5ft) minimum	

5.21.5 RC Site Development Regulations for Single Detached, Duplex and Semi-Detached Dwellings

	Interior or Corner Site	
Front Yard Setback	Front Yard*	3.0m (9.8ft) minimum with a lane
		¹ 4.5m (14.7ft) maximum with a lane
		*Where a semi-detached dwelling has front vehicular access to one unit and rear vehicular access via a lane to the other unit, the front yard setback for the unit with lane access may be increased to a maximum of 7.0m (23.0ft)
	Flanking Yard	6.0m (19.7ft) minimum without a lane
		7.0m (23.0ft) maximum without a lane
Rear Yard Setback	8.0m (26.2ft) minimum	3.0m (9.8ft) minimum on a corner site
		4.5m (14.8ft) maximum on a corner site
Side Yard Setback	² 1.5m (4.9ft) minimum	

¹ C19-14

² C19-14

5.21.5 RC Site Development Regulations for Single Detached, Duplex and Semi-Detached Dwellings

	Interior or Corner Site
Principal Building Height	<p>For single detached and duplex dwellings: Two and one half (2 ½) storeys not to exceed 10.0m (32.8ft) maximum</p> <p>For semi-detached dwellings: Three storeys not to exceed 11.0m (39.4ft) maximum. A maximum differential of one storey shall be allowed between adjacent sites.</p>
Site Coverage	<p>45% maximum for principal building over one storey, excluding decks</p> <p>50% maximum for principal building of one storey, excluding decks</p> <p>52% maximum for all buildings and structures where principal building is over one storey</p> <p>57% maximum for all buildings and structures where principal building is one storey</p>
Density	<p>For single detached dwellings: maximum of one dwelling unit per site, plus one secondary dwelling unit where permitted</p> <p>For semi-detached dwellings: maximum of one dwelling unit per site</p> <p>For duplex dwellings: maximum of two dwelling units per site</p>

5.21.6 RC Site Subdivision Regulations for Multi-Attached Dwellings

	Interior Site	Corner Site
Site Area	187.0m ² (2,011.9ft ²) minimum	271.0m ² (2,917.0ft ²) minimum
Site Width	5.5m (18.0ft) minimum for an Internal unit 7.0m (23.0ft) minimum for an End unit	8.5m (27.9ft) minimum
Site Depth	34.0m (111.5ft) minimum	

5.21.7 RC Site Development Regulations for Multi-Attached Dwellings

	Interior or Corner Site	
Front Yard Setback	Front Yard**	3.0m (9.8ft) minimum with a lane 4.5m (14.8ft) maximum with a lane **Where a multi-attached dwelling has front vehicular access to one or more dwelling units and rear vehicular access via a lane to other units, the front yard setback for units with lane access may be increased to a maximum of 9.0m (29.5ft)
		6.0m (19.7ft) minimum without a lane 7.0m (23.0ft) maximum without a lane
	Flanking Yard	3.0m (9.8ft) minimum on a corner site 4.5m (14.8ft) maximum on a corner site
Rear Yard Setback	8.0m (26.2ft) minimum	
	6.0m (19.6ft) minimum where a garage or carport is attached to the principal building and is accessed from a lane at the rear of the site	

5.21.7 RC Site Development Regulations for Multi-Attached Dwellings

	Interior or Corner Site
Side Yard Setback	¹ 1.5m (4.9ft) minimum
Principal Building Height	Three storeys not to exceed 11.0m (39.4ft) maximum. A maximum differential of one storey shall be allowed between adjacent sites.
Site Coverage	45% maximum for principal building
	52% maximum for all buildings and structures
Density	Maximum of one dwelling unit per site

5.21.8 Additional Development Regulations for RC

- (a) All development and uses within this Land Use District are subject to the applicable provisions of Part 4 – General Regulations for all Land Use Districts, Sections 5.1 to 5.13 of Part 5 – Residential Land Use Districts, Part 11 – Parking and Loading, and Part 11 – Signs; and
- (b) No vehicular access to the street shall be permitted from the front yard when a rear lane is provided.

5.21.9 Site Planning and Design Standards for RC

- (a) Development in this District shall generally follow a Neighbourhood Design Concept for the area which sets out the mix of land uses, housing types, streetscapes, relationships of buildings and architectural form and character; and
- (b) Neighbourhood Design Concepts shall demonstrate compliance with the following principles of urban design, to the satisfaction of the Development Authority:
 - i. Design sites and buildings with people as the primary focus and with setbacks that are generally consistent with existing adjacent development;
 - ii. ²Create animated, active streetscapes with interesting building facades and human-scale building and setbacks, particularly at the street level. Use upper-storey setbacks to minimize the mass and shading of multi-storey buildings and provide additional design features on the first two storeys;

¹ C19-14

² C19-14

- iii. Avoid blank facades and provide articulation or additional design features to break up long solid walls;
- iv. Create appropriate transitions in building scale and height to adjacent areas of lower intensity;
- v. Locate building entrances to connect directly with the public street network and clearly articulate building entrances through design and landscaping features;
- vi. Design sites and buildings that allow for convenient, universal access and have clear signage;
- vii. Promote pedestrian comfort with appropriate landscaping, furniture, weather protection and buffers from vehicular traffic;
- viii. Design for active and alternative transportation by providing convenient access to buildings from transit stops, and including bicycle parking and mid-block pedestrian connections where appropriate;
- ix. Design sites and buildings to facilitate social interaction by including patios, courtyards, plazas and sidewalk amenity space to the extent reasonably feasible, to enliven the public or semi-public realm;
- x. Design sites and buildings for visual interest and maximum use in different seasons and at different times of the day;
- xi. Locate vehicle parking, access and service areas to minimize their impact on pedestrians, surrounding properties and the public realm by:
 - a. Minimizing the width of driveways and curb cuts across sidewalks;
 - b. Providing underground parking where possible and locating surface parking to the side or rear of buildings;
 - c. Locating loading or other service areas to the side or rear of buildings and buffering or screening these areas; and
 - d. Integrating service connections, vents, mechanical rooms and equipment within the architectural treatment of buildings to the maximum extent feasible.
- xii. Ensure that parking areas, service areas and public spaces are well-lit and visible from other locations and clearly signed if they are not visible from the public street.

³5.21.10

a) Notwithstanding the above site development regulations, where a dwelling constructed prior to the adoption of this Bylaw (as amended) has a 1.2m (3.9ft) minimum side yard setback, it shall be considered to be in conformity with the Land Use Bylaw; and

(b) Notwithstanding the above site development regulations, where a dwelling is to be constructed on a site located in a subdivision with an application received and deemed complete prior to the adoption of this Bylaw (as amended), it may be constructed with a 1.2m (3.9ft) side yard.

³ C19-14

5.17 R3 – Small Lot Residential District

5.17.1 R3 Purpose

This District is generally intended to accommodate single detached dwellings and accessory uses on small lots.

5.17.2 R3 Permitted and Discretionary Uses

R3 Permitted Uses:

- Accessory development
- Home office
- Single detached dwelling
- Secondary suite dwelling*
- Swimming pool

R3 Discretionary Uses:

- Bed and breakfast
- Community garden
- Day care facility (limited)
- Group home (limited)
- Home business
- Show home
- Temporary sales centre

*Subject to Regulations in Section 4.35 Secondary Suites.

5.17.3 R3 Site Subdivision Regulations

	Interior Site	Corner Site
Site Area	374.0m ² (4,024.0ft ²) minimum	435.2m ² (4,684.6ft ²) minimum
Site Width	11.6m (38.0ft) minimum	11.8m (42.0ft) minimum
Site Depth	34.0 (111.6ft) minimum	

5.17.4 R3 Site Development Regulations

	Interior Site	Corner Site	
Front Yard Setback	6.0m (19.7ft) minimum 7.0m (23.0ft) maximum	Front	6.0m (19.7ft) minimum 7.0m (23.0ft) maximum
		Flanking	3.0m (9.8ft) minimum 4.5m (14.8ft) maximum
Rear Yard Setback	8.0m(26.2ft) minimum ¹ 6.0m (19.6ft) minimum where a garage or carport is attached to the principal building and is accessed from a lane at the rear of the site		
Side Yard Setback	1.5m (4.9ft) minimum		
Principal Building Height	Two and one half (2 ½) storeys not to exceed 10.0m (32.8ft) maximum		
Site Coverage	45% maximum for principal building over one storey, excluding decks		
	50% maximum for principal building of one storey, excluding decks		
	50% maximum for all buildings and structures where principal building is over one storey 55% maximum for all buildings and structures where principal building is one storey		
Density	Maximum of one dwelling unit per site, plus one secondary suite dwelling where permitted		

5.17.5 Additional Development Regulations for R3:

- (a) All development and uses within this Land Use District are subject to the applicable provisions of Part 4 – General Regulations for all Land Use Districts, Sections 5.1 to 5.13 of Part 5 – Residential Land Use Districts, Part 11 – Parking and Loading, and Part 11 – Signs;

¹ C19-14

- (b) ² Subject to Section 1.3.4, where a dwelling constructed prior to the adoption of this Bylaw has a 1.2m (3.9ft) minimum side yard setback, it shall be considered to be in conformity with the Land Use Bylaw;
- (c) ³ Subject to Section 1.3.4, where a dwelling is to be constructed on a site located in a subdivision with an application received and deemed complete prior to the adoption of this Bylaw, it may be constructed with a 1.2m (3.9ft) side yard; and
- (d) ⁴ Subject to Section 1.3.5, where a dwelling constructed prior to the adoption of this Bylaw exceeds the maximum front yard setback, it shall be considered to be in conformity with the Land Use Bylaw.

² C19-14

³ C19-14

⁴ C19-14

SOUTHFORT

Strata Development Corp.

Scale 1:3000



CITY OF FORT SASKATCHEWAN

Food Gatherers Society – Request for Land Lease

Motion:

That Council direct Administration to investigate the feasibility of leasing the land adjacent to the current Royal Canadian Mounted Police (RCMP) detachment located at 9901 – 90 Street to the Fort Saskatchewan Food Gatherers Society.

Purpose:

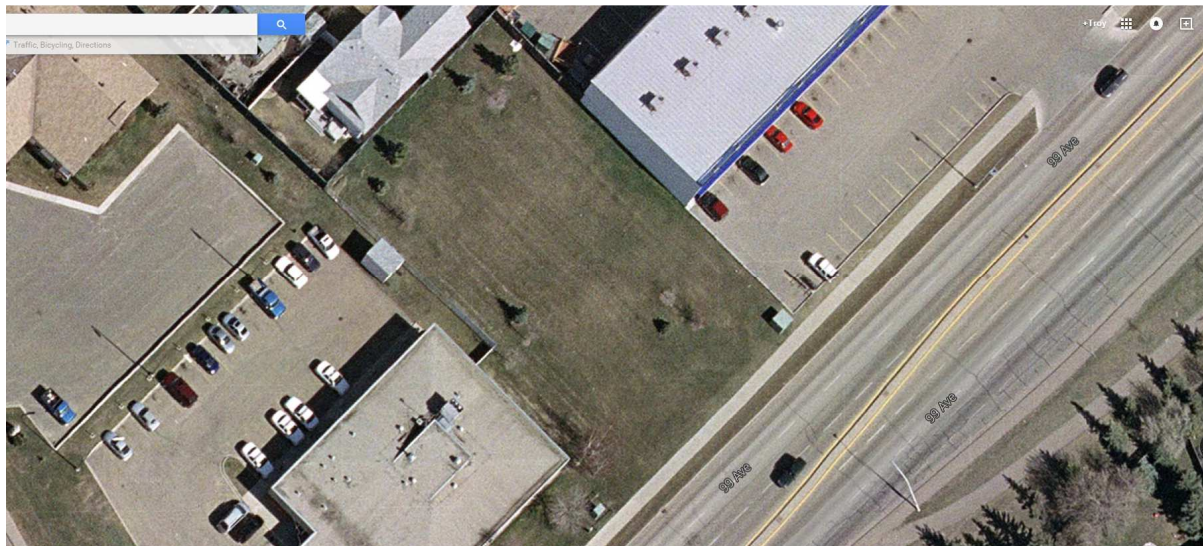
Council's consideration of the request made by the Fort Saskatchewan Food Gatherers Society for a lease on City owned property.

Background:

The Fort Saskatchewan Food Gatherers Society is a volunteer not-for-profit association that distributes food and food related items to people in need in our community. The Society operates a facility which it uses for storage and as a food bank. In addition, food and toy hampers are distributed at Christmas and the Society administers a school snack program to provide food for children who show up to school hungry.

The Society has stated that they are under considerable space restraints and are requesting the City to consider leasing them the city-owned land that sits adjacent to the current RCMP detachment for \$1.00 per year for an unlimited period of time.

Administration recommends that the request from the Food Gatherers Society to lease the subject land from the City of Fort Saskatchewan not be pursued.



Plans/Standards/Legislation:

At the present time, The City does not have a significant amount of strategic land holdings outside of the land dedicated to parks, facilities, operations, and utilities. This hampers the City's ability to undertake innovative developments for the benefit of the community and provides no flexibility to address future operational needs. The City does not have a land management strategy at this time, which is listed as a priority in the Strategic Plan and will likely be addressed in 2016 or 2017.

This request highlights the need for the City to have a land management strategy in place, as there are other not-for-profit groups and community initiatives that also require land. For example, Council directed Administration to seek options for a future Habitat for Humanity project in October 2014.

If Council wishes to have Administration explore this request, several factors would have to be explored:

- Rezoning the land from the current PS (Park Service District) Designation.
- Site servicing requirements.
- Site access and parking requirements.
- Lease arrangements and cost sharing considerations.
- Future land use considerations.

Financial Implications:

This feasibility study would be undertaken administratively. Potential costs may include consultation with technical experts regarding the site logistics. Legal fees may also be incurred regarding leasing options, in addition to a land appraisal consultation.

Alternatives:

1. That Council direct Administration to investigate the feasibility of leasing the land adjacent to the current Royal Canadian Mounted Police (RCMP) detachment located at 9901 – 90 Street to the Fort Saskatchewan Food Gatherers Society.
2. That Council not pursue the request from the Food Gatherers Society to lease the subject land from the City, and advise how they wish to proceed.

Attachments:

Appendix A - February 13, 2015 Letter of Request from the Fort Saskatchewan Food Gatherers Society.

File No.:

Prepared by:	Troy Fleming General Manager, Infrastructure and Community Services	Date: February 24, 2015
Approved/Reviewed by:	Troy Fleming General Manager, Infrastructure and Community Services Acting City Manager	Date: March 3, 2015
Submitted to:	City Council	Date: March 10, 2014



Fort Saskatchewan Food Gatherers Society
Box 3404
Fort Saskatchewan, AB T8L 2T3

February 13, 2015

Legislative Services
City of Fort Saskatchewan
10005 - 102 Street
Fort Saskatchewan, AB T8L 2C5

Dear Mayor & Council:

Please accept this letter for the Fort Saskatchewan Food Gatherers Society to have a delegation at one of the February or March Regular Council meetings.

We wish to make a formal request to ask Council to consider leasing to us the green space land adjacent to the current RCMP detachment for \$1.00/year for an unlimited period of time with permission to build a new home for the Food Bank.

We currently serve approximately 125 plus families per month in Fort Saskatchewan who are unable to provide food for their families, plus provide for the annual Christmas Hamper Campaign. Given the increased volume of families being served by the Food Bank, we have considerable space constraints and require a new home.

We are in the process of developing a building campaign, however we recognize that we cannot afford to buy land and build a building. In reviewing the number of other non-profit groups with similar leases, we believe this request is fair and reasonable and would have no financial impact on the City.

In evaluating the land available, this property would be our first choice given it will be adjacent to Families First new location.

Please contact me at 780-719-3010 to make the necessary arrangements and if necessary to have staff meet with us in advance of the presentation.

Respectfully,

A handwritten signature in cursive script that reads "Marlene Leroux".

Marlene Leroux
Chairperson

CITY OF FORT SASKATCHEWAN

Bylaw C8-15 to Amend Land Use Bylaw C10-13 to Add RHR – High Rise Residential District

Motion:

That Council give first reading to Bylaw C8-15 to amend Land Use Bylaw C10-13 by adding the RHR – High Rise Residential District, which allows residential buildings with a maximum height of 20 storeys.

Purpose:

The purpose of this report is to present Council with information and a recommendation on increasing the residential maximum height, and to consider first reading of Bylaw C8-15.

Background:

The Land Use Bylaw currently allows a range of height maximums depending on the nature of intent of each district. Districts that allow tall buildings include the C5 - Fort Mall Redevelopment District, allowing buildings up to 15 storeys; and the RMH - High Density Multiple Residential District, allowing up to 12 storeys in height. A chart is provided as Appendix "C" showing the maximum allowable height in each districts.

On August 26, 2014, Administration presented Council with Bylaw C19-14 - 2014 Land Use Bylaw Refresh, which proposes minor amendments to address development trends, and provide clarification within the regulations. At that meeting, Administration was directed to bring back information to Council on increasing the height capacity for future buildings high density residential developments in Fort Saskatchewan.

Administration conducted an online survey, and displayed presentation material in public locations such as the Dow Centennial Centre (main lobby), Shell Theatre, Harbour Pool, Jubilee Recreation Centre, and City Hall. The displays and online survey provided the opportunity for the public to give feedback on tall buildings within the City of Fort Saskatchewan. The results of the feedback is summarized in Appendix E.

To assess the potential impacts that tall buildings may have on municipal infrastructure and nearby development, Administration obtained the services of ISL Engineering. An Infrastructure Analysis has been prepared by ISL Engineering, and is available as Appendix "D".

The Infrastructure Analysis focused on three potential locations within the City for high rise development. These three locations include the old hospital site, a site adjacent to Highway 21, and the residential mixed use node, as identified in the Southfort Area Structure Plan. Administration chose to focus on these locations based on size, proximity to transportation and services, and compatibility with nearby land use districts. While the three locations have been identified as potentially suitable for high rise development, the application of the RHR District is not limited nor guaranteed for these sites. The appropriate location would be analyzed and determined through the redistricting process.

To ensure potential impacts associated with tall buildings are minimized, the RHR District ensures reasonable separation between tower locations, and more sensitive land uses. In particular, where the RHR District abuts a district that allows for low density residential, a site would have to be large enough to accommodate a setback proportionate to the height of the high rise. The RHR District also includes regulations to address issues, such as overlooking into sensitive land uses, shadowing, floor area ratios, amenity area, siting of high rise buildings within the site, density, parking, transportation, and so forth.

Should Council approve the proposed RHR District, the new district would allow apartment buildings to be at least 12 storeys in height to a maximum of 20 storeys in height, subject to development regulations. As a new district within the Land Use Bylaw, a landowner would have the ability to make application to redistrict land to RHR - High Rise Residential District. The redistricting application would be vetted by Administration, subject to three readings, and a Public Hearing.

Plans/Standards/Legislation:

Appendix B outlines the related municipal plans for this proposed Land Use Bylaw amendment.

Financial Implications:

Analysis on the financial considerations will be examined and outlined in the subsequent Public Hearing report to Council.

Internal Impacts:

The functions associated with the adoption of this Bylaw can be completed within existing staff capacities. Should Council direct Administration to conduct further analysis, it may be necessary to obtain the services of a consultant.

To meet the *Municipal Government Act* requirements, assistance from Communication and Marketing would be necessary. Should Council grant first reading to Bylaw C8-15, the Public Hearing would be advertised in the local newspaper for two consecutive weeks. The target date for a Public Hearing regarding Bylaw C8-15 is currently scheduled for Tuesday, April 14, 2015 in Council Chambers at 6:00 p.m., or soon thereafter.

Moreover, staff will post the draft district on-line and promote the project through announcements on the website and social media, in order to encourage feedback and comments.

Alternatives:

1. That Council give first reading to Bylaw C8-15 to amend Land Use Bylaw C10-13 by adding the RHR – High Rise Residential District, which allows residential buildings with a maximum height of 20 storeys.
2. That Council not proceed with first reading to Bylaw C8-15, not establishing a land use district with a maximum building height of 20 storeys, and advise how they wish to proceed.

Attachments:

1. Bylaw C8-15
2. Appendix A – Draft RHR Residential High Rise District
3. Appendix B – Relevant Policies
4. Appendix C – Maximum Heights in Relevant Land Use Districts
5. Appendix D – Residential Height Analysis, ISL Engineering
6. Appendix E – Summary Report of Community Feedback

File No.: Bylaw C8-15

Prepared by:	Craig Thomas Senior Development Planner	Date: February 11, 2015
Approved/Reviewed by:	Troy Fleming General Manager, Infrastructure & Community Services & Acting City Manager	Date: March 4, 2015
Submitted to:	City Council	Date: March 10, 2015



CITY OF FORT SASKATCHEWAN

A BYLAW OF THE CITY OF FORT SASKATCHEWAN IN THE PROVINCE OF ALBERTA TO AMEND BYLAW C10-13, LAND USE BYLAW

BYLAW C8-15

WHEREAS the *Municipal Government Act*, R.S.A.,2000, c.M-26 as amended or repealed and replaced from time to time, provides that a municipality has the power to amend the Land Use Bylaw;

NOW THEREFORE, the Council of the City of Fort Saskatchewan, in the Province of Alberta, duly assembled, enacts as follows:

1. This Bylaw is cited as the Amendment to Land Use Bylaw C10-13, as amended, repealed, and/or replaced from time to time.
2. That Schedule “A” of Bylaw C10-13 be amended as follows:
 - a) by adding the following High Rise Residential District:

“5.25 RHR - High Rise Residential District

5.25.1 Purpose

This District is intended to provide for apartment buildings between 12 and 20 storeys and urban design direction to effectively integrate high rise developments with the surrounding planned or existing built form. Preferred sites will be located within close proximity (400.0m or 1312.3ft) to commercial amenities and transit nodes. This zoning is not intended for lands included within the Downtown Area Redevelopment Plan (Bylaw C14-08).

5.25.2 RHR Permitted and Discretionary Uses

Permitted Uses:

- Accessory development
- Apartment dwelling
- Fascia sign
- Home office
- Identification sign
- Projecting sign
- Show suite in an apartment dwelling

Discretionary Uses:

- Assisted living facility
- Business supportive service
- Community garden
- Day care facility
- Eating and drinking establishment (limited)
- Health services
- Professional, financial and office services
- Personal services
- Retail store (convenience)
- Retail store (general)
- Show home
- Temporary sales centre

5.25.3 Site Subdivision Regulations

		Interior or Corner Site
Site Area	Minimum	1,360.0m ² (14,638.9ft ²)

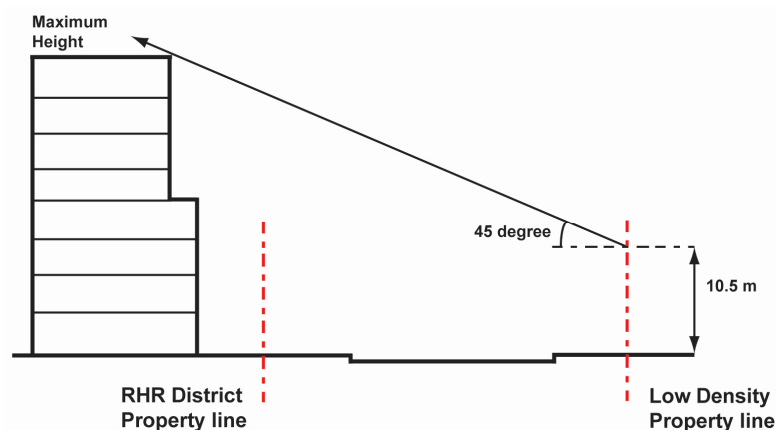
5.25.4 Site Development Regulations

		Interior or Corner Site
Setbacks	Minimum	7.0m (23.0ft)
Unit Density	Maximum	370 dwelling units per net developable hectare
Height	Minimum	12 storeys or 40.0m (131.2ft)
	Maximum	20 storeys or 67.0m (219.0ft), and as per Section 5.13.4(a) of this Bylaw.
Common Amenity Area	Minimum	4.5m ² (48.4ft ²) per dwelling unit.
Private Amenity Area	Minimum	3.0m ² (33.3ft ²) per dwelling unit to be provided by balconies. Balconies may project a maximum of 1.0m (3.3ft) into the minimum setback.

5.25.5 Urban Form, Building Massing and Architectural Character

- (a) The maximum building height shall be limited by the application of a 45° Angular Plane where the RHR District abuts a District that allows low density residential. The 45° Angular Plane, as shown in Figure 5.1, shall be taken from a height of 10.5m (34.4ft) above the nearest property line of the parcel that allows low density residential and subsequent storeys must fit within this angular plane.

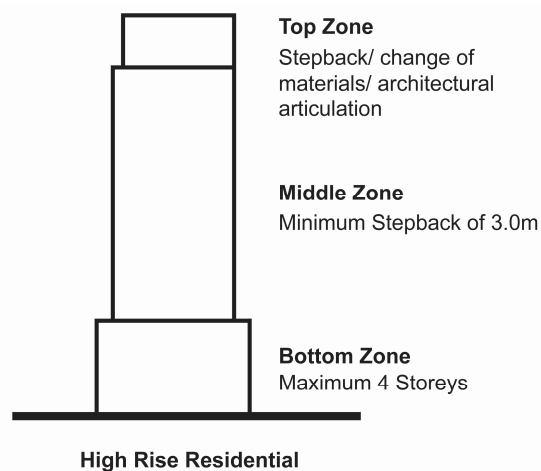
Figure 5.1: 45 Degree Angular Plane for Determining Height Maximums



- (b) Buildings shall provide three distinct vertical zones as per the Figure 5.2, and meet the following step back requirements:

- i. The base zone shall be a minimum of two storeys and a maximum four storeys and shall be integrated with townhouses, apartments or commercial retail units;
- ii. The middle zone shall provide a minimum step-back of 3.0m (9.8ft). To avoid adverse massing effects, the middle zone shall be no wider than 25 metres on any side;
- iii. The top zone shall include the top three stories. The top zone shall provide either an additional stepback or a change in material/colour or special architectural treatment to the satisfaction of the Development Authority.

Figure 5.2: Vertical Zones

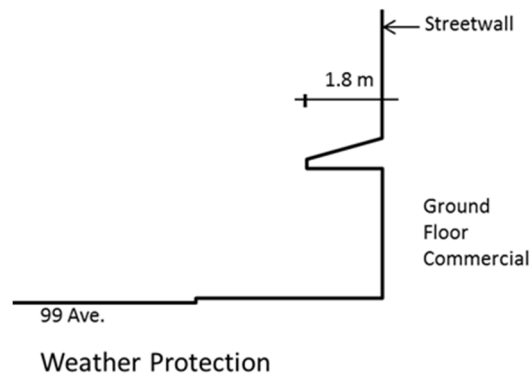


- (c) A minimum separation distance of 25.0m (82.0ft) shall be provided between towers. The 25.0m separation distance shall be measured from the middle zone of each tower located on the property and on any adjacent or abutting properties.
- (d) Architectural treatment of all sides of the building shall create visual interest through the use of architectural features, materials, windows and articulation.
- (e) Buildings shall be finished with glass curtain wall, cement based envelope materials, such as brick veneer, stone veneer, or other manufactured stone veneer, and/or pre-finished metal, and/or painted metal, wood, brick or stone.
- (f) Exterior lighting shall be designed and finished in a manner consistent with the design and finishing of the development, be provided to ensure a well-lit environment and to highlight the development, to the satisfaction of the Development Authority.
- (g) All mechanical equipment shall be visually and acoustically screened from both the public realm and/or adjacent developments or be concealed by incorporating it within the roof envelope or by screening it in a way that is consistent with the character and finishing of the development.

5.25.6 Building Articulation

- (a) The building shall incorporate articulated façades, rooflines, and architectural treatments that establish the building as a distinctive landmark for the surrounding areas.
 - i. The base zone shall incorporate continuous weather protection in the form of a 1.8m (5.9ft) wide canopy or any other architectural element wherever commercial frontages exist to create a comfortable environment for pedestrians, as per Figure 5.3.

Figure 5.3: Canopies and Weather Protection



- (b) The middle and top zone shall be designed to reduce both on and off the site impacts to maintain view corridors, maximize solar penetration, and reduce adverse microclimatic effects related to wind, precipitation and shadowing. Prior to the acceptance of a development permit application, the following studies shall be provided to support this objective:
 - i. A Wind Impact Study prepared by a qualified professional. The Study shall be based on a computer model simulation analysis; and
 - ii. A Sun Shadow Impact Study prepared by a qualified professional.

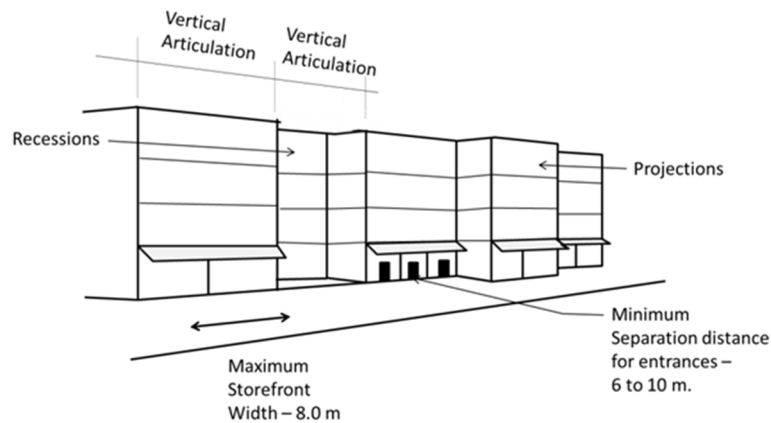
5.25.7 Entrances and Street Character

- (a) All ground level residential units with street frontage shall have individual entrances that front onto the street, adjacent sidewalk, or private outdoor amenity space. Entry transitions, such as steps, fences, gates or hedges, shall be provided to create an appropriate relationship with, and definition of, the public realm and the private space of dwelling units.
- (b) The building shall clearly differentiate residential entrances from commercial entrances through distinct architectural treatment, whereas:
 - i. Entrances for commercial and office uses shall be located at intervals of 6.0m to 10.0m (19.7ft to 32.8ft) along building façades fronting public roadway, as per Figure 5.4; and
 - ii. To ensure the pedestrian amenity areas are maintained, entrances that are adjacent to the public realm shall be recessed at least 1.0 m from the face of

the building.

- (c) Individual retail store frontages at ground floor shall not exceed 8.0m (26.3ft) in width, as per Figure 5.4.
- (d) To avoid monotony in architecture, all buildings shall be required to provide a vertical articulation in the streetwall fronting public roads using a variety of colours, materials, projections as well as recessions in the building façade, as per Figure 5.4;

Figure 5.4: Vertical Articulation Specifications

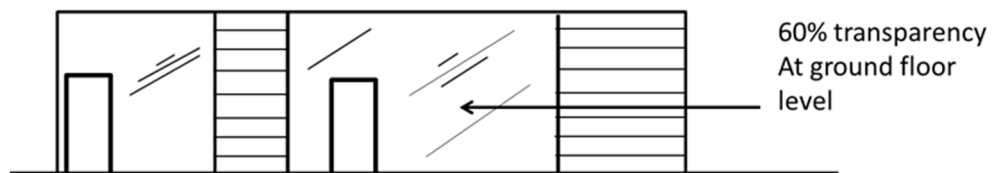


- (e) Common Amenity Areas shall accommodate design features or street related activities, such as architectural elements, landscaping, public art or sidewalk cafes.

5.25.8 Development Regulations for Commercial Uses

- (a) Commercial uses, if developed, shall be limited to the first two storeys of the high rise development.
- (b) Non-residential listed uses shall:
 - i. Not be permitted as a freestanding use in a stand-alone building; and
 - ii. Shall have separate access at grade from residential uses.
- (c) The ground floor of each commercial development shall be required to provide a minimum of 60% transparency measured along the front facade. Tempered or tinted glass that prohibits visibility shall be considered as opaque surface, as per Figure 5.5.

Figure 5.5: Transparency in Ground Level Commercial Developments



5.25.9 Parking, Circulation, Accesses, Loading and Waste Collection

- (a) Notwithstanding Part 11, if a development is located within 200m (656.1ft) of a public transit stop the Development Authority may reduce the required parking by 5%.
- (b) Notwithstanding Part 11, if a development has a car share program the Development Authority may reduce the required parking by 5%, or 4 parking spaces for each car share vehicle in the residential tower, whichever is greater.
- (c) Notwithstanding Part 11, bicycle parking for residents shall be located inside the building in a common area. Bicycle parking for visitors or commercial patrons shall be located adjacent to building entrances.
- (d) Resident parking shall be provided in an above or an underground parkade. Visitor parking may be provided at grade, and shall be located at the rear of buildings and not within a required setback. If an aboveground parkade is located fronting a public roadway, then the following design considerations shall be utilized:
 - i. The ground floor shall include retail uses with multiple entrances;
 - ii. Entrance to the parking facility shall be designed with architectural features to maintain the integrity of retail frontage; and
 - iii. The façade of the upper storeys of the parking facility shall be designed to reflect residential or commercial building character.
- (e) Vehicular access shall be from the flanking roadway or abutting lane. In the event there is no flanking roadway or abutting lane, the vehicular access shall be designed in a manner that has minimal impact on abutting public roadways.
- (f) Driveway ramps shall be at grade at the property line and must not exceed a slope of 6% for a distance of 4.5m (14.7ft) inside the property line.
- (g) Loading, storage and garbage and recycling collection areas shall be located to the rear or sides of the principal building. These areas shall:
 - i. Have a minimum setback of 7.0m (23.0ft) from a public roadway and residential dwellings; and
 - ii. Be incorporated into the overall design theme of the building and screened from public roadways using landscaping or architectural features.

5.25.10 Technical Studies and Assessments

- (a) In addition to Section 3.4.2 of this Bylaw, the Development Authority shall also request that the applicant complete and submit any or all of the following:
 - i. Geotechnical Assessment;
 - ii. Servicing Analysis; and

- iii. Traffic Impact Assessment.

5.25.11 Off-site Improvements

- (a) The Development Authority may condition the permit requiring that the applicant enter into an agreement with the City to do the following improvements necessary to serve the development and address off-site requirements.
 - i. Relocation of all underground and above ground utilities and maintaining required clearances as specified by the utility companies;
 - ii. The construction of on-street fire hydrants;
 - iii. Removal of all existing accesses as necessary to the site, with the restoration of the right-of-way;
 - iv. Provide sidewalk connections from the site to adjacent developments to create a continuous pedestrian environment;
 - v. The improvements to adjacent intersections to facilitate traffic movements into the area, if deemed required by a Traffic Impact Assessment; and
 - vi. Upgrading of adjacent right-of-ways directly abutting the site to appropriate standards; or
 - vii. Any improvements determined to be necessary as identified in Section 3.10.2.

5.25.12 Additional Development Regulations for RHR

- (a) All development and uses within this Land Use District are subject to the applicable provisions of Part 4 - General Regulations for all Land Use Districts, Sections 5.1 to 5.13 of Part 5 - Residential Land Use Districts, Part 11 - Parking and Loading, and Part 12 – Signs.
 - (b) In addition to Part 4 – Section 4.8 and Part 5 – Section 5.10 of this Bylaw, landscaping and site design shall:
 - i. Provide a minimum of one deciduous tree every 10.0m (32.8ft) along the street frontage;
 - ii. Provide two minimum 3.0m (9.8ft) wide walkways through the site (from the front property line to the building and from the visitor parking area to the building) to the satisfaction of the Development Authority.
3. If any portion of this Bylaw is declared invalid by a court of competent jurisdiction, the invalid portion must be severed and the remainder of the Bylaw is deemed valid.

4. This Bylaw becomes effective upon third and final reading.

READ a first time this day of 2015.

READ a second time this _____ day of _____ 2015.

READ a third time and passed this day of 2015.

MAYOR

DIRECTOR, LEGISLATIVE SERVICES

Date Signed: _____

5.25 RHR - High Rise Residential District

5.25.1 Purpose

This District is intended to provide for apartment buildings between 12 and 20 storeys and urban design direction to effectively integrate high rise developments with the surrounding planned or existing built form. Preferred sites will be located within close proximity (400.0m or 1312.3ft) to commercial amenities and transit nodes. This zoning is not intended for lands included within the Downtown Area Redevelopment Plan (Bylaw C14-08) or those subject to an Area Structure Plan.

5.25.2 RHR Permitted and Discretionary Uses

Permitted Uses:

- Accessory development
- Apartment dwelling
- Fascia sign
- Home office
- Identification sign
- Projecting sign
- Show suite in an apartment dwelling

Discretionary Uses:

- Assisted living facility
- Business supportive service
- Community Garden
- Day care facility
- Eating and drinking establishment (limited)
- Health Services
- Professional, Financial and Office
- Personal Services
- Retail store (convenience)
- Retail store (general)
- Show home
- Temporary sales centre

5.25.3 Site Subdivision Regulations

		Interior or Corner Site
Site Area	Minimum	1,360.0m ² (14,638.9ft ²)

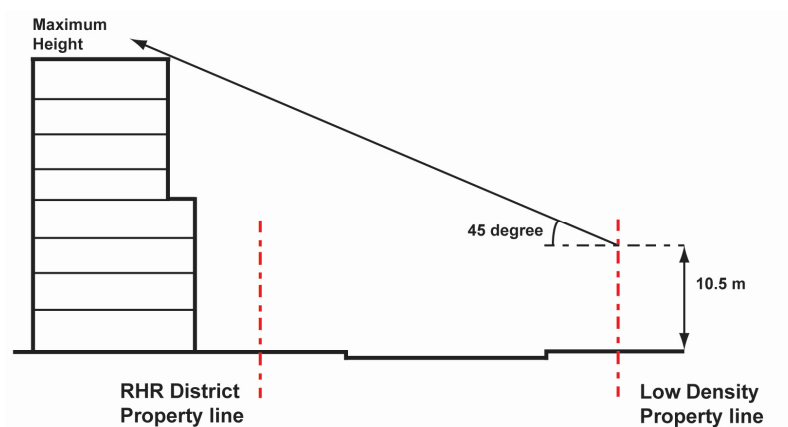
5.25.4 Site Development Regulations

		Interior or Corner Site
Setbacks	Minimum	7.0m (23.0ft)
Unit Density	Maximum	370 dwelling units per net developable hectare
Height	Minimum	12 storeys or 40.0m (131.2ft)
	Maximum	20 storeys or 67.0m (219.0ft), and as per Section 5.13.4(a) of this Bylaw.
Common Amenity Area	Minimum	4.5m ² (48.4ft ²) per dwelling unit.
Private Amenity Area	Minimum	3.0m ² (33.3ft ²) per Dwelling shall be provided for balconies. Balconies may project a maximum of 1.0m (3.3ft) into the minimum setback.

5.25.5 Urban Form, Building Massing and Architectural Character

- (a) The maximum building height shall be limited by the application of a 45° Angular Plane where the RHR District abuts a District that allows low density residential. The 45° Angular Plane, as shown in Figure 5.1, shall be taken from a height of 10.5m (34.4ft) above the nearest property line of the parcel that allows low density residential and subsequent storeys must fit within this angular plane.

Figure 5.1: 45 Degree Angular Plane for Determining Height Maximums



- (b) Buildings shall provide three distinct vertical zones as per the Figure 5.2, and meet the following step back requirements:
- The base zone shall be a minimum of two storeys and a maximum four storeys and shall be integrated with townhouses, apartments or commercial retail units;
 - The middle zone shall provide a minimum step-back of 3.0m (9.8ft). To avoid adverse massing effects, the middle zone shall be no wider than 25 metres on any side;
 - The top zone shall include the top three stories. The top zone shall provide either an additional stepback or a change in material/colour or special architectural treatment to the satisfaction of the Development Authority; and

Figure 5.2: Vertical Zones

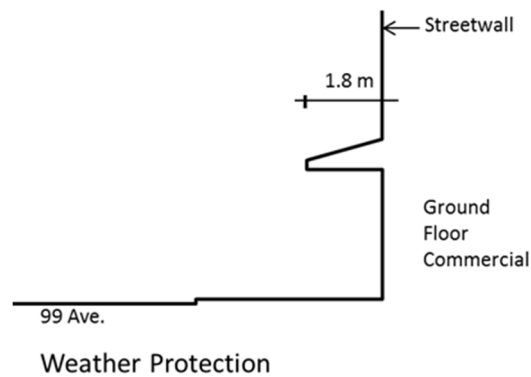


- (c) A minimum separation distance of 25.0m (82.0ft) shall be provided between towers. The 25.0m separation distance shall be measured from the middle zone of each tower located on the property and on any adjacent or abutting properties.
- (d) Architectural treatment of all sides of the building shall create visual interest through the use of architectural features, materials, windows and articulation.
- (e) Buildings shall be finished with glass curtain wall, cement based envelope materials, such as brick veneer, stone veneer, or other manufactured stone veneer, and/or pre-finished metal, and/or painted metal, wood, brick or stone.
- (f) Exterior lighting shall be designed and finished in a manner consistent with the design and finishing of the development, be provided to ensure a well-lit environment and to highlight the development, to the satisfaction of the Development Authority.
- (g) All mechanical equipment shall be visually and acoustically screened from both the public realm and/or adjacent developments or be concealed by incorporating it within the roof envelope or by screening it in a way that is consistent with the character and finishing of the development.

5.25.6 Building Articulation

- (a) The building shall incorporate articulated façades, rooflines, and architectural treatments that establish the building as a distinctive landmark for the surrounding areas.
 - i. The base zone shall incorporate continuous weather protection in the form of a 1.8m (5.9ft) wide canopy or any other architectural element wherever commercial frontages exist to create a comfortable environment for pedestrians, as per Figure 5.3.

Figure 5.3: Canopies and Weather Protection

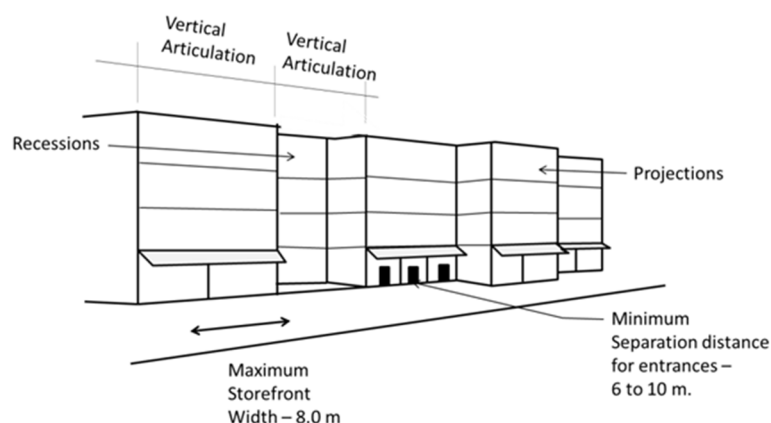


- (b) The middle and top zone shall be designed to reduce both on and off the site impacts to maintain view corridors, maximize solar penetration, and reduce adverse microclimatic effects related to wind, precipitation and shadowing. Prior to the acceptance of a development permit application, the following studies shall be provided to support this objective:
 - i. A Wind Impact Study prepared by a qualified professional. The Study shall be based on a computer model simulation analysis; and
 - ii. A Sun Shadow Impact Study prepared by a qualified professional.

5.25.7 Entrances and Street Character

- (a) All ground level residential units with street frontage shall have individual entrances that front onto the street, adjacent sidewalk, or private outdoor amenity space. Entry transitions, such as steps, fences, gates or hedges, shall be provided to create an appropriate relationship with, and definition of, the public realm and the private space of dwelling units.
- (b) The building shall clearly differentiate residential entrances from commercial entrances through distinct architectural treatment, whereas:
 - i. Entrances for commercial and office uses shall be located at intervals of 6.0m to 10.0m (19.7ft to 32.8ft) along building façades fronting public roadway, as per Figure 5.4; and
 - ii. To ensure the pedestrian amenity areas are maintained, entrances that are adjacent to the public realm shall be recessed at least 1.0 m from the face of the building.
- (c) Individual retail store frontages at ground floor shall not exceed 8.0m (26.3ft) in width, as per Figure 5.4.
- (d) To avoid monotony in architecture, all buildings shall be required to provide a vertical articulation in the streetwall fronting public roads using a variety of colours, materials, projections as well as recessions in the building façade, as per Figure 5.4;

Figure 5.4: Vertical Articulation Specifications

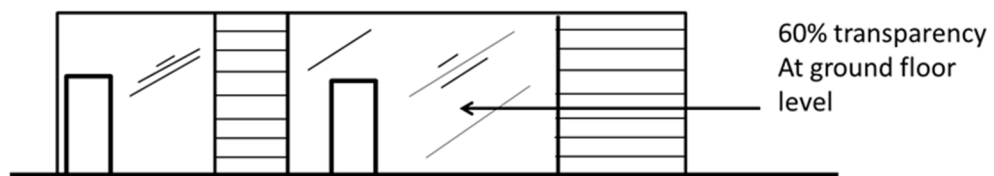


- (e) Common Amenity Area shall accommodate design features or street related activities, such as architectural elements, landscaping, public art or sidewalk cafes.

5.25.8 Development Regulations for Commercial Uses

- (a) Commercial uses, if developed, shall be limited to the first two storeys of the high rise development.
- (b) Non-residential listed uses shall:
 - i. Not be permitted as a freestanding use in a stand-alone building; and
 - ii. Shall have separate access at grade from residential uses.
- (c) The ground floor of each commercial development shall be required to provide a minimum of 60% transparency measured along the front facade. Tempered or tinted glass that prohibits visibility shall be considered as opaque surface, as per Figure 5.5.

Figure 5.5: Transparency in Ground Level Commercial Developments



5.25.9 Parking, Circulation, Accesses, Loading and Waste Collection

- (a) Notwithstanding Part 11, if a development is located within 200m (656.1ft) of a public transit stop the Development Authority may reduce the required parking by 5%.
- (b) Notwithstanding Part 11, if a development has a car share program the Development Authority may reduce the required parking by 5%, or 4 parking spaces for each car share vehicle in the residential tower, whichever is greater.
- (c) Notwithstanding Part 11, bicycle parking for residents shall be located inside the building in a common area. Bicycle parking for visitors or commercial patrons shall be located adjacent to building entrances.

- (d) Resident parking shall be provided in an above or an underground parkade. Visitor parking may be provided at grade, and shall be located at the rear of buildings and not within a required setback. If an aboveground parkade is located fronting a public roadway, then the following design considerations shall be utilized:
 - i. The ground floor shall include retail uses with multiple entrances;
 - ii. Entrance to the parking facility shall be designed with architectural features to maintain the integrity of retail frontage; and
 - iii. The façade of the upper storeys of the parking facility shall be designed to reflect residential or commercial building character.
- (e) Vehicular access shall be from the flanking roadway or abutting lane. In the event there is no flanking roadway or abutting lane, the vehicular access shall be designed in a manner that has minimal impact on abutting public roadways.
- (f) Driveway ramps shall be at grade at the property line and must not exceed a slope of 6% for a distance of 4.5m (14.7ft) inside the property line.
- (g) Loading, storage and garbage and recycling collection areas shall be located to the rear or sides of the principal building. These areas shall:
 - i. Have a minimum setback of 7.0m (23.0ft) from a public roadway and residential dwellings; and
 - ii. Be incorporated into the overall design theme of the building and screened from public roadways using landscaping or architectural features

5.25.10 Technical Studies and Assessments

- (a) In addition to Section 3.4.2 of this Bylaw, the Development Authority shall also request that the applicant complete and submit any or all of the following:
 - i. Geotechnical Assessment;
 - ii. Servicing Analysis; and
 - iii. Traffic Impact Assessment.

5.25.11 Off-site Improvements

- (a) The Development Authority may condition the permit requiring that the applicant enter into an agreement with the City to do the following improvements necessary to serve the development and address off-site requirements.
 - i. Relocation of all underground and above ground utilities and maintaining required clearances as specified by the utility companies;
 - ii. The construction of on-street fire hydrants;
 - iii. Removal of all existing accesses as necessary to the site, with the restoration of the right-of-way;
 - iv. Provide sidewalk connections from the site to adjacent developments to create a continuous pedestrian environment;
 - v. The improvements to adjacent intersections to facilitate traffic movements into the area, if deemed required by a Traffic Impact Assessment; and
 - vi. Upgrading of adjacent right-of-ways directly abutting the site to appropriate standards; or
 - vii. Any improvements determined to be necessary as identified in Section 3.10.2.

5.25.12 Additional Development Regulations for RHR

- (a) All development and uses within this Land Use District are subject to the applicable provisions of Part 4 - General Regulations for all Land Use Districts, Sections 5.1 to 5.13 of Part 5 - Residential Land Use Districts, Part 11 - Parking and Loading, and Part 12 – Signs.
- (b) In addition to Part 4 - Section 4.8 and Part 5 – Section 5.10 of this Bylaw, landscaping and site design shall:
 - i. Provide a minimum of one deciduous tree every 10.0 m (32.8ft) along the street frontage;
 - ii. Provide two minimum 3.0 m (9.8ft) wide walkways through the site (from the front property line to the building and from the visitor parking area to the building) to the satisfaction of the Development Authority.

**Related Policy to Bylaw C8-15
RHR High Rise Residential District**

Municipal Development Plan – Bylaw C16-10

6.0 Developing Community Area	
6.2.1	Reinforce the development of complete neighbourhood units by encouraging a range of dwelling unit types and densities, along with supporting services, in each residential neighbourhood within the City.
6.2.5	Consider proposals for sensitive residential infill redevelopment projects that contribute to the livability of existing neighbourhoods.
6.5.6	Support amendments to the Southfort Area Structure Plan (ASP) to facilitate the development of higher density residential and commercial uses in the location generally identified as a Residential Mixed Use Centre in the MDP Future Land Use Plan (Map 2).
7.0 Community Design	
7.1.1	Encourage the development of the Downtown and Mixed Use Centres as primarily walkable precincts, with special attention given to the public realm and facilities for pedestrians.
7.1.5	Encourage a variety of land uses in the Downtown, Mixed Use Centres, and the General Urban Area, to promote integrated, complete neighbourhoods where residents can carry out most of their day-to-day activities.
9.0 Housing	
9.1.1	Encourage a range of housing types within all areas of Fort Saskatchewan, with close access to neighbourhood services and amenities.
9.1.2	Increase the overall density of housing in existing urban areas to reduce the requirement for additional infrastructure servicing and to meet the density targets established in the Capital Region Plan.
9.1.3	Increase the overall planned densities in the Southfort and Westpark ASP areas to bring these plans into conformance with the density targets for Fort Saskatchewan in the Capital Region Growth Plan.
9.1.4	Support sensitive infill and redevelopment in the Downtown, Residential Mixed Use Centres, General Urban Area, and Core Residential land use districts.
12.0 Sense of Community	
12.1.1	Promote land use patterns and mobility connections that foster community interactions.
13.0 Responsive Local Community	
13.2.1	Continue to encourage redevelopment of the mall and old hospital sites.

Community Sustainability Plan – R173-14

UR – Urban Resources	
UR1	Rezone city to allow mixed use development, higher densities and a higher percentage of land (approximately 10-15% more) being dedicated to green spaces.
UR4	Develop the City around neighbourhood nodes so people can walk to their nearest node for daily needs and amenities.
UR6	Emphasize the pedestrian experience in all urban design ensuring people have places to socialize and connect.
UR19	Restrict maximum lot size and minimum density to promote higher density.
CC – Compassionate Community and Sense of Community	
CC5	Adopt land use policies to allow for a greater range of housing options including mixed use and high density developments
CC25	Create a sense of community where people make an effort to know their neighbours.

Residential

District	Maximum Allowable Height	Notes
R1 – Large Lot Residential	2 ½ Storeys not to exceed 10 m	
R2 – Medium Lot Residential	2 ½ Storeys not to exceed 10 m	
R3 – Small Lot Residential	2 ½ Storeys not to exceed 10 m	
R4 – Lane Lot Residential	2 ½ Storeys not to exceed 10 m	
R5 – Semi-Detached/Duplex Residential	2 ½ Storeys not to exceed 10 m	
RE – Residential Estate Lot	3 Storeys not to exceed 14 m	
RC – Comprehensively Planned Residential	3 Storeys not to exceed 11 m	
RML – Low Density Multiple Residential District	3 Storeys not to exceed 11 m	A maximum differential of one storey shall be allowed between adjacent sites
RMM – Medium Density Multiple Residential District	4 Storeys not to exceed 18.2 m	Buildings over three storeys shall provide appropriate transitions in height, scale, and massing to adjacent sites.
RMH – High Density Multiple Residential District	12 Storeys, not to exceed 40 m	Buildings over three storeys shall provide appropriate transitions in height, scale, and massing to adjacent sites.

Commercial

District	Maximum Allowable Height	Notes
C1 – Neighbourhood Retail and Service District	7.0 metres	
C2 – Vehicle Orientated Retail and Service District	14 metres	
C3 – Commercial Shopping Centre District	14 metres	
C4 – Central Business District	14 metres	
C5 – Fort Mall Redevelopment District	Up to 15 Storeys	

Industrial

District	Maximum Allowable Height	Notes
IL – Light Industrial	15 metres	
IM – Medium Industrial	18 metres	

Direct Control

District	Maximum Allowable Height	Notes
DC(C)-01 Westpark North Site	14 metres	

Suite 100, 7909 – 51 Avenue Edmonton, AB T6E 5L9 T: 780.438.9000 F: 780.438.3700

November 18, 2014

Our Reference: 14243

The City of Fort Saskatchewan
10005 102 Street
Fort Saskatchewan, Alberta T8L 2C5

Attention: Janel Smith
Director, Planning and Development

Dear Madam:

Reference: Residential Building Heights Analysis

1.0 Introduction

In accordance with our October 1, 2014 proposal and further discussion during our October 9 meeting, ISL Engineering and Land Services (ISL) has completed a high level analysis of municipal and transportation engineering constraints, the preparation of a draft residential high rise district, and a graphic to illustrate building heights currently permitted in the City.

The purpose of this letter report is to provide you with the results of our high level municipal and transportation engineering analyses and highlight how those results have informed the draft RHR – Residential High Rise District. The draft District is provided in Appendix A.

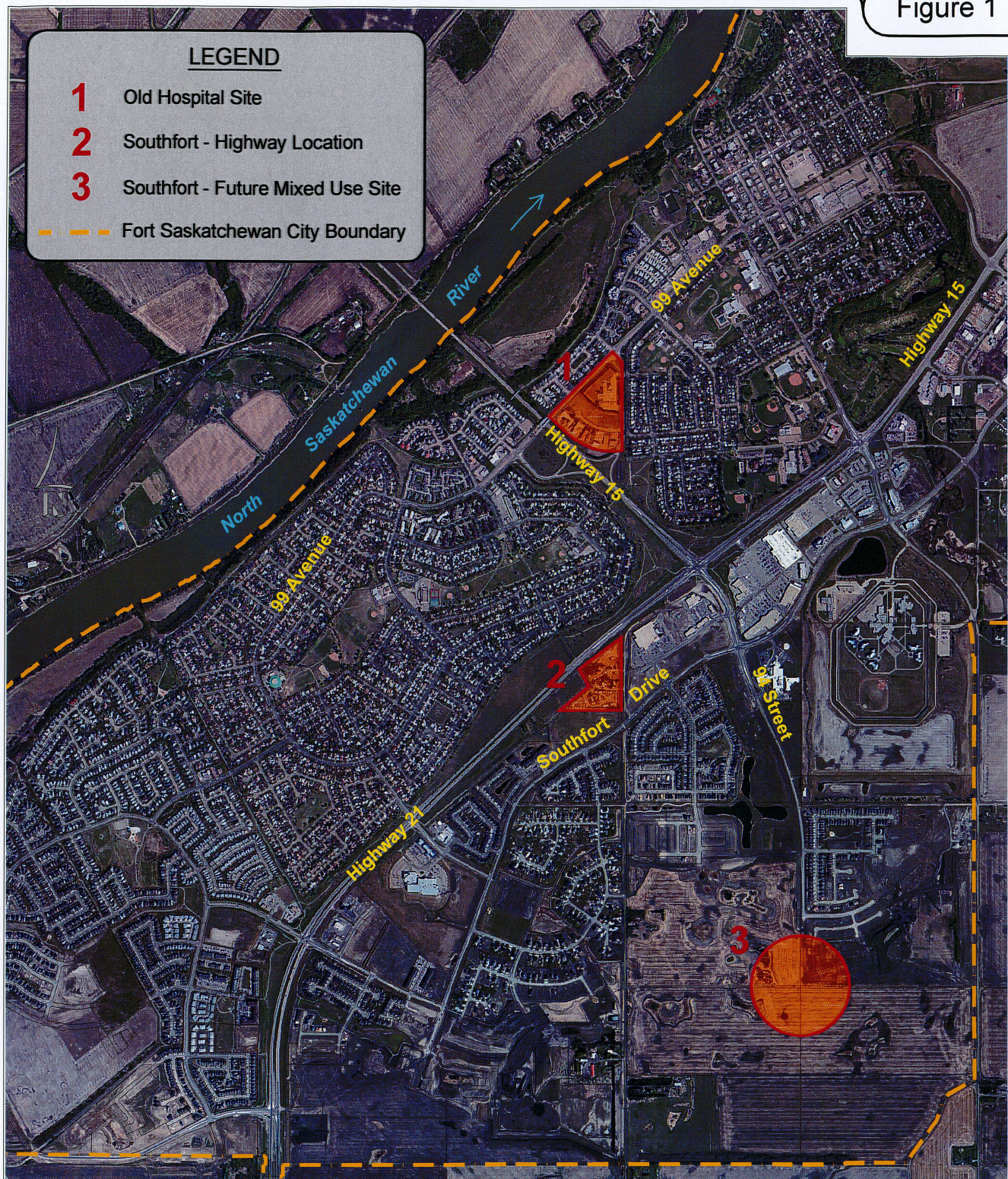
1.1 Locations

At the October 9 meeting, it was agreed that three locations would be examined for the purposes of conducting the high level engineering analysis to investigate whether existing or planned infrastructure can accommodate a high rise tower. These three locations are the old hospital site, a vacant site south of Highway 21 in Southfort, and a site in a planned mixed use node also in Southfort (see Figure 1).

2.0 Municipal Engineering Analysis

Using City documents, ISL conducted a high level analysis of municipal (storm, water, wastewater) and transportation infrastructure to determine if the existing systems could accommodate the development of a high rise residential tower at the three locations. Based on this analysis, there are no immediate or evident constraints to develop a 12-20 storey residential high rise in the three locations. Detailed analyses are required to support any proposed development to confirm the finding described below.

Figure 1

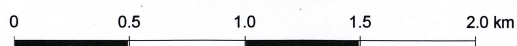


LOCATION MAP

Fort Saskatchewan Building Heights Analysis

City of Fort Saskatchewan

Scale - 1:25,000



November 2014

2.1 Impacts on Stormwater Drainage System

As increasing building heights do not have a direct impact on the percentage of a site that is impervious, and thus the peak and total runoff from a site, the impacts on the City's stormwater drainage system were not assessed. While redevelopment can result in an increase of imperviousness, it can be offset onsite using a combination of Low Impact Development techniques such as green roofs, landscaped rain gardens, etc. It is also feasible to utilize underground stormwater storage facilities to offset any increases in runoff.

2.2 Impacts on Water Distribution System

Increasing building heights can result in increased potable water demand associated with the increased population density. Depending on the specific development provisions (e.g. uses, height, site area, density), higher fire flow demands may be required as compared to other permitted uses. ISL compared the tentative redevelopment site locations with the existing and proposed water distribution network from the 2008 Water Distribution Study to provide a high level assessment of the impacts of allowing higher buildings at these sites. The results are as follows:

- The old hospital site is located adjacent to a 300mm and a 450mm water main and is in close proximity to the main reservoir and pump station. Thus the site appears to have an adequate water distribution network to support the redevelopment of the site to accommodate a 20-storey residential tower.
- The proposed site immediately southwest of Highway 21 and 92 Street is located just north of a 300mm water main along Southfort Drive. This site also appears to have an adequate water distribution network to support the redevelopment of the site to accommodate a 20-storey residential tower. The water distribution system will be further strengthened in the future as development progresses to the south.
- The proposed greenfield development in the southeast part of Southfort is proposed to have a 300mm water main grid based on the 2008 study. As long as this area is developed with a strong water distribution system (as supported by future hydraulic analysis), there are no concerns about the proposed increased building heights.

For each of the above cases, a site specific hydraulic analysis should be carried out at the time of plan amendment (if required to accommodate the development), and if no plan amendment is required, at redistricting; this is reflected in the draft RHR District regulations.

2.3 Impacts on Wastewater Collection System

Increased building heights can result in higher wastewater flows, which can become problematic where the existing wastewater sewers are at capacity. ISL compared the tentative redevelopment site locations with the existing and proposed wastewater collection system from the 2009 Sanitary Sewer Model Input Data Verification to provide a high level assessment of the impacts of allowing higher buildings at these sites. The results are as follows:

- The old hospital site is located adjacent to a 300mm sanitary sewer which connects into larger diameter trunk(s) to the north. Without conducting a hydraulic analysis, it is not possible to determine if this trunk has adequate capacity. However, the presence of the 300mm sewer does offer greater potential for accommodating infill than would a smaller size.

- The proposed site immediately southwest of Highway 21 and 92 Street is located a few hundred metres west of an existing 675mm sanitary trunk along Southfort Drive. This trunk should be able to provide the needed conveyance capacity for the increased building heights, subject to a detailed analysis at the time of development.
- The proposed greenfield development in the southeast part of Southfort will presumably be serviced by the 675mm trunk described above. The increased flows associated with the increased building heights can be expected to be very small as compared to the overall wastewater flows in the area and the available capacity in this trunk, and thus the increased building heights should not be a factor for the wastewater servicing.

For each of the above cases, a site specific hydraulic analysis should be carried out at the time of plan amendment (if required to accommodate the development), and if no plan amendment is required, at redistricting; this is reflected in the draft RHR District regulations.

3.0 Transportation Engineering Analysis

ISL performed a high level review of transportation impacts to identify constraints, if any, at the identified locations for a high rise residential development, and appropriate district regulations for consideration. Specific site impacts would be identified when a proposed development is submitted and the details of the development are confirmed (e.g. number of units, site access).

ISL reviewed the 2009 Roadway and Right of Way Review Report, which outlines existing capacity constraints can be mitigated in the short and long term through improvements.

The 2009 Right of Way Review Report assumed the following population growth in the tentative site redevelopment locations:

1. Old Hospital – no population or employment growth assumed (traffic zone 205)
2. Southwest of Highway 21 and 92 Street – 1618 population increase and 120 employment increase (traffic zone 505)
3. Greenfield development in southwest Southfort - 6335 population increase and 585 employment increase (traffic zones 507, 508, and 517)

If the identified improvements are implemented little or no additional mitigating measures would be required for the highway and greenfield locations in Southfort. As the 2009 Right of Way Review Report did not anticipate any population growth for the old hospital site, it is likely its redevelopment with a high rise residential building would require additional mitigating measures to address traffic capacity constraints. A Traffic Impact Assessment would be necessary to identify such measures. We also reviewed the “Old Health Centre Site Redevelopment Brief”, and found the transportation guidelines appropriate and applicable to the potential high rise residential development.

Although the additional mitigating measures for a residential tower cannot be determined without knowing the specific location, site access and number and type of units, we recommend that the draft district regulations consider the following issues to minimize the overall transportation impact while supporting the higher density development:

1. Allow mixed use – ground floor commercial or office will reduce vehicle trips generated by the site as residents may not need to leave the site to fulfill their needs.

2. Encourage alternative modes of transportation:
 - a. Create walkable neighbourhoods that provide mixed land use and grocery stores within 400m (preferably 200m)
 - b. Require direct sidewalk connectivity from building doors to City sidewalks
 - c. Require sidewalks offsite to connect to other uses
 - d. Require bicycle parking inside the building main floor for residents
 - e. Require bicycle parking near the building doors for visitors – if the building is built near the property line allow the bike parking on the City right of way.
3. Require unbundled parking, where the residential unit and the parking stall are separate purchases. This creates a strong financial incentive to reduce the number of cars owned and attracts persons who can manage their needs with less cars. Unbundling can reduce required parking by 10% to 20%.

Operationally, unbundling is difficult to achieve through Land Use Bylaw district regulations, as it is a matter of tenancy, not use. As a result, we suggest that the City explore how lower parking requirements may be achieved through its processes associated with reviewing and approving applications for condominiums and the within the regulations of the Condominium Act.
4. Encourage connectivity to public transit. If a location is within 200m walking distance of a public transit stop, require a 5% reduction in required parking.
5. Encourage a car share. If the building has a car share reduce required parking by 5% to 10% or by 4 to 8 parking spaces for each car share vehicle in the residential tower.
6. Prescribe parking maximums, not minimums.

4.0 Draft District

The draft RHR - Residential High Rise District is attached to this letter report as Appendix A. The text below highlights key features of the draft district and identifies how its provisions address the findings of the municipal and transportation analyses.

4.1 Development Regulations and Urban Design

The proposed District contains regulations intended to create buildings that present an attractive aesthetic through the use of building materials, site design, and reducing building mass. Below highlights some of the regulations that enable this to be achieved.

1. Although the building height and density is set at a maximum of 20 storeys and 370 units per hectare respectively, height and density will be determined by a combination of factors including:
 - the results of the angular plane calculations,
 - a Wind Impact Study;
 - Sun/Shadow Study;
 - Hydraulic analysis; and
 - Transportation Impact Assessment.

2. To ensure a compatible relationship with similar residential districts, such as the RMM District and RMH District, a building setback of 7.0m (23.0ft) is required. The District also allows for zero front and side building setbacks on sites where pedestrian oriented streets are developed.
3. A maximum podium height of four storeys, landscaping in addition to requirements of the existing Land Use Bylaw, incorporation of continuous weather protection in the form of a 2.0m wide canopy for commercial frontages, and underground parking is required to facilitate a comfortable pedestrian environment;
4. To ensure building articulation and visual interest architectural treatment of all sides of the high rise development is required, and a minimum stepback of 3.0m is required for that portion of a building four storeys, and through façade articulation for those that are greater than 20.0m.

4.2 Municipal Engineering

In order to address potential impacts on stormwater drainage system, the water distribution system, and the waste water collection system the following regulations are included in the draft District:

- The development shall provide green roofs, landscaped rain gardens, or utilize underground stormwater storage facilities to offset runoff; and
- Prior to the approval or issuance of any Area Structure Plan (ASP) or ASP amendment, Land Use Bylaw amendment, or development permit, the owner shall provide a hydraulic analysis to demonstrate sufficient fire flows to the development and that the existing wastewater collection system can accommodate the anticipated wastewater flows.
- If off-site improvements are necessary to accommodate development the owner shall enter into a Development Agreement with the City that shall include an engineering drawing review and approval process. Improvements to be addressed in the Agreement include but are not limited to the relocation and repair of infrastructure, construction of on-street fire hydrants, construction of sidewalk connections to create a continuous pedestrian environment and improvements to adjacent intersections to facilitate traffic movements into the area.

4.3 Transportation Engineering

In order to address potential impacts on the transportation system the following regulations are included in the draft District:

- Prior to the issuance of any development permit for the Old Hospital Site the owner shall be required to provide a Traffic Impact Assessment (TIA) to assess anticipated traffic and mitigate any transportation system impacts.
- The development shall include a minimum of two 3.0m (9.8) wide walkways from the front property line to the building and from the visitor parking area to the building and may require off-site sidewalk connections to ensure connectivity from building doors to City sidewalks and other land uses;
- Resident bicycle parking shall be located inside the main floor of the building.
- If a development is located within 200m (656.1ft) of a public transit stop the Development Officer shall reduce the required parking by 5%.
- If a development has a car share program the Development Officer shall reduce required parking by 10% or 8 parking spaces for each car share vehicle in the high rise development.

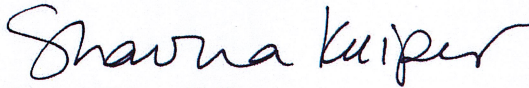
5.0 Graphic Illustration

The illustration of the RHR - Residential High Rise District is provided in Appendix B. The purpose of the graphic is to provide to City Council a visual representation of high rise development in the context of existing development height limits in the City. The illustration represents the development of a 20-storey tower along a typical cross section of 99 Avenue (Old Hospital Site).

6.0 Closing

We look forward to discussing the results of our work with you. We will be contacting you to schedule a time to meet and discuss the results of our work and draft district with you. If you have any questions, please feel free to contact me.

Sincerely,



Shauna Kuiper, M.Pl.
Senior Planner

Encl.



Appendix A

Draft RHR – Residential High Rise District



5.24 RHR – Residential High Rise District

5.24.1 RHR Purpose

This District is intended to provide for high rise housing and urban design direction for the successful integration of high density developments between 12 and 20 storeys, on large sites located outside the downtown and adjacent to arterial or collector roadways, preferably within 400.0m (1312.3ft) of a Residential Mixed Use Centre, or where identified by a statutory plan.

5.24.2 RHR Permitted and Discretionary Uses

RHR Permitted Uses:

- Accessory development
- Apartment dwelling
- Fascia sign
- Home office
- Identification sign
- Park
- Projecting sign
- Show suite in an apartment dwelling

RHR Discretionary Uses:

- Assisted living facility
- Business support service
- Community garden
- Day care facility
- Eating and drinking establishment (limited)
- Health service
- Office service
- Personal service
- Professional, financial and office service
- Public Utility
- Retail store (convenience)
- Retail store (general)
- Show home
- Temporary sales centre

5.24.3 RHR Site Subdivision Regulations for Apartment Dwellings

	Interior or Corner Site
Site Area	1,360.0m ² (14,638.9ft ²) minimum
Site Width	40.0m (131.2ft) minimum
Site Depth	34.0m (111.5ft) minimum

5.24.4 RHR Site Development Regulations for Apartment Dwellings**

	Site
Angular Plane	Where the lot abuts a District containing a Single Detached Dwelling, Semi-detached Dwelling, or Duplex Dwelling as a permitted use, a 45° Angular Plane shall be taken from a height of 10.5m (34.4ft) above the property line and subsequent storeys must fit within this angular plane.
Building Height	<p>a. The minimum Building Height shall be a minimum of 12 storeys or 40.0m (131.2ft) and a maximum of 20 storeys or 67.0m (219.0ft).</p> <p>b. The Building Height shall be determined by:</p> <ul style="list-style-type: none"> • Angular plan requirements; • Wind Impact Study; • Sun/Shadow study; • Hydraulic Analysis; and • Transportation Impact Assessment.
Building Setback	<p>a. A minimum Building Setback of 7.0m (23.0ft) shall be required from all lot lines and shall accommodate design features or street related activities, such as architectural elements, landscaping, public art or sidewalk cafes that contribute to the pedestrian-oriented character of the area.</p> <p>b. Notwithstanding (a.) above there shall be no Setback from the front or side lot lines where adjacent buildings abut the lot line to form a pedestrian-oriented street.</p> <p>c. Where no Setback is provided, development shall be subject to an encroachment agreement, if required.</p> <p>d. No utility servicing equipment shall be located within the front yard of any building.</p>
Podium Height	The maximum height of a podium base shall not exceed four (4) storeys nor 16.0m (52.0ft).
Tower Stepbacks	A minimum stepback of 3.0m (9.8ft), shall be required for that portion of the building exceeding 14.0m (45.9) or four (4) Storeys.
Tower Separation	The minimum distance between a residential tower and any other tower shall be 25.0m (82.0ft).
Density	A maximum of 370 dwelling units per net developable hectare.
Site Coverage	<p>a. 60% maximum for all buildings and structures.</p> <p>b. Notwithstanding "a" site coverage may be increased to 100% where the front or side lot lines of adjacent buildings abut the lot line to form a pedestrian-oriented street.</p>
Minimum Common Amenity Area	A Minimum Common Amenity Area of 4.5m ² (48.4ft ²) per Dwelling shall be provided in compliance with Section 5.14 of this Bylaw.
Minimum Private Amenity Area	A Minimum Private Amenity Area of 3.0m ² (33.3ft ²) per Dwelling shall be provided and achieved through the use of balconies. Balconies may project a maximum of 2.0m (6.56ft) into the minimum Setback, except where there is no Setback required.

Landscaping	<ul style="list-style-type: none"> a. In addition to Part 4 - Section 4.8 and Part 5 – Section 5.10 of this Bylaw, a minimum of one deciduous tree shall be provided (minimum 50.0mm caliper, canopy occurring 2.0m (6.56ft) above finished grade) every 10.0m (32.8ft) along the street frontage, where the site is not built to property lines. b. The detailed Landscape Plan shall include two minimum 3.0m (9.8) wide walkways through the site (from the front property line to the building and from the visitor parking area to the building) to the satisfaction of the Development Officer.
Parking and Loading	<ul style="list-style-type: none"> a. Bicycle parking for residents shall be located inside the main floor of the building. b. Bicycle parking for visitors or commercial patrons shall be located adjacent to building entrances. If the development is built to the property line bike parking may be located on City right-of-way, subject to an encroachment agreement. c. Notwithstanding Part 11, if a development is located within 200m (656.1ft) of a public transit stop the Development Officer shall reduce the required parking by 5%. d. Notwithstanding Part 11, if a development has a car share program the Development Officer shall reduce the required parking by 10%, or 8 parking spaces for each car share vehicle in the residential tower. e. Resident parking shall be provided underground. Visitor parking may be provided at grade. f. Vehicular access shall be from the flanking roadway or abutting lane. In the event there is no flanking roadway or abutting lane, the vehicular access shall be designed in a manner that has minimal impact on abutting public roadways. g. Driveway ramps shall be at grade at the property line and must not exceed a slope of 6% for a distance of 4.5m (14.7ft) inside the property line. h. Adequate sight lines shall be maintained for vehicles entering and exiting the parkade, to the satisfaction of the Development Officer. Mirrors and/or a warning device may be required at the entrances by the Development Officer. i. Loading, storage and garbage collection areas shall be located to the rear or sides of the principal building. j. Service function areas, such as loading docks, truck parking, and utility meters, shall be incorporated into the overall design theme of the building and/or landscape.

**** Internal site setbacks for condominium sites may be reduced and shall be determined at the discretion of the Development Authority.**

5.24.5 Urban Design Regulations and Site Planning

- (a) Architectural treatment of all sides of the building shall create visual interest through the use of architectural features, materials, windows and articulation.
- (b) The tower exterior shall be finished with glass curtain wall, cementitious envelope materials, pre-finished metal, and/or painted metal.
- (c) The building shall clearly differentiate residential entrances from commercial entrances through distinct architectural treatment.
- (d) Exterior lighting shall be designed and finished in a manner consistent with the design and finishing of the development, be provided to ensure a well-lit environment and to highlight the development, to the satisfaction of the Development Officer.
- (e) All mechanical equipment shall be screened from public view or be concealed by incorporating it within the roof envelope or by screening it in a way that is consistent with the character and finishing of the development.

5.24.6 Massing and Building Articulation

- (a) The podium shall incorporate continuous weather protection in the form of a 2.0m (6.56ft) wide canopy or any other architectural element wherever commercial frontages exist to create a comfortable environment for pedestrians.
- (b) The setbacks in the mid-portion of the tower shall be designed to reduce the impact of the upper levels above the podium base portion of the building, to maintain view corridors, maximize solar penetration, and reduce adverse microclimatic effects related to wind and shadowing.
- (c) The tower shall be differentiated from the podium, but should reinforce the design details, materials, and architectural expression from the podium.
- (d) The building shall incorporate articulated façades, rooflines and architectural treatments that establish the building as a distinctive landmark for the surrounding areas.
- (e) The Development shall incorporate design features to minimize adverse microclimatic effects such as wind tunnelling, snow drifting, rain sheeting, shadowing, and loss of sunlight, both on and off-site, consistent with the recommendations of the following studies to the satisfaction of the Development Officer.
 - a. Prior to the issuance of a development permit the submission of a preliminary Wind Impact Statement or a detailed Wind Impact Study, or both shall be required. The Statement or Study shall be prepared by a qualified, registered Professional Engineer, to professional standards. The Study shall be based on a computer model simulation analysis.
 - b. Prior to the issuance of a development permit the submission of a Sun Shadow Impact Study shall be required. The Study shall be prepared by a qualified, registered

Professional Engineer, to professional standards. The development shall minimize shadowing of on-site or adjacent amenity space, and adjacent development.

5.24.7 Entrances

- (a) Building entrances shall be oriented toward a public roadway.
- (b) All ground level residential units with street frontage shall have individual entrances that front onto the street and private outdoor amenity space. Entry transitions, such as steps, fences, gates or hedges, shall be provided to create an appropriate relationship with, and definition of, the public realm and the private space of dwelling units.
- (c) Main building entrances for any Use shall be designed for universal accessibility as per the Barrier Free Design Guide published by the Safety Codes Council (Alberta) 1999, as amended.
- (d) Level changes from the sidewalk to entrances of the building shall be minimized.

5.24.8 Development Regulations for Commercial Uses

- (a) Commercial uses, if developed, shall be developed within the first two (2) storeys of the high rise development;
- (b) Personal service, retail store (convenience) uses, professional, financial and office service uses and eating and drinking establishment (limited) shall not exceed 1,000.0 m² (10,763.9ft²) in gross floor area or 10% of the gross floor area of the high rise development in which they are located, whichever is less. These uses are not permitted as a freestanding use in a stand-alone building, shall only be located in the first two floors of the high rise development, and shall have separate access at grade from residential uses;
- (c) The placement and type of windows shall allow viewing into the building to promote a positive pedestrian-oriented street.
- (d) At least 70% of ground floor commercial façades shall have clear glazing on the exterior; and
- (e) Where the building frontage exceeds 20.0m (65.6ft), the front façade of the building shall be visually broken up with articulation at a minimum of 20.0m (65.6ft) intervals.

5.24.9 Sustainable Practices

- (a) The development may apply LEED techniques to reduce consumption of water, energy, and materials consistent with best practices in sustainable design. Green sustainable targets may include Sustainable Sites, Water Efficiency, Energy and Atmosphere, Materials and Resources, and Indoor Environmental Quality.
- (b) The development may include Low Impact Design techniques to manage stormwater and reduce run off. This may be achieved by providing elements such as green roofs, landscaped rain gardens, or onsite bioretention facilities.

5.24.10 Technical Studies and Assessments

- (a) In addition to Section 2.8.6 of this Bylaw, the Development Authority may also request that the applicant complete and submit any or all of the following:
 - i. Traffic Impact Assessment;
 - ii. Hydraulic Analysis;
 - iii. Wind Study; and
 - iv. Sun/Shadow Study.
- (b) Notwithstanding the above, the owner shall be required to submit a Traffic Impact Assessment for development of the Old Hospital site south of 99 Avenue, east of Highway 15.

5.24.11 Off-site Improvements

- (a) Prior to the issuance of any development permit, the owner shall enter into a Development Agreement with the City for off-site improvements necessary to serve the development. The Agreement shall include an engineering drawing review and approval process.
- (b) Improvements to be addressed in the Agreement shall include any infrastructure improvements identified in any completed Traffic Impact Assessment, Hydraulic Analysis or other study, and are not limited to the following:
 - i. Relocation of all underground and above ground utilities and maintaining required clearances as specified by the utility companies;
 - ii. The construction of on-street fire hydrants, to the satisfaction of the Development Officer;
 - iii. Provision of bicycle parking in accordance with Section 11.2 that is attributed to non-residential uses within the first two (2) storeys, where no front yard setback is provided.
 - iv. Removal of all existing accesses as necessary to the site, with the restoration of the right-of-way to the satisfaction of the Development Officer;
 - v. Repair of any damage to the abutting roadways and sidewalks resulting from construction of the development, to the satisfaction of the Development Officer. The site shall be inspected by the Development Officer prior to the start of construction and once again when construction is complete;
 - vi. Provide sidewalk connections from the site to adjacent developments to create a continuous pedestrian environment;
 - vii. The improvements to adjacent intersections to facilitate traffic movements into the area, if deemed required by a Traffic Impact Assessment or the Development Authority; and
 - viii. Upgrading of adjacent right-of-ways directly abutting the site to appropriate standards.

5.24.12 Additional Development Regulations for RHR

- (a) All development and uses within this Land Use District are subject to the applicable provisions of Part 4 - General Regulations for all Land Use Districts, Sections 5.1 to 5.13 of Part 5 - Residential Land Use Districts, Part 11 - Parking and Loading, and Part 12 - Signs;

- (b) Prior to the issuance of a Development Permit, the Owner shall enter into an Encroachment Agreement for any portion of the development that is to be located on public land to the satisfaction of the Development Officer.

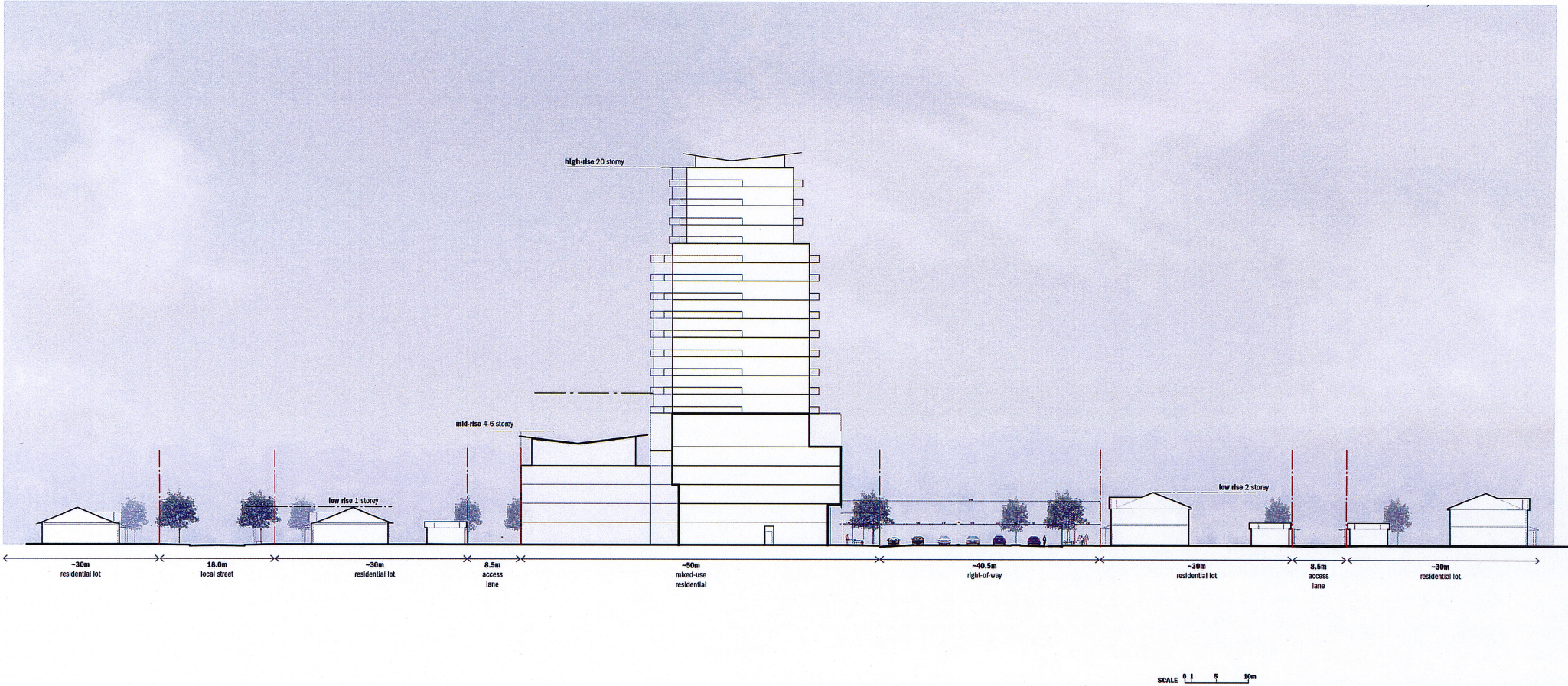


Appendix B

Illustration – Building Heights Analysis



City of Fort Saskatchewan Height Analysis



Summary Report of Public Feedback

Building Height Limit for Apartments in Fort Saskatchewan

Purpose:

This report summarizes the public feedback and responses received regarding the possibility of increasing the maximum allowable height of high-rise apartment buildings in the City of Fort Saskatchewan.

Background:

An important component of potentially increasing the height limit of high-rise apartment buildings was public engagement. This included providing information on the topic, and collecting public feedback.

Planning & Development Services led the public engagement with support from Communications & Marketing. The public engagement and feedback strategy was designed to ensure the public could respond through a variety of methods. This included an online survey and poster/feedback form displays set up at different City facilities.

The public engagement occurred over a three week period beginning on Friday February 6, 2015 and ended on Friday February 27, 2015.

Online Survey:

The *Building Height Limit for Apartments in Fort Saskatchewan Survey* was accessible on the City of Fort Saskatchewan's website during the three week public consultation period. The online survey consisted of two questions (Attachment 1). The first was a multiple choice question asking whether the respondent supported 12, 15, or 20 Storeys. The second question was a text box asking the respondent to type in their opinions on height limits for high-rise apartments in the City.

The online survey was designed so one survey could be submitted per IP address to prevent the survey from being compromised. The respondent did not have to answer all of the questions to submit the survey.

The survey was available on the Public Engagement webpage. During the initial launch, there was a link on the City's main webpage to the survey. Planning & Development Services had an information page on the proposed height increase of high-rise apartments in Fort Saskatchewan that also linked to the survey.

The online survey was promoted through the City of Fort Saskatchewan's main Facebook Page and Twitter account. It was also promoted through Planning & Development Services' Facebook page and Twitter account.

During the three week consultation period, 121 online surveys were submitted.

Poster/Feedback Form Displays:

Another public engagement method also included setting up an information/feedback poster displays at different City facilities (Attachment 2). Three large mounted posters (40" x 32" inches) were created and included scaled graphics and information on the proposed height increase for high-rise apartment buildings. The poster had spaces where respondents could place a sticker if they supported 12, 15, or 20 Storeys.

The poster displays provided feedback forms (Attachment 3). Respondents filled out the forms by hand and submitted them into folders that were collected by staff.

There were three poster/feedback form displays in total that were set-up during the three week consultation period (Attachment 4).

Poster display 1 was set-up in the Shell Theatre lobby located in the Sherritt Cultural Pavilion of the Dow Centennial Centre (DCC) from February 6th to 23rd. The display was then moved to the lobby in City Hall from February 23rd to 27th. A total of 173 stickers were placed on this poster.

Poster display 2 was set-up in the Dow Centennial Centre (DCC) lobby from February 6th to 27th. A total of 325 stickers were placed on this poster.

Poster display 3 was set-up in Harbour Pool's lobby from February 9th to 17th. This display was then moved to the Jubilee Recreation Centre lobby from February 17th to 24th. A total of 171 stickers were placed on this poster.

A total of 18 completed feedback forms were collected from all of the poster displays during the three week engagement period.

Results:

The public feedback results were collected and calculated after Friday February 27th.

Online Survey:

The results from the online surveys were reviewed and totalled. The results of the multiple choice question were calculated to determine the total number in support of 12 Storeys, 15 Storeys, 20 Storeys, and n/a responses. A breakdown of the results are included in this report (Attachment 5).

Online Survey Results	
Number of Storeys	Number of Votes
12 Storeys	65
15 Storeys	17
20 Storeys	28
N/A	11
Total	121

The opinion/comments provided in question two were collected and provided for review (Attachment 5). All of the survey results collected follow the City of Fort Saskatchewan's *Freedom of Information and Protection of Privacy Act* (FOIP) policies.

Poster/ Feedback Form Displays:

The results from the poster/feedback form displays were reviewed and totalled. This included counting the stickers placed on the three poster displays and calculating the total number in support for 12 storeys, 15 storeys, and 20 storeys. A breakdown of the results are included in this report (Attachment 6).

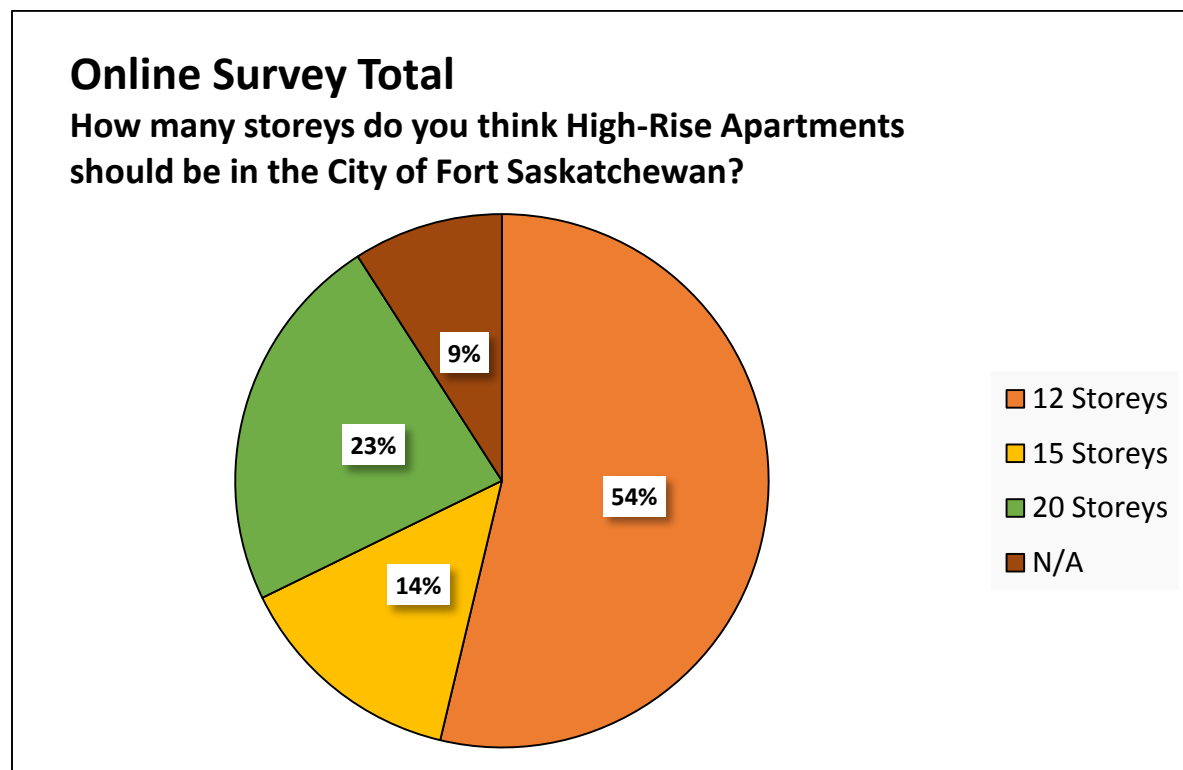
Total Poster Results from all Facility Locations	
Number of Storeys	Number of Votes
12 Storeys	226
15 Storeys	92
20 Storeys	351
Total	669

The feedback forms were collected and the respondent comments transcribed (Attachment 6). All of the feedback results follow the City of Fort Saskatchewan's FOIP policies.

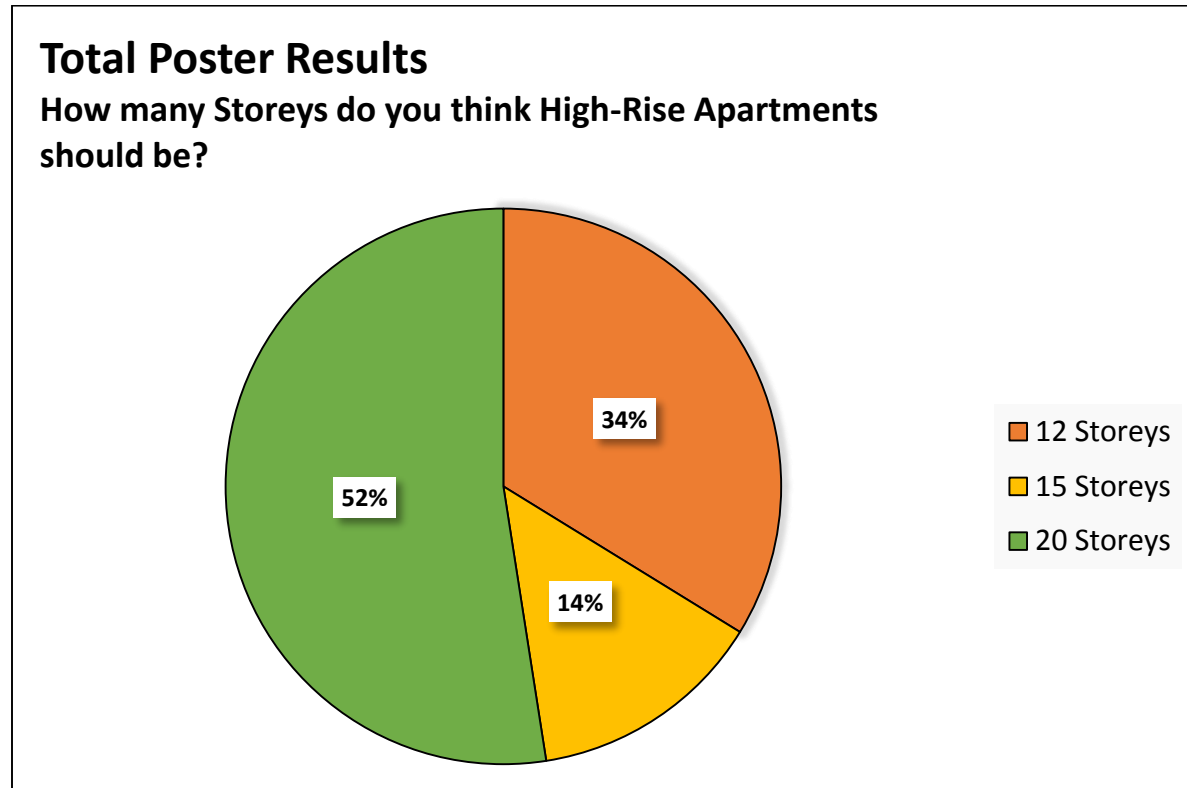
Analysis:

The feedback received shows that the public has various perspectives on the proposed height limits of 12 storeys, 15 storeys, and 20 storeys. Respondents provided a variety of comments for and against different building height limits.

The online survey results show that a majority of respondents (54%) support a height limit of 12 storeys for apartment buildings. This is followed by 20 Storeys (23%), 15 storeys (14%), and no answer/none of the above (9%).



The poster/feedback form display results show that a majority of respondents (52%) support a height limit of 20 storeys for apartment buildings. This is followed by 12 Storeys (34%), and 15 storeys (14%), and no answer (9%).



A number of themes emerged from the public opinion and feedback. Some of the reoccurring themes are listed below for each building height.

12 Storey Height Limit:

- Supporters of 12 storeys want to retain the existing “small town” feel of Fort Saskatchewan.
- Effects of high-rise apartments on the city’s skyline.
- Concerns on existing infrastructure (roads, sewers, etc.) and whether it can accommodate high-rise apartment buildings.
- Concerns on traffic congestion and increased number of cars generated from high-rise apartment buildings.

15 Storey Height Limit:

- Respondents feel this is an appropriate compromise between a 12 storey and 20 storey height limit.
- Accommodating new growth while retaining the “small town” feel of Fort Saskatchewan.

20 Storey Height Limit:

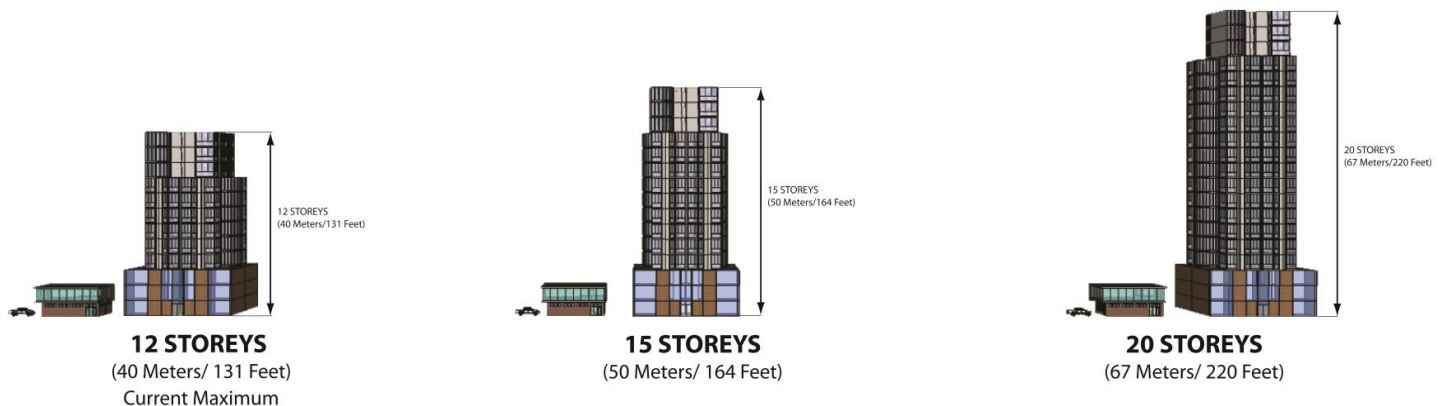
- Respondents feel this provides an opportunity for accommodating new growth in the City.
- Provide new and affordable housing units for residents.
- An alternative to the shortage of greenfield lands for new developments.
- The economic benefits of “building up not out” to accommodate growth at higher densities.

The analysis generated from the collected public feedback shows that the public has a variety of views and opinions on the potential increase of height limits for high-rise apartment buildings.

Height Limit for Apartment Buildings outside of the Downtown

The City of Fort Saskatchewan is considering whether the maximum height limit for high-rise apartments should be increased. Currently, apartments can be 12 storeys in designated areas outside of the Downtown. We are looking at whether the maximum building height outside of the Downtown should be increased.

How many storeys do you think High-Rise Apartments should be in the City of Fort Saskatchewan?



12 Storeys (40 Meters/131 Feet)- Current height limit
15 Storeys (50 Meters/164 Feet)
20 Storeys (67 Meters/220 Feet)

We want to hear your opinions on the height limit of apartment buildings in the City of Fort Saskatchewan. Click the link below to access the survey

[SURVEY- BUILDING HEIGHT LIMIT FOR APARTMENTS IN FORT SASKATCHEWAN](#)

Thank you for your responses and valuable feedback. Some residents have asked why less than 12 storeys was not an option. Currently, the height limit outside of the downtown is 12 storeys. This current height has been in place since 2008. At this time, we are not considering decreasing the height limit. Thank you again, and please continue to provide your input through the discussion box.

You can also fill out the Feedback Form below and submit it to landuseplanning@fortsask.ca or fax it to 780-992-6180. Forms can be submitted in person to Planning & Development Services, 2nd Floor of City Hall - 10005 102 Street.

[Feedback Form Apartment Building Height Limit](#)

[Apartment Building Height Comparison poster](#)

Schedule I- Building Height Limit for Apartments in Fort Saskatchewan Survey

Review Form & Survey x

www.foortsask.ca/Admin/Components/Form/Form/Review/?ID=33&BackUrl=%2fAdmin%2fComponents%2fForm%2fForm%2findex

visionLive™
Fort Saskatchewan

Site Preview My Dashboard Siddons, Matthew (3) Log Out

VISION
INTERNET

My Tasks Site Content Help

Review Form & Survey

SHOW all options BACK

Building Height Limit for Apartments in Fort Saskatchewan

Start Date: February 06, 2015

Page 1

The City of Fort Saskatchewan is considering whether the maximum height limit for high-rise apartments should be increased. Currently, apartments can be 12 storeys in designated areas outside of the downtown. We are looking at whether the maximum building height outside of the Downtown should be increased.

1. How many storeys do you think High-Rise Apartments should be in the City of Fort Saskatchewan?

☐ 12 Storeys (40 Meters/131 Feet)
☐ 15 Storeys (50 Meters/164 Feet)
☐ 20 Storeys (67 Meters/220 Feet)

2. We want to hear your opinions on the height limit of apartment buildings in the City of Fort Saskatchewan.

Please provide your comments in the box below

Page 1 / 1

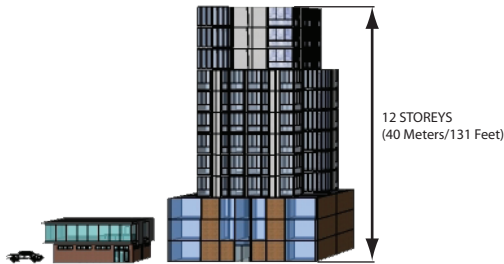
© Copyright 2003-2015, Vision Technology Solutions, LLC. All rights reserved.

CITY OF FORT SASKATCHEWAN

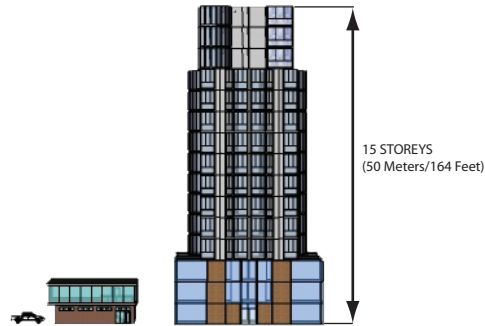
HOW HIGH SHOULD WE GO?

Attachment 2

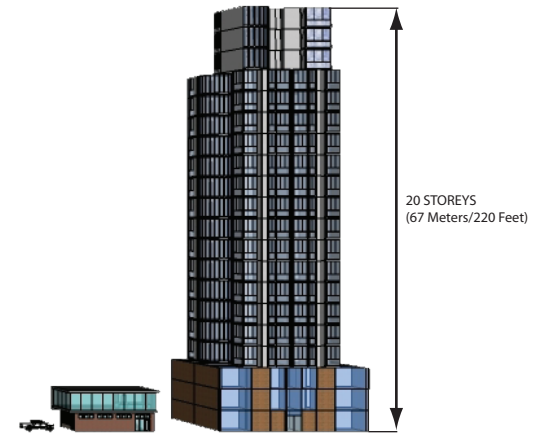
How many Storeys do you think High-Rise Apartments should be?



12 STOREYS
(40 Meters/ 131 Feet)
Current Maximum



15 STOREYS
(50 Meters/ 164 Feet)



20 STOREYS
(67 Meters/ 220 Feet)

Apartment building heights outside the Downtown

The City is reviewing height limits for high-rise apartments. Currently, apartments can be 12 storeys in designated areas outside of the Downtown. To support growth and sustainability, we are looking at whether the maximum building height outside the Downtown should be increased.

Final locations will be determined through the rezoning process. All rezonings include a Public Hearing, which provides residents with the chance to voice their opinions to Council.

We want to hear your opinion!

We are interested in hearing your opinion on apartment building heights. Please **place a sticker in the boxes** above to show which **height you support**. Please include your comments on the feedback forms provided. Visit fortsask.ca for more information, or to submit your comments online.



CITY OF
FORT SASKATCHEWAN

Planning & Development Services

Location: 2nd Floor of City Hall,
10005 – 102 Street Fort Saskatchewan
Phone: 780-992-6198

Fax: 780-992-6198

Website: www.fortsask.ca

Email: landuseplanning@fortsask.ca

Facebook: facebook.com/fortsaskplanning

Twitter: @fortplanning



BUILDING HEIGHT LIMIT FEEDBACK FORM

The City of Fort Saskatchewan is considering whether the maximum height limit for high-rise apartments outside of the Downtown should be increased. We want to hear your opinions on the height limit of apartment buildings in the city.

Name: _____

Date: _____

Neighbourhood of Residence: _____

Building Heights you Support: ____ 12 Storeys ____ 15 Storeys ____ 20 Storeys

Comments:

Forms can be placed in the envelope provided, emailed to landuseplanning@fortsask.ca, or faxed to 780.992.6198. For more information go to fortsask.ca. Personal info on these forms will not be shared.

Planning & Development Phone: 780.992.6198 E-mail: landuseplanning@fortsask.ca

engaged people, thriving community

Attachment 4- Poster/Feedback Form Display

Attachment 5- Public Feedback Online Survey Results

Building Height Limit for Apartments in Fort Saskatchewan Survey Results

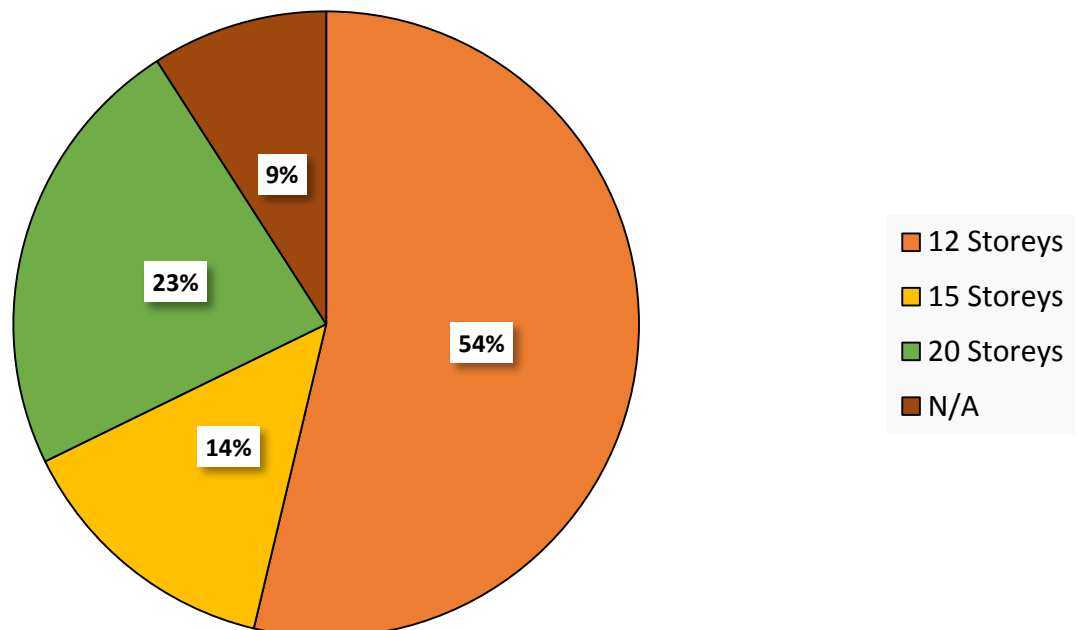
1. How many storeys do you think High-Rise Apartments should be in the City of Fort Saskatchewan?

- 12 Storeys (40 Meters/131 Feet)
- 15 Storeys (50 Meters/164 Feet)
- 20 Storeys (67 Meters/220 Feet)

Online Survey Results	
Number of Storeys	Number of Votes
12 Storeys	65
15 Storeys	17
20 Storeys	28
N/A	11
Total	121

Online Survey Total

How many storeys do you think High-Rise Apartments should be in the City of Fort Saskatchewan?



2. We want to hear your opinions on the height limit of apartment buildings in the City of Fort Saskatchewan.

# Storeys	Opinions/Comments
N/A	Considering the existing building landscape of Fort Sask, constructing an apartment building more than ~ 8 stories is a bad idea. More than 12 stories?...a terrible idea. Where could something like that be built without having an existing neighbourhood live in the shadows? Plus, it would look ridiculous to have a 12+ story building in our low-rise city. Perhaps in the future (decades from now), 12+ stories might make sense, but should be located in future, annexed City land. If you are currently looking at the parcel of land that is zoned high density on Town Crest Road, and considering 12+ stories for that, I think that is a mistake. It would look terrible, and would be a running joke of the Capital Region.
N/A	I believe even 12 stories is too high in our community. Nothing is even close to that height currently. To jump to that height (or higher) would look ridiculous.
12	12 Storeys would be a big step in bringing higher densities to downtown Fort Saskatchewan, I am not sure how going higher would be a benefit. Would underground parking be proposed?
12	I feel when they become too high, it takes away from the small city feel.
20	I think MORE information is needed. I think it depends on where it is being built, whether in middle of mainly residential, or something closer to the new downtown area, mall, commercial area? All depends on where.
12	Considering FSFD only has a ladder truck that extends to 100 Ft , I wouldn't suggest going to much that 12 stories until such time that it is in the Capital budget to replace that unit with a bigger one.
20	I think Fort Saskatchewan is in need of more rental places given the transient workers who come in and out of the town. This will open up more hotel rooms and rooms for rent in houses and even apartment rentals for long term residents. There is however, always a downside. The crime has been growing in the town exponentially since the town has grown. Many friends and family of mine have had their vehicles broken into or vandalized, and my boyfriend even had his home broken into just this week. If we grow the rental options in the town, then we need to seriously think about growing the police force and monitoring before we expand.
20	I think go as high as you want. More room for people to live without taking up valuable land.
12	I think the height restriction should stay the same and not be increased. Part of what makes Fort Saskatchewan have the 'small town' warm, homey' feeling is the absence of high rises which I think is great, and necessary. I don't see any need to build buildings upwards of 15 stories tall here in the Fort.
20	Why should there be sky high limits the Fort is growing we either grow together or fold together
12	Do not increase this limit in order to market the hospital land. Let us try to have some small town left in us, we do not need to continue to bend and break the rules for builders. We have the mindset that this is the only way they will come here/build here but it is not the case and we should not be the

	community that is always out for business, make this a great city to live because it is listening to citizens and maintaining small town charm
12	The lower the better. We don't need our beautiful city to start looking gross with massive buildings. Can our fire department handle a 20 story fire?
15	There is no real restrictions like a nearby airport. But a 15 or 20 storey would look very out of place. And if not done right, styling and color wise, you could have a 20 storey eye sore in a few years.
12	It all comes down to parking and traffic flow. The more people in one area the more problems with traffic congestion.
N/A	Even 12 storeys is too tall
12	I think 12 stories in more than tall enough.
12	too many cheap looking apartments are being built in Fort Saskatchewan
12	I think apartments are terrible for our community. They create congestion, are an eye sore and if low rent can attract crime etc.
20	No limits. Good for the environment, good for lower cost units.
12	I don't even agree with the 12 stories I think anything over 5 is excessive. This takes away from the visual appeal of the city this size. Ridiculous that this is even being considered
12	12 stories does not fit the small town feel we have in the Fort. Increasing height is not an option most want in the Fort. The only one to being it will be the developers. Do not let them keep over riding or hood winking council. Repeat no increase in height. In fact the max should be 6 stories!
20	I feel that allowing taller buildings only encourages more housing to be available which would encourage affordable living, which as a university starting to realise that staying in Fort Saskatchewan is unrealistic, sounds great
15	I'm curious why this is being pursued? Do we currently have any buildings that are even 12 stories? I think if there's a call for taller apartments, then we should make allowances for that, within reason. However, having said that, I would hope they control the location more closely so we don't ever have a repeat of the tall-ish condo right in the back yard of single story homes like the development along the river.
20	Height shouldn't matter. The more people the better in our city
15	Tall apartments are fine as long as the firemen have ladders high enough to save people if they start on fire.
15	At this time, 12-15 stories would tower over everything else. Start at that and see how it goes.
15	Don't get too carried away but another 3 stories aren't going to hurt anyone.
12	I thought we had a 4 Storey Maximum height in Fort Sask Your question only goes the smallest at 12 storeys I plan on fighting this apartment building the whole way - 4 stories max!
12	I do not think we should be considering high rise buildings as an option. This is a family community and is not the place for cheap high rise buildings so a developer can cash in on low income priced units.
20	Build them tall skyscrapers in the fort! Be sweet!
N/A	None of the above. Apartment buildings should not be any higher than 6 stories in this community. Especially if they are built right next to an existing apartment building.
12	I love living in the fort because it's so different from Edmonton. Edmonton is too crowded. I feel high rises would affect the beauty of our city.

15	It should be dependent on what the fire dept. can accommodate. If they can successfully attend and attack a 20 story bldg., then that would be fine.
12	Tall high rises take away from the small town feel. I moved here to get away from the big city feel of large overpowering buildings! If I wanted big lurking buildings I would move to a big ugly City!
20	It would be good for a fast growing city such as ourselves to have tall apartments downtown, for that will bring more residents downtown, and more development.
12	12 storeys outside of the downtown core is enough. Apartment buildings reduce direct sunlight and may reduce privacy in yards.
N/A	You have asked the wrong question. It should have been do you think high rises should be allowed in Fort Saskatchewan
15	It really doesn't matter to me. 12 or 15 stories is fine as long as it is out of the downtown core.
N/A	Without a full time fire dept. to ensure better response times then a volunteer service can offer a high rise of these heights would be crazy. That's a huge task to evacuate and do search and rescue in something of those sizes! It's not fair to the fire fighters of this city to put them in that situation
20	Time for us to grow UP
N/A	I think fort Saskatchewan should still be considered a town and I don't agree that the apartments should be built over 6 stories. I feel that if we had high rise apartments we would lose our small town charm.
12	I'm not clear as to why this is currently an issue considering our highest building is only 5-6 storeys. Anything over 12 would look incredibly out of place. However, if there is interest I don't see a real reason why not to build higher (provided that ER services are upgraded to handle it).
12	It's a difficult choice between effective land use and maintaining the small city atmosphere that makes the Fort so appealing. I find high towers in Edmonton and Calgary block out sun and are sterile buildings without much architectural merit.
12	12 storeys is a great starting point. We can always make them taller in the future.
N/A	High rise apartments should not exist in residential areas with houses. We now have to deal with an apartment building looking right down into our backyard. There goes our privacy.
12	We shouldn't start too high. It would look out of place here. Start with 12 stories and we can get higher later
20	Let them grow tall! Help build our city and allow it to grow!
20	I personally think having the tallest set at 20 stories high would give ample room for growth, and more opportunity for housing/new business opportunities. Seeing high-rises in Fort Saskatchewan would be an amazing thing to see, as to how far our community has grown within the 21 years I've been in the city.
12	I don't know anything about the technicalities of apartments, however, as a member of Fort Sask I like the small town feel. Seeing even a 12 story building makes me feel like a "big city" kind of look. I don't want to see 12 story buildings either. 5-6 even seems high. I like driving into the Fort and still saying to my kids - "look there is the water tower - home is close" and they are 11 and 12 now. I am proud to say where we live - the tallest building

	is the water tower even though the Integra building is tall also. I took the kids there for a pop so they could see above the trees and the city we live in.
20	If the occupancy can be filled, they should be allowed to go as high as safety services can realistically protect them should fire or some other disaster take place. High occupancy is the best way to ensure public transit succeeds
12	Actually I think high rises belong in the downtown. Though I do consider downtown to include areas somewhat beyond the official area. My home is across from Co-op, but not considered downtown, for example. Obviously it is downtown in fact. I would go for 15 or even 20 stories downtown, in the old mall area, for example.
15	Might help city centre look like the centre of community.
12	The reason why I moved to Fort Saskatchewan was for how unique it was. Not like any other typical City. No huge high rise buildings towering in the beautiful downtown core. No huge ugly high rise buildings in the pristine neighbourhoods. I wish your survey would have included no high rises. The 5 story buildings we have now are high enough. Don't take how unique and beautiful Fort Saskatchewan really is away. It would be an awful shame.
12	They block the sun, I think 12 is good unless there is a very good argument for more???
12	Please not too high! This is a small city, and I think it will dwarf us. I also think it's unnecessary.
12	Low-rise apartments rather than high-rises give our city a "small town" charm. High-rise apartments would ruin the family-friendly feel of the Fort and make it just like any other city. High-rise apartments would definitely detract from our beautiful city and ruin our sky-line.
12	Tower Idea for Hospital Site - I would like to know whose idea this was. Does City Council not understand what this community needs? It does not need any kind of high rise condos - 12, 15 or 20 storey. We need affordable housing for seniors and perhaps another Dr. Turner Lodge type of facility. That whole hospital area needs to be reserved for a senior's development. These are the folks that made this City was it is today. If any of you think it is because of Mayor and Council (past, present or future) you are sadly mistaken. Without our seniors this City would be less than nothing. They deserve our support and respect. The people of this City make it what it is. If you want a Legacy - let it be that you were proud of our seniors and want to make sure that they are taken care of at a time in their life when they need it most. To sell that property to a developer to build a 10 or 12 story condo in the midst of a well-developed 'small town' area of this city is laughable at best. When is Mayor and Council going to start putting the needs of this Community above their need for a LEGACY. Having a large condo building in that area of town WILL NOT bring people to the downtown area - businesses are still building on the other side of the highway. There is not proper infrastructure in that area. The only road into and out of that area is congested during rush hours. There are no proper sidewalks or safe crossings. It is bad enough for those that live in behind that area or ones that go into the 2 clinics there. What about when the residents move into Dr. Turner? You cannot honestly believe this is a good idea for the City of Fort Saskatchewan. Your Heritage Centre got turned down and I truly hope this does too. I cannot believe the order of priority for this Mayor and Council. Mayor thinks the City of Fort Saskatchewan will continue to boom during this

	economic downturn or recession or whatever you want to call it. Perhaps the City will survive; I certainly hope so. But this has affected, is going to or will affect many residents in this City that depend on the oil industry for their paycheques. While the City may survive, some of our residents may not. Where are your priorities? Seriously, I cannot believe I voted for some of you. Make no mistake, it won't happen the next time around.
12	I believe that the skyline of the city should (as much as possible) allow for clear, open views of the surrounding river valley and open areas. My family and I chose to live in this city because of the abundance of green space, open sky, and the beauty of the river valley and parks. I believe the overall aesthetic of the city would be negatively impacted by large, looming high-rises of any kind.
12	Tall buildings are not only an eyesore in a community like this, but also a safety concern. If plans are in the works to build these structures then consideration must also be given to how the city would respond to a fire or other emergency on the top floors.
12	They should be limited to 6 stories or less. We're Fort Saskatchewan, not Edmonton. People who want to live in a big, dense city can move there. Please don't bring the high density out here.
12	I think 12 or lower would suite the city and fit in ok. Anything taller would take away from the cultural side and over all look of the city.
N/A	None of the above. Building a 12 storey will cause traffic problems and take away from the city of FS. We are not a big city that needs high rises. We moved to The Fort to get away from "the big city"
12	I believe that the maximum number of stories should be decreased to 5 floors.
12	I don't have any problem with hi rises I just feel that 12 stories is high enough for our little city. But if we had to go higher I don't have a problem with that either.
12	So far we already have 12 story on the books but we do not have a 12 story building yet. I think 12 is enough.
12	I only put 12 because there was no lower option. We moved here because it wasn't a large, tall, loud city. We understand growth, but please do not lose the charm of our city. It's what brings people here.
12	If you build any of the options of these high rises in such a small area the parking or downtown area will become congested as there is little access in or out for that amount of population. Disappointed as the community is become condensed and lack family appeal for parks or sports areas. Money and or time as members of council should be spent investing into the roads and traffic flows and upgrading the hwy 15 bridge to twin it! :(
N/A	There was no option for 4 or 6. What gives? I feel firmly that 12 or more is way too much. That would also clog one of the only roads out of town. Not a fan.
12	I do not want to see skyscrapers in the Fort. Even 12 storeys is too high. Six storeys would be the highest.
12	Leave it as it is
N/A	6 stories is plenty high enough in the older part of the Fort! We are not and never will be the big CITY nor do we wish to be!!
12	lower as possible in my opinion for safety concern
20	We need more apartments this is good!

20	will run out of space if don't
20	Going up costs nothing. Going out costs millions.

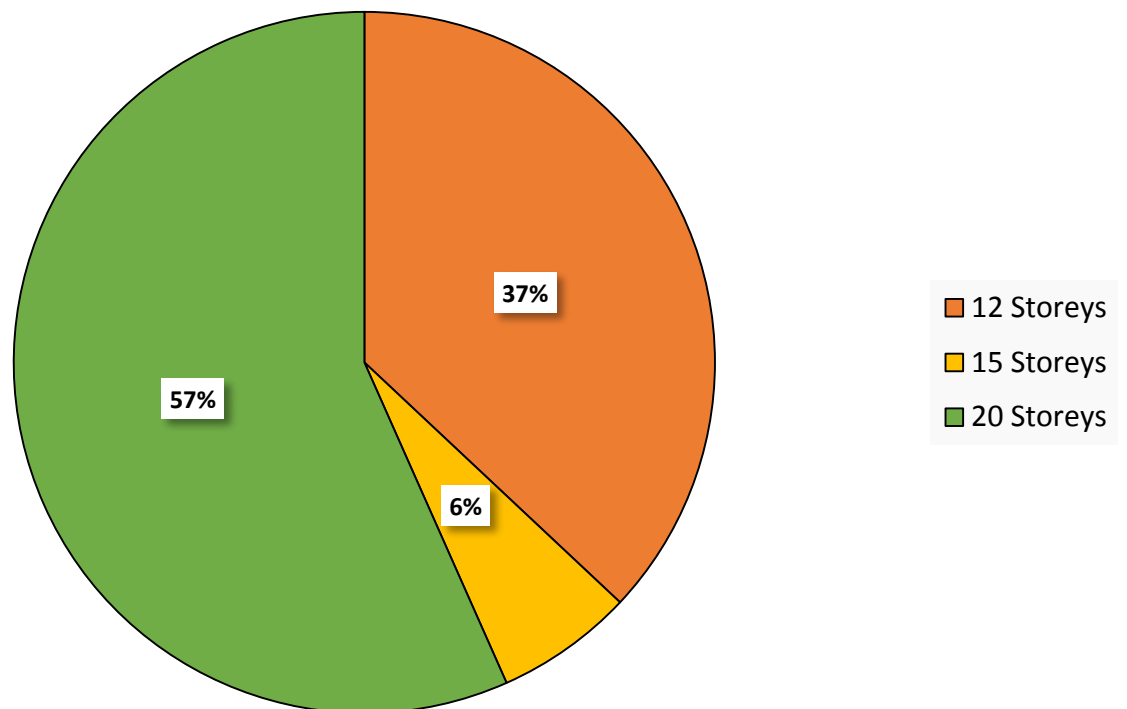
Attachment 6- Poster/Feedback Form Display Results**Building Height Limit for Apartments in Fort Saskatchewan Poster Results****Poster 1- Shell Theatre Lobby/City Hall Lobby**

1. How many Storeys do you think High-Rise Apartments should be?

Poster 1 Results	
Number of Storeys	Number of Votes
12 Storeys	64
15 Storeys	11
20 Storeys	98
Total	173

Poster 1

How many Storeys do you think High-Rise Apartments should be?



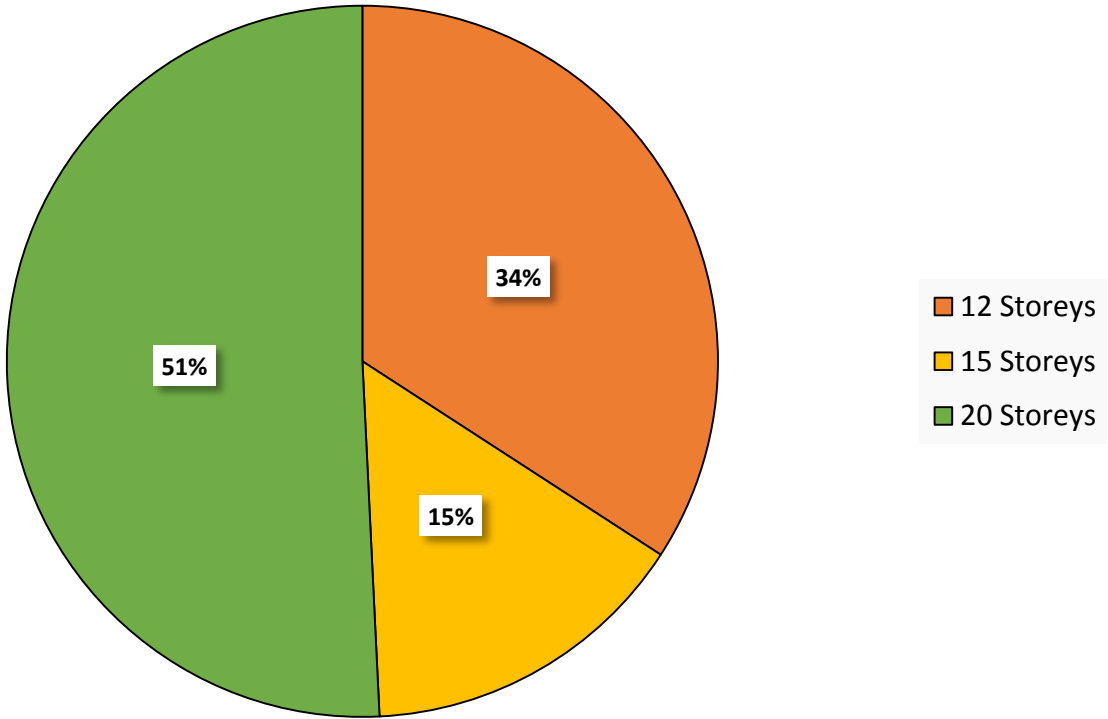
Poster 2- Dow Centennial Centre (DCC) Lobby

How many Storeys do you think High-Rise Apartments should be?

Poster 2 Results	
Number of Storeys	Number of Votes
12 Storeys	111
15 Storeys	49
20 Storeys	165
Total	325

Poster 2

How many Storeys do you think High-Rise Apartments should be?



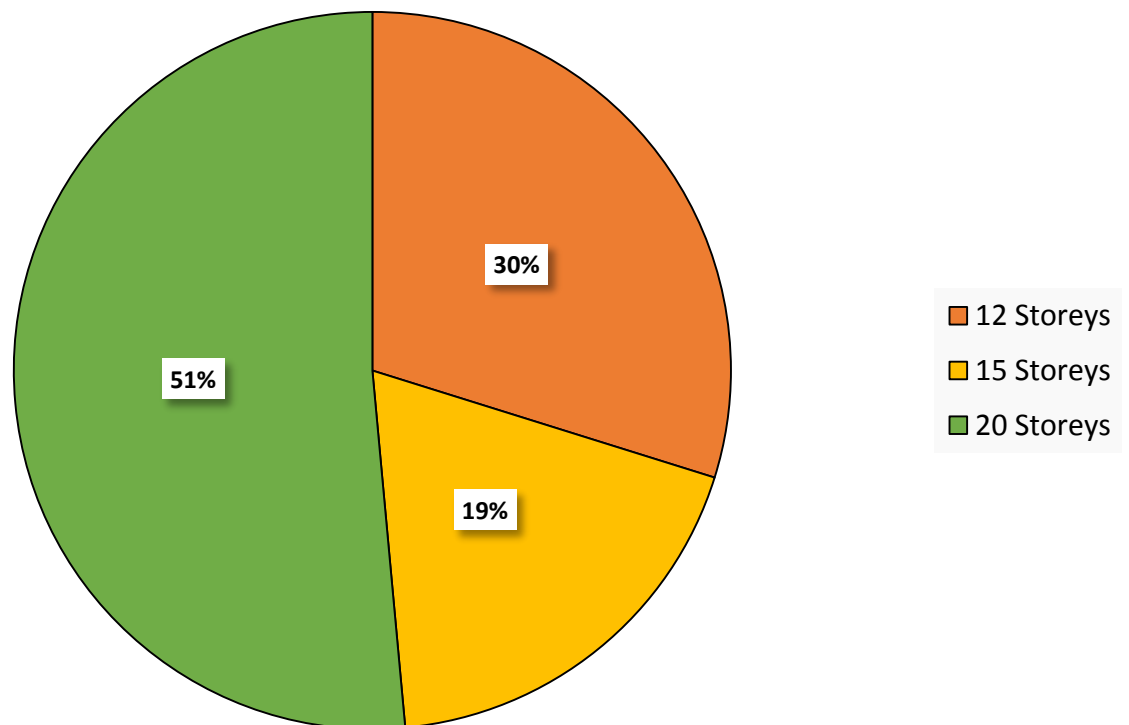
Poster 3- Harbour Pool Lobby/Jubilee Recreation Centre Lobby

How many Storeys do you think High-Rise Apartments should be?

Poster 3 Results	
Number of Storeys	Number of Votes
12 Storeys	51
15 Storeys	32
20 Storeys	88
Total	171

Poster 3

How many storeys do you think High-Rise Apartments should be?



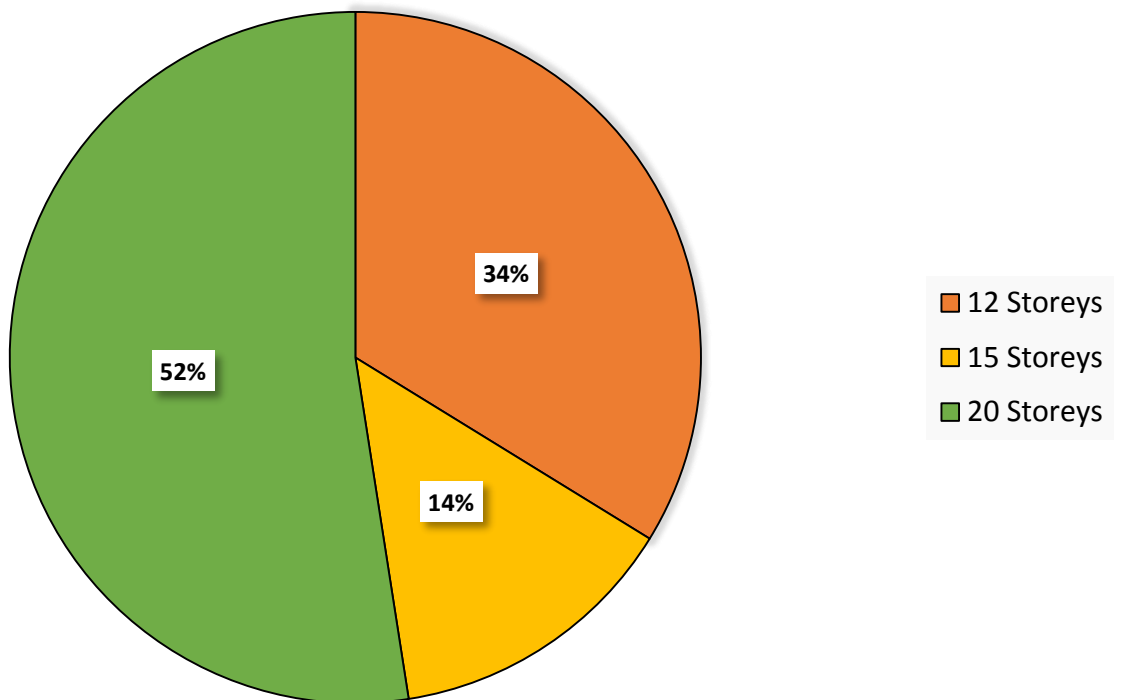
Total Poster Results- All Three Posters

How many Storeys do you think High-Rise Apartments should be?

Total Poster Results	
Number of Storeys	Number of Votes
12 Storeys	226
15 Storeys	92
20 Storeys	351
Total	669

Total Poster Results

How many Storeys do you think High-Rise Apartments should be?



Building Height Limit Feedback Form Results

The City of Fort Saskatchewan is considering whether the maximum height limit for high-rise apartments outside of the Downtown should be increased. We want to hear your opinions on the height limit of apartment buildings in the city.

# Storeys	Opinions/Comments
15	I think we need to be cognizant of the resources of the Fire Department as well. If we are building taller buildings then their needs may be necessary as well (ability to properly respond to a fire on floors 15-20).
15	
15	I just believe this to be a happy medium; wouldn't be against 20. Looking @ Ross Creek Building I believe is 4 story's, 3 times that would still look good but not out of place.
12	I love the feel of a small city, most homes are bungalows. We don't want a "concrete jungle"
N/A	This should not be a public question based on aesthetics. Look at how well city infrastructure is designed in order to support then appropriate population density.
12	Encouraging the growth necessary to flourish
20	
12	Any bldg. over 12 storeys is completely out of place in Fort Sask., in fact I believe even 12 is too high. And any high-rises must include underground parking. Also, Can you imagine the traffic congestion of all those vehicles coming out to one of our roadways? Definitely need to balance Developer "Greed" with city's long term vision.
20	To sustain the economic growth of the city and area, accommodations must be made for the people who will drive the growth. A properly engineered and constructed 20 storey building will be just as much space as a 12 storey building and will allow for better use of real estate so that school and parks will have the space they deserve.
15	
12	Keep small town feeling!!! If 20 stories- might as well live in Edmonton
15 & 20	As we get closer to building towards the outskirts of the city limits and industrial plans blocking one area, it only makes sense to build up. Let's be the city we know we can be.
15	A large building or buildings to suit the city's growing needs but not too large to lose the smaller city setting.
12	Suggestion- Top floor (railings) - ltd light for 'light free' sky watching. - The more levels, the more apartments, and thus the more vehicles (almost everybody seems to 'need' a vehicle it seems) and more parking. - What about cooling in summer. Heat rises and in my apt building, anything above 1 st or 2 nd floor for sure tends to be over-warm summer

	(and at times, winter too)? Are each expected to supply their own cooling (air conditioning) or is adequate building cooling planned? - Fire safety and rescue problems/Increased “view” blockage.
20	The higher the better. Do a 1,000 stories! This will have less impact on the surrounding ecosystem due to urban sprawl.
15	
N/A	Am opposed to any apartment building height above 4 storeys. Would support family- friendly- affordable low cost housing only. Do not turn Fort Sask. into another Sherwood Park or St. Albert.
N/A	How tall can the Fire Department handle?

City of Fort Saskatchewan

Bylaw C9-15

Adopt the Josephburg Road North Industrial Area Structure Plan

Motion:

That Council give first reading to Bylaw C9-15 to adopt the Josephburg Road North Industrial Area Structure Plan.

Purpose:

The purpose of this report is to present Council with information on the updated Josephburg Road North Industrial Area Structure Plan, and to consider first reading of Bylaw C9-15.

Background:

The Josephburg Road North Industrial Area Structure Plan (JRNI-ASP) was adopted by Council as Bylaw C13-09 on January 25, 2011. The purpose of the JRNI-ASP is to provide a policy framework for orderly planning in the area, respond to future subdivision and development proposals, and establish a preliminary servicing concept for the Josephburg Road North Industrial area.

The update to the JRNI-ASP was undertaken in order to consider alternative servicing for a portion of the Area Structure Plan (ASP) area. The update also reviewed land use patterns to assess the future industrial development needs for Fort Saskatchewan.

The JRNI-ASP update predominantly consists of minor text amendments throughout the document in order to bring the information contained up to date (such as population statistics, references to repealed Land Use Bylaws, etc.). Major changes can be summarized in three points:

1. **Change of land use designation** within the southeast quadrant of the ASP area from light industrial to medium industrial. The entire ASP area will be designated as medium industrial with this update.

The land use designation was re-designated from light to medium industrial in order to coincide with the alternate servicing. The ASP area west of Range Road 221 will have alternate servicing, which has been designed for less intense uses associated with medium industrial land uses.

2. **Alternate servicing provisions** include trickle water feed, on-site sanitary systems, and use of storm ponds for fire suppression east of Range Road 221. Full urban services are still required west of Range Road 221.

The existing JRNI-ASP requires full urban servicing within the entire ASP area; however, medium industrial developments are often land intensive and require less servicing than a typical commercial or light industrial use. Minimal activity has occurred within the Josephburg ASP area for the past six years for a variety of reasons, such as limited flexibility of the ASP policies regarding servicing requirements.

Therefore, alternative servicing was contemplated with this ASP update. Alternate servicing includes use of septic tanks, trickle water with on-site cisterns, and storm ponds for fire suppression.

3. **Revised Highway 15 Vicinity Overlay boundary** due to the revised servicing model. The Highway 15 Vicinity Overlay area is intended to allow some minor commercial uses that are compatible with the industrial character of the area, subject to a Risk Assessment. The Highway 15 Vicinity Overlay area now lies west of Range Road 221 along the Highway 15 frontage.

The Land Use Bylaw (LUB) currently zones a portion of the subject area with the Highway 15 Vicinity Overlay. The Overlay allows uses in addition to the underlying zoning, subject to a Risk Assessment. To ensure conformance between the JRNI-ASP and the LUB, the Overlay boundaries within the LUB will require amendment through the LUB 2015 Refresh (provided surplus funding is available).

As alternate servicing was being considered, and a portion of the subject area was graduating from a light industrial designation to a medium industrial designation, a Risk Assessment was prepared by Doug McCutcheson and Associates. The Risk Assessment concluded the alternate servicing for industrial uses was acceptable, with 'shelter-in-place' design considerations potentially warranted within the Highway 15 Vicinity Overlay. No concerns were identified with graduating the land use from light to medium.

Plans/Standards/Legislation:

The *Municipal Government Act* (MGA) provides municipalities the ability to adopt an ASP in order to provide a framework for subsequent subdivision and development of an area of land. As per the MGA, an ASP must describe the sequence of development proposed for the area, the land uses proposed for the area, either generally or with respect to specific parts of the area, the density of population proposed for the area either generally or with respect to specific parts of the area, and the general location of major transportation routes and public utilities.

The MGA requires that while preparing a statutory plan, a municipality must provide a means for any adjacent municipality and any person who may be affected to make suggestions and representations. As such, public consultation during the update of the JRNI-ASP included the following:

1. Two public Open Houses:
 - a) The first Open House was held November 7, 2013 at the outset of the update to gather information from stakeholders, landowners, neighbours, and the general public regarding the state of the current ASP, and recommended changes to consider during the update process. Alternative options for servicing and land use were provided for consideration.
 - b) A second Open House was held November 27, 2014 to present a draft of the JRNI-ASP update.
2. The draft JRNI-ASP and opportunities to provide feedback were also provided through the City's website.

3. A presentation of the draft JRNI-ASP was made to the Alberta Industrial Heartland Association Economic Development Committee to inform our stakeholders of the intended changes included in the update.
4. The draft JRNI-ASP was sent to a number of stakeholders for their information and comments, including Alberta Transportation, landowners within the ASP area, and neighbouring landowners. No comments have been received to date from any of these stakeholders.
5. The draft JRNI-ASP was circulated to Strathcona County, Sturgeon County, Lamont County, and the City of Edmonton. Responses were not received from the City of Edmonton and Lamont County. Sturgeon County provided a letter identifying no concerns regarding the JRNI-ASP update. Strathcona County provided a list of concerns, and comments regarding the draft JRNI-ASP. Most of the comments received were minor issues, and textual amendments. Some concerns related to the need for Alberta Transportation's involvement and the need for a Traffic Impact Assessment (TIA). As amending the road network was not part of the update, the need for a supplemental TIA was considered unnecessary. However, it was agreed further TIAs may be necessary when the local road network is established at the Outline Plan stage.

Capital Region Board (CRB) approval is required for ASP adoptions and/or amendments within 1.6km of the Alberta Industrial Heartland. Following first reading, Administration will submit an application under the Regional Evaluation Framework. The Public Hearing and final readings will be scheduled after the CRB renders a decision.

Should Council grant first reading to Bylaw C9-15, and CRB approval be given, ads will be published in the local newspaper as per the MGA requirements. Notifications will also be mailed to landowners, adjacent landowners, and the neighbouring municipality.

Financial Implications:

Analysis on the financial considerations will be examined and outlined in the subsequent Public Hearing report to Council.

Alternatives:

1. That Council give first reading to Bylaw C9-15 to adopt the Josephburg Road North Industrial Area Structure Plan.
2. That Council not proceed with first reading to Bylaw C9-15, thus leaving the current Josephburg Road North Industrial Area Structure Plan Bylaw C13-09 in place, and advise how they wish to proceed.

Attachments:

1. Bylaw C9-15
2. Schedule A – Updated Josephburg Road North Industrial Area Structure Plan
3. Appendix A – Redline original Josephburg Road North Industrial Area Structure Plan
4. Appendix B – 2009 JRNI-ASP Land Use Concept Map
5. Appendix C – 2015 JRNI-ASP Land Use Concept Map

File No.: Bylaw C9-15

Prepared by:	Claire Negrin Long Range Planner	Date: March 4, 2015
Approved/Reviewed by:	Troy Fleming General Manager, Infrastructure & Community Services & Acting City Manager	Date: March 4, 2015
Submitted to:	City Council	Date: March 10, 2015



CITY OF FORT SASKATCHEWAN

**A BYLAW OF THE CITY OF FORT SASKATCHEWAN IN
THE PROVINCE OF ALBERTA TO ADOPT THE JOSEPHBURG ROAD NORTH
INDUSTRIAL AREA STRUCTURE PLAN**

BYLAW C9-15

WHEREAS the *Municipal Government Act*, R.S.A.,2000, c.M-26 as amended or repealed and replaced from time to time, provides that a municipality may adopt an Area Structure Plan;

NOW THEREFORE, the Council of the City of Fort Saskatchewan, in the Province of Alberta, duly assembled, enacts as follows:

1. This Bylaw is cited as the City of Fort Saskatchewan Josephburg Road North Industrial Area Structure Plan.
2. The attached Schedule "A" forms part of this Bylaw.
3. This Bylaw repeals Bylaw C13-09 and amendments thereto.
4. If any portion of this Bylaw is declared invalid by a court of competent jurisdiction, the invalid portion must be severed and the remainder of the Bylaw is deemed valid.
5. This Bylaw becomes effective upon third and final reading.

READ a first time this	day of	2015.
READ a second time this	day of	2015.
READ a third time and passed this	day of	2015.

MAYOR

DIRECTOR, LEGISLATIVE SERVICES

DATE SIGNED: _____



Josephburg Road North Industrial Area Structure Plan

BYLAW C9-15

SCHEDULE A

Prepared For:
The City of Fort Saskatchewan

Prepared By:
Stantec Consulting Ltd.
File: 1161 70800

Adopted as Bylaw C13-09 January
25, 2011

**Reviewed and Updated for the City
of Fort Saskatchewan by Scheffer
Andrew Ltd. in 2014**

Adopted as Bylaw C9-15 _____

This Page Left Intentionally Blank

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Table of Contents

1.0 INTRODUCTION	1.3
1.1 INTRODUCTION	1.3
1.2 PURPOSE	1.3
1.3 LAND OWNERSHIP	1.3
1.4 REGIONAL CONTEXT	1.7
1.5 ORIENTATION	1.7
2.0 STATUTORY AND POLICY CONTEXT.....	2.11
2.1 CITY OF FORT SASKATCHEWAN MUNICIPAL DEVELOPMENT PLAN (2010 – 2030).....	2.11
2.2 ALBERTA'S INDUSTRIAL HEARTLAND AREA STRUCTURE PLAN	2.13
2.3 CITY OF FORT SASKATCHEWAN LAND USE BYLAW	2.14
2.4 CITY OF FORT SASKATCHEWAN STRATEGIC PLAN.....	2.15
2.5 STRATHCONA COUNTY STATUTORY PLANS	2.15
3.0 SITE CHARACTERISTICS AND DEVELOPMENT CONSIDERATIONS	3.16
3.1 TOPOGRAPHY AND VEGETATION.....	3.16
3.2 PRELIMINARY ECOLOGICAL ASSESSMENT.....	3.16
3.3 EXISTING AND SURROUNDING USES.....	3.21
3.3.1 Existing Land Uses	3.21
3.3.2 Surrounding Land Uses	3.22
3.3.3 Natural Constraints	3.22
3.3.4 Man-Made Constraints.....	3.23
3.3.5 Parks, Recreation, Open Space and Trails.....	3.28
3.4 TRANSPORTATION.....	3.28
3.4.1 Highway 15	3.28
3.4.2 Fort Saskatchewan By-Pass.....	3.29
3.4.3 Township Road 550	3.30
3.4.4 Range Road 221	3.30
3.4.5 Range Road 220.....	3.30
3.4.6 Dangerous Goods Routes	3.30
3.5 MUNICIPAL SERVICES	3.30
3.6 EMERGENCY SERVICES.....	3.31
3.6.1 Fire, Police, Ambulance and Disaster Services	3.31
3.6.2 Emergency Preparedness	3.31
3.7 INDUSTRIAL RISK ASSESSMENT.....	3.31
4.0 LAND USE CONCEPT, OBJECTIVES AND POLICIES.....	4.34
4.1 DEVELOPMENT CONCEPT	4.34
4.2 INDUSTRIAL DEVELOPMENT	4.37
4.3 MEDIUM INDUSTRIAL – HIGHWAY 15 VICINITY OVERLAY.....	4.40
4.4 UTILITY RIGHT OF WAYS.....	4.43

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.5 ENVIRONMENTAL MANAGEMENT	4.45
4.6 TRANSPORTATION.....	4.47
4.7 SUSTAINABLE DEVELOPMENT PRACTICES	4.54
4.8 MUNICIPAL SERVICING.....	4.55
Water Servicing Concept	4.58
Sanitary Servicing Concept.....	4.58
Storm Servicing Concept	4.63
Figure 8 4.65	
Shallow Utilities (Power, Gas, Telecommunications).....	4.69
Pipeline / Utility Corridors.....	4.69
4.9 HIGHWAY 15 CORRIDOR DESIGN GUIDELINES.....	4.70
4.10 RISK MANAGEMENT	4.72
4.11 IMPLEMENTATION	4.74
APPENDIX 1 – LAND OWNERSHIP	4.79
APPENDIX 2 – LAND USE STATISTICS	4.80
APPENDIX 3 – REFERENCES	4.81

List of Figures

Follows Page

1.0	PLAN AREA	1.5
2.0	LAND OWNERSHIP	1.9
3.0	SITE CONTOURS	3.17
4.0	NATURAL SITE FEATURES AND MAN-MADE CONSTRAINTS	3.19
5.0	LAND USE CONCEPT	4.35
6.0	TRANSPORTATION NETWORK	4.51
7.0	CONCEPTUAL WATER SERVICING	4.61
8.0	CONCEPTUAL SANITARY SERVICING	4.65
9.0	CONCEPTUAL STORM SERVICING	4.67
10.0	CONCEPTUAL DEVELOPMENT PHASING	4.77

List of Tables

Page

1	Pipeline Information Summary	3.25
2	Utility Right-of-Way Information Summary	3.27

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

1.0 Introduction

1.1 INTRODUCTION

The purpose of the Josephburg Road North Industrial Area Structure Plan (ASP) is to provide a policy framework for orderly planning in the area, respond to future subdivision and development proposals, and establish a preliminary servicing concept for the Josephburg Road North Industrial area. The objectives of this ASP are to:

- Prepare a policy document and a future development concept for the plan area;
- Establish the general location of land uses and transportation routes to support development within the plan area;
- Conceptually identify public utilities and storm water management facilities (SWMF) necessary to support development;
- Outline preliminary and conceptual servicing requirements for the proposed development concept;
- Determine the appropriate sequencing and phasing of development based on servicing, access and market demand; and,
- Prompt the creation of a Boundary Road Accord Agreement between the City of Fort Saskatchewan and Strathcona County for Range Road 220 when warranted.

1.2 PURPOSE

The plan area covers approximately 590 ha (1,459 ac). As shown in **Figure 1.0 - Plan Area**, the plan area is located within the municipal boundaries of the City of Fort Saskatchewan. The plan area boundaries are as follows:

Northern Boundary:	Highway 15
Eastern Boundary:	Range Road 220 (Strathcona County)
Western Boundary:	Highway 15
Southern Boundary:	Township Road 550 (Strathcona County)

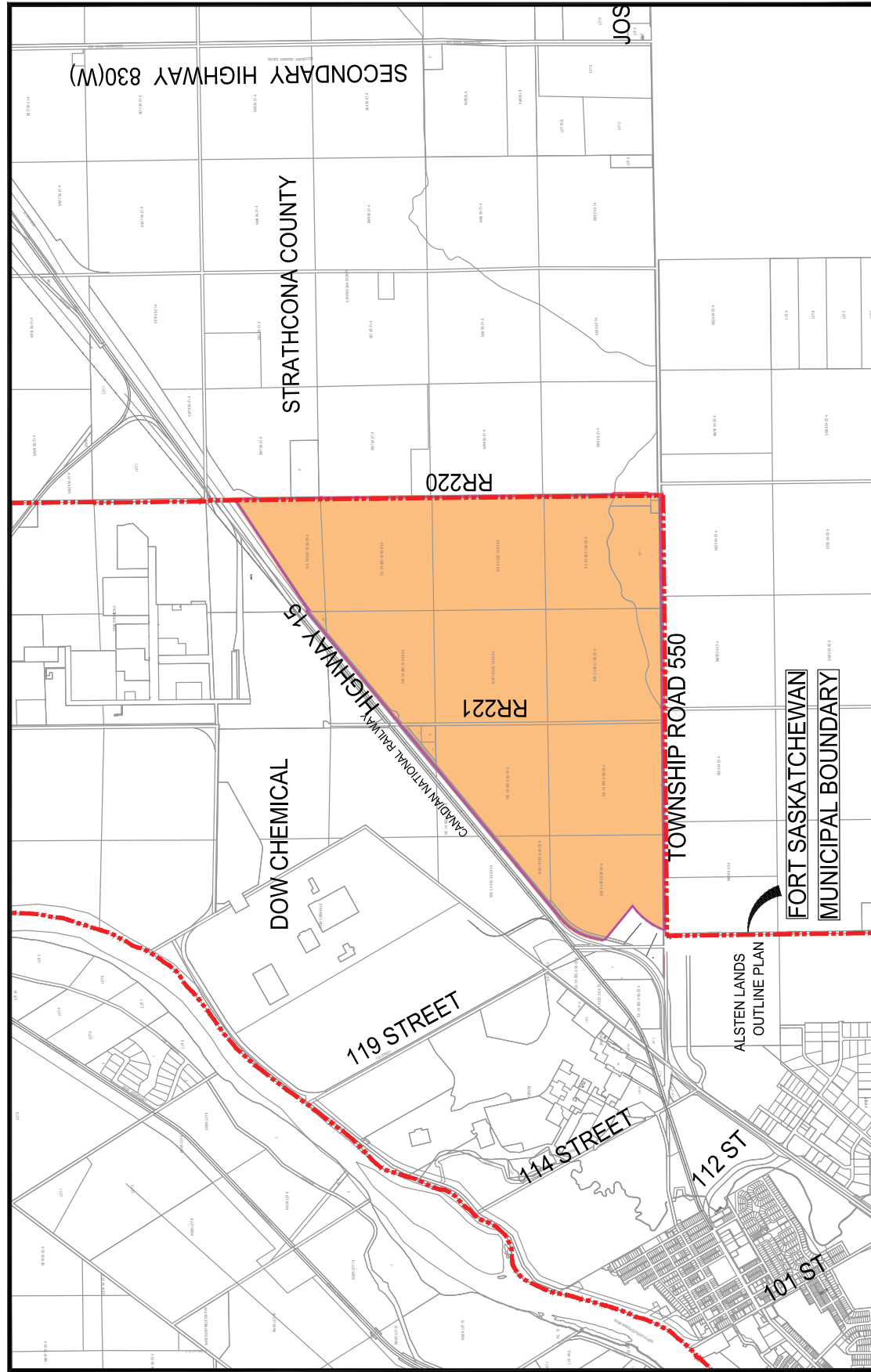
1.3 LAND OWNERSHIP

Most of the parcels within the Plan area are un-subdivided (i.e. full quarter sections) or have been severed by the Highway 15 right-of-way or the existing drainage ditch in the southeast portion of the Plan area. Three smaller parcels (ranging from 0.5 ha to 5 ha) are registered in the southeast corner of the plan area (Range Road 220), and are currently titled to private landowners. Most of the land in the plan is held by private developers, while a private landowner currently owns approximately 2½ quarter sections adjacent to Range Road 220.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

Figure 1.0
Plan Area



Josephburg Road North Industrial

Area Structure Plan

Bylaw C13-09 1.5

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Figure 2.0 - Land Ownership and **Appendix 1 – Land Ownership** show the land ownership within the plan area.

1.4 REGIONAL CONTEXT

Fort Saskatchewan is centered within Alberta's Industrial Heartland Region, and is home to numerous industrial petrochemical and related industries, providing an important link to the Athabasca Oil Sands of northeastern Alberta and international markets. It is one of Canada's largest processing centres for petroleum, petrochemicals and chemicals, and employs a large, skilled workforce. The City of Fort Saskatchewan is home to 22,808 residents, and the majority of land uses include residential, light/medium/heavy industrial, commercial, business and agricultural uses. Highway 21 provides one of the main access routes to Fort Saskatchewan while Highway 15 provides a second link to the Capital Region, and access to northern and eastern Alberta.

The Alberta's Industrial Heartland region lies within portions of five municipalities: The City of Fort Saskatchewan, the City of Edmonton, the Counties of Strathcona, Sturgeon and Lamont. It is designated for long-term heavy and medium industrial growth, and is already home to over 50 world-class companies, with still more facilities proposed.

The City of Fort Saskatchewan is a member of the Capital Region Board (CRB), along with 23 other municipalities in the Capital Region. Member municipalities are subject to the Capital Region Growth Plan (CRGP), which is a regional growth management document. The City of Fort Saskatchewan, has been identified as being a priority growth area within the CRGP.

1.5 ORIENTATION

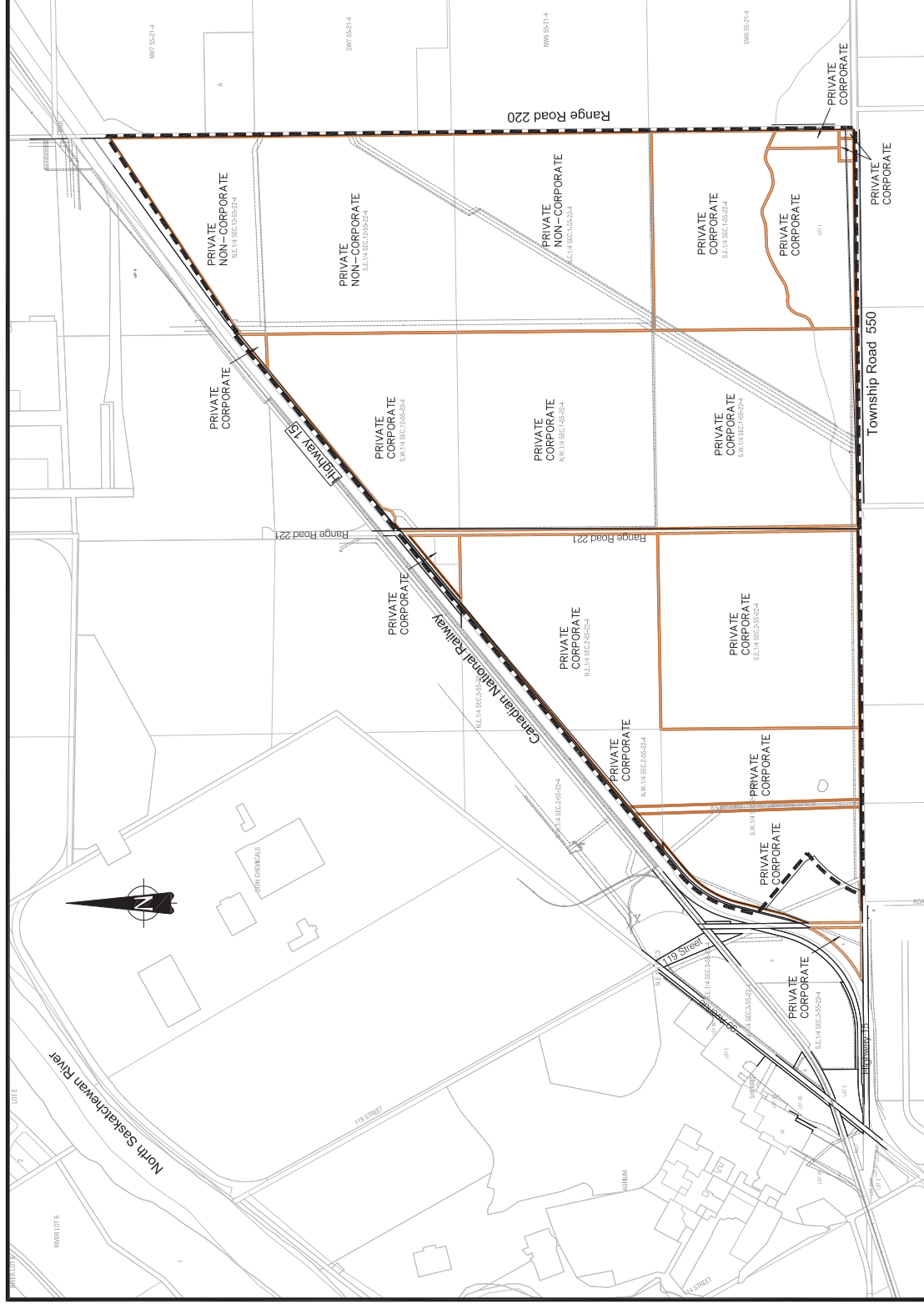
This document contains four (4) sections and 3 appendices.

- Section 1 provides the Introduction, purpose and regional context of the plan;
- Section 2 describes the Statutory and Policy Context;
- Section 3 outlines the Site Characteristics and Development Considerations;
- Section 4 Describes the Land Use Concept, Objectives and Policies;
- Appendix 1 contains background information on Land ownership;
- Appendix 2 provides the proposed Land Use Statistics;
- Appendix 3 contains a listing of technical studies used in support of the preparation of the development and servicing concepts.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

Figure 2.0
Land Ownership



NTS

Ownership Boundary
ASP Boundary



CITY OF
FORT SASKATCHEWAN
ALBERTA

Josephburg Road North Industrial Area Structure Plan

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

2.0 Statutory and Policy Context

The ASP has been prepared in context of its geographical location within the Edmonton Capital Region and the Heartland Industrial Area.

It has been planned having regard for the City of Fort Saskatchewan's statutory plans, policies and design principles which govern land development. These include the City of Fort Saskatchewan Municipal Development Plan (MDP), the Fort Saskatchewan Alberta's Industrial Heartland Complementary Area Structure Plan, and Land Use Bylaw C10-13. Other relevant policies and design principles are further identified and summarized in the following sub-sections as they relate to the creation of an industrial park within the plan area.

It must be noted that the ASP underwent a review and update in 2013/2014, in order to ensure alignment with the revised MDP, LUB and CRGP. This review and update involved amendments to the proposed land uses and servicing concepts within the plan area, in order to provide for greater flexibility and encourage development within the plan area.

Future applicants seeking amendments to the ASP or applying for rezoning, subdivisions or development permits are required to consult the actual documents for specific guidance on detailed requirements as they apply to particular properties.

The following documents provide important direction and a policy context for the ASP.

2.1 CITY OF FORT SASKATCHEWAN MUNICIPAL DEVELOPMENT PLAN (2010 – 2030)

The Municipal Development Plan (MDP) was adopted in September 2010 as Bylaw C16-10, and is intended to express the community's vision, goals, objectives and policies to direct its physical, social and economic development from 2010 to 2030.

Several of the policies contained within the MDP have been used in the preparation of this ASP, ensuring conformity with Council direction with respect to the development of these lands for industrial purposes. The relevant policies include:

Section 6.8 ➔ Industrial ➔ General Policies for all Industrial Areas

Section 6.8.1 Implement the Industrial Heartland Area Structure Plan (ASP) and the Josephburg Road North Industrial ASP, which set the main policy direction for the development of industrial areas.

Section 6.8.3 Encourage the utilization of eco-industrial planning principles, seeking enhanced environmental and economic performance through collaboration in managing environmental and resource issues, including energy, water and

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

materials, as outlined in the Industrial Heartland ASP and the Eco-Industrial Master Plan Strategy.

Section 6.8.4 Require, through provisions of the Land Use Bylaw and other municipal bylaws, quality development to be maintained through the application of standards for siting and design of buildings, landscaping, screening of storage and parking areas, and signage.

Section 6.8.7 Permit interim agricultural uses that will not preclude the future use of the land for industrial purposes.

Section 6.8.8 Ensure that developments mitigate visual impacts from public roads.

Section 6.8 ➔ Industrial ➔ Light and Medium Industrial

Section 6.8.15 Complete and implement the Josephburg Road North Industrial Area Structure Plan (ASP).

Section 6.8.16 Ensure compatibility between the Light and Medium industrial designation and surrounding urban development by directing only Light Industrial uses to those lands within proximity of the General Urban Area.

Section 6.8.17 Proactively work to ensure a supply of fully serviced Light and Medium Industrial lands.

Section 6.8.18 Ensure that Light and Medium Industrial uses meet current industrial Risk standards and guidelines.

Section 6.8.19 Use a risk management approach based on principles of: risk reduction at the source; risk reductions through land use controls and prescribed mitigation measures; emergency preparedness; emergency response; and, risk communication.

Section 7.2 ➔ Community Design ➔ Sustainable Design Policies

Section 7.2.3 Work with the development community to provide incentives for the development of buildings and developments that meet green building standards.

Section 8.2 ➔ Mobility ➔ Pedestrians and Cyclists

Section 8.2.2 Plan for pedestrian and cyclist facilities as part of development and redevelopment proposals, ensuring the provision of adequate walking and cycling paths and lanes, and adequate cycle facilities such as secure storage, changing rooms, and showers where appropriate/feasible.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Section 8.4 ➔ Mobility ➔ Roadways

- Section 8.4.1 *Use the Transportation Master Plan as a reference to ensure that adequate rights-of-way are preserved and incorporated into future development areas.*
- Section 8.4.2 *Limit vehicular accesses along Highway 15 and 21 to provide an expressway standard.*
- Section 8.4.3 *Ensure that Transportation Impact Studies are prepared to evaluate the impacts of major development on the safe and efficient movement of pedestrians, cyclists, public transit and vehicles.*

Section 10.2 ➔ Parks and the Natural Environment ➔ Municipal, School and Environmental Reserves

- Section 10.2.3 *Require that environmental reserve, municipal and school reserves be identified through the ASP and ARP processes, to the satisfaction of the Municipality.*
- Section 10.2.4 *Ensure that industrial ASP and ARP processes consider preservation of land for parkland, and that municipal reserves are established for this purpose.*

Section 10.4 ➔ Parks and the Natural Environment ➔ Pathways and Trails

- Section 10.4.1 *Facilitate the continued development of community and regional trail systems that provide connectivity for both leisure and commuting purposes.*

Section 11.4 ➔ Infrastructure and Resource Management ➔ Stormwater Management

- Section 11.4.2 *Maximize retention of stormwater or require enhancement to natural wetlands where possible to ensure a high quality of stormwater effluent.*
- Section 11.4.3 *Minimize effective impervious area for all new development to reduce development related stormwater run-off.*

Section 13.3 ➔ Responsive Local Economy ➔ Economic Diversification

- Section 13.3.1 *Support economic diversification by ensuring there are sufficient commercial and industrial lands available to suit a variety of business opportunities.*

2.2 ALBERTA'S INDUSTRIAL HEARTLAND AREA STRUCTURE PLAN

The City of Fort Saskatchewan Alberta's Industrial Heartland Area Structure Plan (ASP) was approved under Bylaw C19-00 in August 2001. At the same time, the municipalities of Lamont County, Strathcona County and Sturgeon County approved similar, complimentary ASP's for

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

lands within their jurisdictions. Together, these four ASP documents refer to the area now marketed internationally as the Alberta's Industrial Heartland.

The purpose of these four ASP's is to guide future industrial development within each municipality and the region as a whole. More specifically the plans:

- Encourage more efficient use of the land resource;
- Establish complementary land use policies for industrial, transition and environmental areas in the Heartland Region;
- Reduce environmental impacts;
- Reduce land use conflicts and establish buffers and transition areas around heavy industry;
- Facilitate efficient provision and extension of transportation and utility infrastructure and joint use of these services;
- Encourage the use of eco-industrial principles, e.g. co-generation.

The land use concept for the ASP identifies the entire plan area as being within the Medium Industrial Policy Area. This policy area is intended to serve as a transition zone between heavy industrial uses and lighter/non-industrial land uses. Important guidelines outlined in this ASP, and which dictate development is as follows:

- This area will accommodate medium industrial activities that could support the activities associated with the heavy industry activities throughout the Heartland;
- Activities could include: heavy industrial and agricultural equipment sales, rentals and repairs; laboratories; temporary indoor storage; veterinary clinics or hospitals; minor eating establishments; contracting services; vehicle repair and service stations and greenhouses/nurseries;
- Access to and from Highway 15 will be centralized in one or two intersections;
- To maintain the point of entry into the Heartland Area, regulations for the development will require a high standard of site design, signage, open space, architectural control and landscaping.

2.3 CITY OF FORT SASKATCHEWAN LAND USE BYLAW

The plan area is, in large part, currently zoned as IR – Industrial Reserve, but is agricultural and presently under cultivation. The IR district is generally intended to reserve those areas of the City which are rural in character or land use but are intended for future industrial development. Approximately 47 ha (117 acres) adjacent to Highway 15 and Range Road 221 is currently zoned as IM - Medium Industrial; this district is generally intended to establish an area of industrial uses where site regulations require a high standard of site design, open space, and landscaping. This district is intended to serve as a buffer between heavier industrial and other land uses.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

In addition to the lands zoned as IM and IR, there are approximately 23.5 ha (58 acres) of land at the intersection of Range Road 220 and Township Road 550, that are currently zoned as IL - Light Industrial. This district is intended to provide for light industrial uses that do not adversely affect adjacent land uses or cause any external, objectionable or dangerous conditions outside of any building on the light industrial site. This district is also intended to serve as a buffer between heavier industrial and other land uses.

In addition to the zoning currently in place, a portion of the plan area adjacent to Highway 15 is subject to the Highway 15 Vicinity Overlay. The purpose of the Overlay is to provide for limited highway commercial uses adjacent to Highway 15 as identified in **Figure 5.0 – Land Use Concept** of the ASP.

Lands that are adjacent to, but not within, the plan area and are within the City of Fort Saskatchewan are currently zoned as IL-Light Industrial, IM-Medium Industrial or IH-Heavy Industrial.

2.4 CITY OF FORT SASKATCHEWAN STRATEGIC PLAN

The City of Fort Saskatchewan's Strategic Plan: 2020 Vision – Clarity for the Future, identifies economic development as a significant strategic priority. Within that priority, it is the City's objective to attract medium industrial activities by providing an adequate amount of serviced and available industrial land for sale to prospective industries. The preparation of this ASP provides for the proper planning and development of the industrial area in supporting the City in achieving their strategic objective of creating a sizeable medium industrial land base.

2.5 STRATHCONA COUNTY STATUTORY PLANS

Associated statutory plans and the Land Use Bylaw within Strathcona County to the east and south of the plan area include the Municipal Development Plan Bylaw 1-2007, Strathcona County Alberta's Industrial Heartland ASP Bylaw 65-2001 and the Land Use Bylaw 8-2001.

The MDP identifies lands south and east of the plan area as being under the Agriculture Large Holdings Policy Area or the Agri-Industrial Transition Policy Area. The Strathcona County Alberta's Industrial Heartland ASP identifies the lands adjacent to Highway 15, north-east of the plan area, as being within the Strathcona Transition Policy Area.

The Land Use Bylaw has zoned these lands adjacent to the plan area as AG Agriculture: General. The plan area is also in proximity to the Josephburg Airport (within 1.5 miles). As such, the A-O Airport Vicinity Protection Overlay applies to some of these lands.

Although the airport is within Strathcona County, a portion of the eastern plan area is affected by the conical and inner-horizontal surfaces and take-off/approach surface of the runway. Therefore, it should be recognized that the airport may influence the height and type of development permitted within the plan area.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

3.0 Site Characteristics and Development Considerations

3.1 TOPOGRAPHY AND VEGETATION

As shown in **Figure 3.0 - Site Contours**, the topography of the land within the plan area is primarily flat, with gradual sloping from east to west. The highest point is at an elevation of 631.1 m (within the eastern portion of the N.E. 1/4 SEC.1-55-22-4), while the lowest elevation is 624.1 m (within the eastern portion of the S.W. 1/4 SEC.2-55-22-4). The maximum elevation difference across the entire plan area is approximately 7.0 m.

The majority of the land within the plan area has been cleared of vegetation and is under cultivation. However, there are several depressional areas loosely scattered throughout the plan area and a large tree stand in the north-east corner. Stantec Consulting Ltd. undertook a preliminary ecological assessment of these features, described in Section 3.2 below, to provide a preliminary baseline inventory of site features.

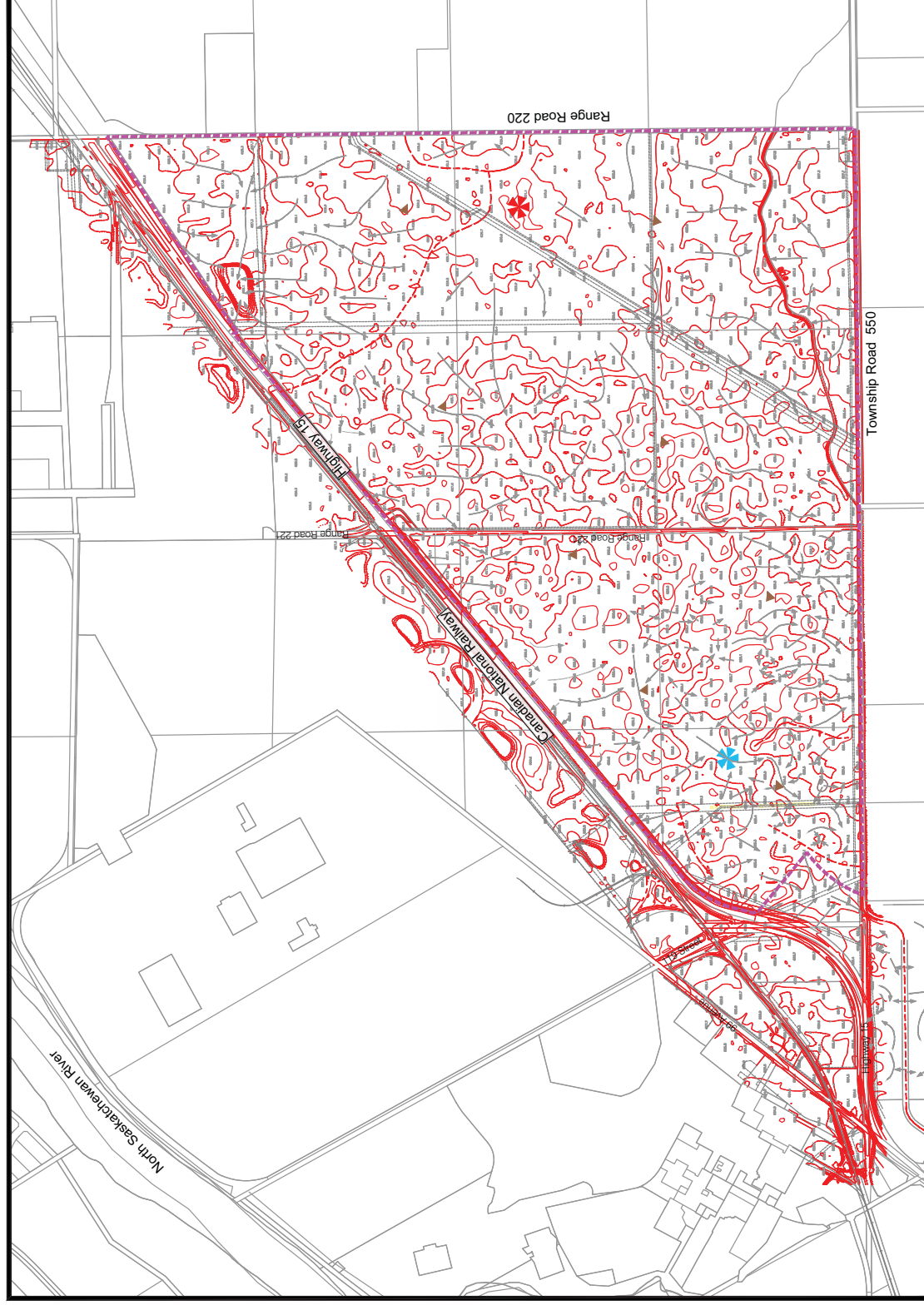
3.2 PRELIMINARY ECOLOGICAL ASSESSMENT

The ASP is located within the Parkland Natural Region and Central Parkland Subregion. The Parkland Natural Region is a transition vegetation zone, characterized by fescue grassland interspersed with pockets of aspen forest and mixed wood forest. The Central Parkland Subregion typically consists of groves of poplar intermixed with grasslands dominated by rough fescue. As part of the ecological assessment, a site reconnaissance was conducted in the summer 2008. This investigation identified a total of 3 wetlands (defined as having standing water and/or the presence of hydrophilic vegetation), a tree stand and a drainage course. Two of the wetlands, identified as Wetland (W1) and Wetland (W2) on **Figure 4.0 – Natural Site Features and Man-Made Constraints**, have been classified as intermittent bodies of water. Both wetlands are at low-lying elevations and are absent of distinct hydrophilic vegetation such as cattails, sedges, rushes and distinctive soils usually characteristic of higher order wetland systems. It is likely that these areas contain open water during portions of the wet season and would seldom hold water for prolonged periods of time. No wildlife was observed at either of these locations. Therefore, given their low-lying elevations, it is suggestive that the primary function of these wetland features would be as a surficial run-off storage feature.

The third wetland, Wetland (W3), did not appear on the landscape until after 1996 and is a large, open water feature with steep banks and no outlying fringe of vegetation, suggestive of a man-made feature. However, various waterfowl species were identified using the area.

The existing tree stand is located in a portion of the NE and SE ¼ Sec 12-55-22-W4M, and is identified as “Tree Stand”. This area has a large contiguous stand that contained healthy mature balsam poplar with a thick willow understory, a meadow-like interior consisting of reed canary

Figure 3.0
Site Contours



- ASP Boundary
- Existing Drainage Divide
- Existing Drainage Direction
- LOW POINT = 624.1 m
- HIGH POINT = 631.1 m

Josephburg Road North Industrial

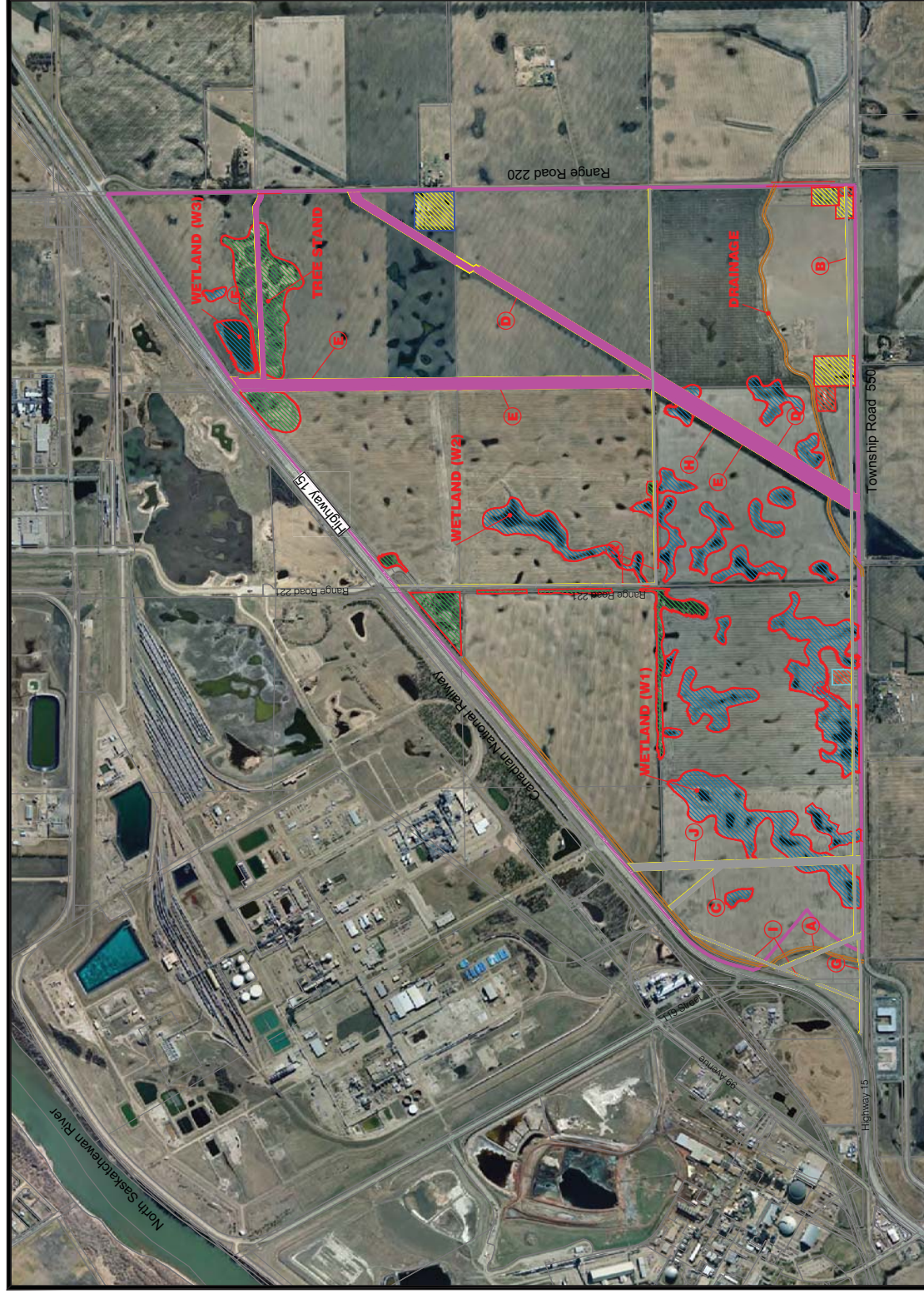
Area Structure Plan

Bylaw C13-09^{3,19}

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Figure 4.0

Natural Site Features and Man-Made Constraints



Legend

- A. Gasline R/W 4729hw
- B. Gas Pipeline R/W 3014KS
- C. Products Pipeline R/W 1913TR
- D. Pipeline R/W 952 5272; Petroleum Products Pipeline 822 1189; Oil Pipeline R/W 3859NY
- E. Oil Pipeline R/W 5275TR; Gas Pipeline R/W 792 2509
- F. Light Hydrocarbons Liquids Pipeline R/W 5073TR



- Residential Uses
- Agricultural Use
- Existing Drainage Channel
- Vegetation
- Low Lying Areas
- Pipeline/ Utility Corridor
- ASP Boundary

Josephburg Road North Industrial Area Structure Plan

Bylaw C13-09^{3,21}

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

grass. Surrounding the stand were intermittent depressional areas that could potentially hold water for portions of the year. The preliminary site visit suggests that the stand is sustainable and a permanent feature on the landscape. A review of historical air photos revealed that the woodlot has been present on the landscape as a depressional area since 1949 and it appears vegetation has been associated with it since 1962. In addition, a variety of wildlife species were observed throughout the stand, either directly or indirectly, including migratory birds and the presence of deer were indicated.

The drainage course runs from the southwest corner of the SW ¼ 1-55-22-W4M to the east of the SE ¼ 1-55-22-W4M, and has been evident on the landscape since 1949. The channel appears to be a natural creek, which has been heavily modified over time. The historical air photo review indicates that the drainage channel likely contains water during parts of the year. Further investigation of the channel would be required prior to any development if additional modifications are proposed.

Based on the ecological assessment, Wetlands (W1) and (W2) have been heavily modified through agricultural practices over time, while Wetland (W3) appears to be a man-made feature. If (W1), (W2) or the drainage course, are to be conserved, it is recommended that a buffer be implemented around the perimeter to protect wildlife habitat and water quality within the wetland. Due to historical agricultural activities, it is also recommended that restoration activities occur within the buffer and/or riparian area to improve habitat quality. However, as Wetlands (W1) and (W2) are considered to have low ecological value, they may not require compensation under the Provincial Wetland Restoration/Compensation Guide (Alberta Environment 2007). If compensation is required it should be done in accordance with Alberta Environment policies and procedures.

3.3 EXISTING AND SURROUNDING USES

3.3.1 Existing Land Uses

The existing land uses within the plan area are identified in **Figure 4.0 – Natural Site Features and Man-Made Constraints**. The majority of the plan area is agricultural land, with few permanent structures. There are no other land uses in the plan. Specific land uses within the plan area include:

Agriculture

The majority of land in the plan area is under agricultural cultivation. There are no horticultural or intensive livestock operations within the plan area.

Residential

There are five existing dwellings within the plan area, each located on separately titled parcels. Associated with each parcel are also numerous outbuildings (i.e. grain silos, workshops, garages,

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

etc.). Two of the homes have access directly to Range Road 220, while the other three access directly to Township Road 550.

3.3.2 Surrounding Land Uses

Land uses to the east and south are within the jurisdiction of Strathcona County, while land uses to the north and west are within the City of Fort Saskatchewan.

Agriculture

The predominant land use to the south and east are agricultural in nature. Directly adjacent to the plan area are five existing farmsteads accessing either Range Road 220 or Josephburg Road.

Industrial

Heavy industrial uses are located to the north and west of the plan area. These uses are petrochemical in nature and include multi-national firms such as Dow Chemical Canada Inc, Agrium Inc and Sherritt International. These industries cover a large number of hectares and are accessed directly from Highway 15.

To the south-west of the plan area is the Alsten Lands Outline Plan area, containing a mixture of light and medium industrial lands on parcels ranging from 1.0 ha to 2.0 ha in area. Servicing of the first phase of development began in the summer of 2008, with a total plan area consisting of approximately 123 ha (304 ac).

3.3.3 Natural Constraints

There are few natural constraints to development of these lands for development purposes. The constraints as they are present today include:

Drainage Course

As detailed in Section 3.2, there is an existing drainage course which traverses from the southwest corner of the SW ¼ 1-55-22-W4M to the east of the SE ¼ 1-55-22-W4M. The channel is a tributary of Ross Creek, originating further east in Strathcona County, and has been heavily modified and re-channeled over time. The drainage course continues for approximately 1 mile into the southeastern portion of the plan area, eventually crossing to the south side of Township Road 550 (outside of the plan area) and continuing west until teeing into Ross Creek approximately 1km south of the North Saskatchewan River.

Based on the preliminary findings of the Ecological Assessment outlined in Section 3.2, further investigation will be required if modifications are proposed to the drainage course (i.e. with respect to compensation). At a minimum, appropriate setbacks to the channel should be observed based upon the minimum requirements of the Municipal Government Act and the Subdivision and Development Regulations at the time of detailed Outline Plan preparation. As required by the City,

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

geotechnical investigations should be undertaken prior to subdivision to determine the suitability of the lands adjacent to the channel for more intensive industrial development.

Wetlands/Treed Area

As discussed in Section 3.2, there is an existing tree stand and three wetland areas existing in the plan area. The wetland areas have been heavily modified over time and are deemed to have low ecological value. The tree stand has been deemed to be a sustainable and permanent feature on the landscape and is recommended for retention.

3.3.4 Man-Made Constraints

The significant man-made constraints within the plan area are shown on **Figure 4.0 – Natural Site Features and Man-Made Constraints** and are detailed below:

Oil, Gas and Petro-Chemical Pipelines

There are numerous pipeline right-of-ways that pass through the plan area. In total there are seven (7) distinct pipeline corridors, containing a total of 22 separate pipelines. **Table 1 – Pipeline Information Summary** provides additional details regarding the pipelines.

The pipeline corridor identified as “A” contains three operating ATCO Gas and Pipelines Ltd natural gas pipelines, is situated within an approximately 15m right-of-way and is located in the extreme southwest corner of the plan area.

Pipeline corridor “B” contains two operating ATCO Gas and Pipelines Ltd natural gas pipelines and is located within a 6m wide right-of-way. This right-of-way is approximately 20m north of the north boundary of Josephburg Road and extends east to west for approximately 2 miles along the south portion of the plan area.

Pipeline corridor “C” contains three operating Keyera Energy Ltd high pressure pipelines within a right-of-way that varies in width from 6m to 18m. A portion of the corridor is within an existing power line right of way, and then bends to the northwest where it crosses to the north side of Highway 15.

The largest pipeline corridor (identified as “D” and “E”) originates in the south central part of the plan area and is approximately 66m in width, diverging into two separate corridors (48m in width to the north and 32m in width to the north-east). This corridor contains pipelines belonging to Nova Chemicals, BP Canada Energy Company, Alberta Ethane Development Company, Suncor, Praxair, Shell Canada and Access Pipeline. All of the pipelines contain products that are under extremely high pressures.

Pipeline corridor “F” contains two operating BP Canada Energy Company high pressure pipelines within an 18m wide right-of-way. The corridor enters the plan area from east of Range Road 220,

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

crossing through the centre of the treed area, before bending sharply to the north and crossing Highway 15.

Pipeline right-of-way “G” contains one operating Praxair high pressure pipeline within a 5m wide right-of-way. The corridor enters the southwest corner of the plan area from south of Josephburg Road, heading west before crossing to the north side of Highway 15.

Air Products is currently in the application process to receive approval for the construction of two pipeline segments through the plan area, beginning at their existing facilities in the refinery row area of Strathcona County just outside Edmonton to end at the Williams Energy (Canada) Inc. facility north of the ASP. A significant portion of the right-of-way proposed is adjacent to the existing “D” and “E” pipeline corridors, along with a lateral portion adjacent to the “G” corridor. The pipelines would be to transport hydrogen gas containing no hydrogen sulphide. These alignments will be confirmed upon approval by the ERCB and will be respected within any Outline Plan or detailed subdivisions which are affected.

As the ASP is adjacent to heavy industry and contains a significant number of existing pipelines, it is important to recognize the potential for additional pipeline infrastructure within or adjacent to existing corridors within the plan area.

Historically, the City has provided policy direction regarding the joint use of utility corridors. This ASP continues to encourage the joint use of utility pipeline corridors, transmission lines, and other utility right-of-ways and structures of a compatible nature to minimize adverse visual, environmental, or safety impacts, as well as to minimize the fragmentation of properties.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Table 1: Pipeline Information Summary

License #	Licensee	Status / Substance	Operating Pressure (kPa)	Utility Right of Way Plan	Map Reference ID
1826-59	ATCO Gas and Pipelines (South)	Abandoned/ Natural Gas	0	Gasline R/W 4729HW	Pipeline Corridor "A"
4932-1	ATCO Gas and Pipelines (South)	Operating/ Natural Gas	4,450		
1826-61	ATCO Gas and Pipelines (South)	Operating/ Natural Gas	3,450		
1826-24	ATCO Gas and Pipelines (South)	Operating/ Natural Gas	3,450		
9453-1	ATCO Gas and Pipelines (South)	Operating/ Natural Gas	3,450	Gas Pipeline R/W 3014KS	Pipeline Corridor "B"
1826-25	ATCO Gas and Pipelines (South)	Operating/ Natural Gas	3,450	Gas Pipeline R/W 3014KS	Pipeline Corridor "B"
8594-6	Keyera Energy Ltd	Operating/ High Vapour Pressure Products	4,960	Products Pipeline R/W 1913TR	Pipeline Corridor "C"
8478-6	Keyera Energy Ltd	Operating/High Vapour Pressure Products	6,230		
8588-7	Keyera Energy Ltd	Operating/High Vapour Pressure Products	4,930		
34335-1	Suncor Energy Inc	Operating/Carbon Dioxide	7,290	Pipeline R/W 952 5272 Petroleum Products Pipeline R/W 822 1189 Oil Pipeline R/W 3859NY	Pipeline Corridor "D"
27896-3	Praxair Canada Inc.	Operating/Miscellaneous Gases	6,800		
19780-23	Shell Canada Limited	Operating/Low Vapour Pressure Products	9,930		
19780-4	Shell Canada Limited	Operating/Low Vapour Pressure Products	9,930		
46674-30	Access Pipeline Inc.	To be constructed/Low Vapour Pressure Products	9,930		
43179-1	Nova Chemicals Corporation	Operating/High Vapour Pressure Products	9,930	Oil Pipeline R/W 5278TR Gas Pipeline R/W 792 2504	Pipeline Corridor "E"
14763-36	Nova Chemicals Corporation	Operating/High Vapour Pressure Products	9,930		
9570-1	BP Canada Energy Company	Operating/High Vapour Pressure Products	9,930		
13023-16	Alberta Ethane Development Company	Operating/High Vapour Pressure Products	9,930		
16967-1	Nova Chemicals Corporation	Operating/High Vapour Pressure Products	9,930		

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

License #	Licensee	Status / Substance	Operating Pressure (kPa)	Utility Right of Way Plan	Map Reference ID
22037-1	BP Canada Energy Company	Operating/Low Vapour Pressure Products	9,930	Light Hydrocarbons Liquids Pipeline R/W 5053TR	Pipeline Corridor "F"
9570-11	BP Canada Energy Company	Operating/High Vapour Pressure Products	9,930		
27896-2	Praxair Canada Inc	Operating/Miscellaneous Gases	6,800	Pipeline R/W 952 1455	Pipeline Corridor "G"

Powerline / Telephone Rights-of-Way

There are two existing powerline right-of-ways within the southwestern portion of the plan area, and as described in **Table 2 – Utility Right of Way Information Summary**. Powerline right-of-way "H" is situated within a 30.5m wide corridor and contains a series of 2 parallel above ground transmission towers which originate at an existing sub-station at the northwest corner of the 114 Street / Highway 15 intersection. The transmission lines extend from the sub-station east through the Alsten Lands Outline Plan, then turn northwards, crossing Township Road 550 and entering the plan area approximately 400m east of Range Road 223 and in the approximate centre of the S.W.¼ Sec. 2-55-22-W4, where they proceed northwards and cross to the north side of Highway 15 and continue to the northeast paralleling the CN Railway mainline.

The second powerline right-of-way, identified as "I", is an underground powerline contained within a 9.2m wide corridor in the extreme southwestern corner of the plan area. The right-of-way enters the plan area from the corner of Lot 1, Block 1, Plan 0820100 and travels in a northeast direction through the S.W.¼ Sec. 2-55-22-W4, ultimately crossing Highway 15 and continuing north.

It is forecasted that there will be a major increase in the demand for electricity as industrial development continues in the Heartland area. As such, the electrical transmission system was expanded to meet this growing demand. The expansion of the transmission system involved the construction of a new high voltage transmission line connecting power generation in the Keephills / Wabamun area (west of Edmonton) to the Heartland region. The transmission line runs within the transportation utility corridor (TUC) through southeast Edmonton and along the western boundary of Strathcona County (west of Sherwood Park), the transmission line then extends north from the TUC into Sturgeon County finally terminating at the Heartland substation. The transmission system is now energized and is capable of supply electricity to development in the region.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Right-of-way “J” is an existing buried, telephone cable within a 5m wide right-of-way. The corridor begins in the plan area at the intersection of Range Road 221 and Highway 15, traveling southward on the west side of N.W.¼ Sec. 1-55-22-W4 for approximately 1km, eventually heading east 1.6km and out of the plan area.

Table 2: Utility Right of Way Information Summary

Owner	Utility Right of Way Plan	Map Reference ID
Altalink Management Ltd	Powerline Right of Way 6661KS	Utility Corridor “H”
Altalink Management Ltd	Powerline R/W 1110RS	Utility Corridor “I”
Alberta Government Telephones	Buried Cable R/W	Utility Corridor “J”

Utility Right-of-Ways

The plan area does not contain any municipal infrastructure improvements such as sanitary, storm or water mains.

The developing Alsten Lands Outline Plan light and medium industrial park to the southwest of the plan area will eventually construct some of the infrastructure required to support servicing within the plan area. Approximately 189.44 ha of the plan area west of Range Road 221 can be fully serviced; while the remaining plan area east of Range Road 221 will have a reduced level of servicing such as a trickle feed water supply, and a low pressure sanitary sewer network.

Highways and Roadways

The plan area is bound on the north and west by Highway 15, a four lane divided urban expressway and part of the provincially designated high load corridor system. Access to Highway 15 from the plan area is currently available from three separate intersections located at Range Roads 220, 221 and Township Road 550.

The south boundary of the plan area is Township Road 550, an improved, two lane rural road under the City of Fort Saskatchewan’s municipal jurisdiction. The roadway terminates slightly west of the plan area where it intersects with Highway 15. The eastern boundary of the plan area is Range Road 220, an unimproved two lane rural roadway. The municipal boundary between the City of Fort Saskatchewan and Strathcona County is on the west side of the Range Road 220 right-of-way. All access to Range Road 220 is controlled solely by Strathcona County. The only internal roadway is Range Road 221, again an unimproved two lane rural roadway, extending north-south in the ASP.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Josephburg Airport

A majority of the eastern and central portions of the plan area are affected by Strathcona County's Josephburg Airport Vicinity Overlay zone. This overlay zone applies restrictions to areas in proximity to the airport to buildings and structures and ensures continuing flight safety and air navigation for the airport by prohibiting uses within the zone that would cause excessive radio interference, conflict with aircraft movements, create a fire or explosives hazard, accumulate any material or waste edible by, or attractive to, birds or development that may create glare or lighting that interferes with lights necessary for aircraft landing or taking off.

3.3.5 Parks, Recreation, Open Space and Trails

There are currently no developed parks, recreation facilities or trail systems within the plan area. There is potential for extending the City's primary trail network into the plan area, as identified in the Recreation, Culture and Parks Facilities Master Plan document. Further details should be considered through development of subsequent Outline Plans.

3.4 TRANSPORTATION

The following discussion outlines the existing transportation characteristics of the plan area including highways and roadways.

3.4.1 Highway 15

Highway 15 is a four lane divided urban expressway with a concrete median separating opposing traffic for the majority of its length adjacent to the plan area. The Highway is the main transportation route into the City from the north, intersecting with Highway 21 within the City limits. At the Highway 21/Highway 15 junction, Highway 15 continues to the northeast, accommodating the major transportation into the Heartland area with a 70km/h posted speed limit. In 2005 the Average Annual Daily Traffic west of Range Road 220, as it traveled parallel to the plan area, was in excess of 9,300 vehicles. There are four existing traffic signals impacting the plan area along Highway 15 at the intersections with Township Road 550, the Dow main gate (east of 119 Street), Range Road 221, and Range Road 220.

The Strathcona Area Industrial Heartland Transportation Study Update was completed for Strathcona County by Stantec in 2007. Although the area studied was for industrial lands within the boundaries of Strathcona County, the intersection of Highway 15 and Range Road 220 is the eastern boundary of the City and the Highway is an important link into the region, and as such was impacted by this study.

The study, as it affects this ASP, was intended to develop a conceptual major internal road network for the area and establish the characteristics of the road network (major intersection configurations, right-of-ways). The issues identified in the study concluded that heavy traffic

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

volumes on Highway 15 in peak hours cause significant delay at signals through Fort Saskatchewan, and that maintaining reasonable traffic flows through the City is desired.

Key findings and recommendations highlighted in the study indicated the intersection of Highway 15 with Range Road 220 was a location for potential major intersection improvements. The intersection improvements identified for the intersection of Highway 15 with Range Road 220 have since been completed. Eventually as development evolves, a grade separated interchange at the Highway 15/Range Road 220 intersection would be required to provide adequate capacity for the long-term Daily Operational and major Turnaround traffic demands.

Based on the study, it was recommended that at a minimum traffic signals through the study area as well as through the City be controlled through a centralized traffic control system. Although not a consideration of the study, a more robust approach would be the development of a free-flow by-pass route around the City.

Strathcona County has also undertaken a Functional Planning Study of the Highway 15 corridor between Range Road 220 and Highway 830 north in order to determine medium and long-term needs of the highway, inclusive of future grade-separated intersection locations and their possible footprints. This study is ongoing.

3.4.2 Fort Saskatchewan By-Pass

The Capital Region 10-Year Provincial Highways Plan, a follow up on the Capital Region Integrated Growth Management Plan Report, was released by the Alberta Government to the public in August 2008. The document outlined the province's transportation funding policy, the municipal grant programs available and the 10-Year highways plan. The plan recognized the province's priorities with respect to Capital Region highway improvements. Within and in proximity to the plan area, the plan outlined the following improvements that directly or indirectly impact the plan area:

- Intersection upgrade at the Highway 15 / Range Road 220 intersection;
- The Fort Saskatchewan by-pass highway, consisting of a new 4-lane roadway and two new interchanges; one at the south end of the City intersecting with Highway 21 and another at the intersection of the proposed by-pass with Highway 15 and Secondary Highway 830 east of the City.

The proposed Fort Saskatchewan by-pass south of the plan area presents possible opportunities to access the south part of the plan, thus creating additional prospects for end users and increasing accessibility. The by-pass alignment remains conceptual at this time; however traffic modeling undertaken by the City indicates that the bypass will be required by 2022. As development progresses eastwards in the plan area a more detailed right-of-way may be available, and therefore prior to approval of an Outline Plan(s) for the eastern portion of the plan

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

area, the alignment of the by-pass should be confirmed to ensure consistency with regional transportation facilities.

It should be noted that the Capital Region Board Integrated Regional Transportation Master Plan identifies a new river crossing south of Fort Saskatchewan, connecting Highway 28 with Highway 21 and Township Road 540. This river crossing and roadway may, at some future point, constitute part of the Fort Saskatchewan by-pass. For more information see Figure 3 of the Integrated Regional Transportation Master Plan.

3.4.3 Township Road 550

Township Road 550 is currently a paved two-lane, rural cross-section roadway, forming the southerly boundary of the plan area. The roadway has been designated as an arterial roadway classification by the MDP, however at this time it operates as a rural roadway.

3.4.4 Range Road 221

Range Road 221 is an 8m wide paved rural road that is contained within the original 20m (66 ft.) government road allowance. The ASP development concept proposes to re-align the majority of Range Road 221, ultimately requiring that the majority of the right-of-way be closed and consolidated with adjacent parcels. The existing intersection locations at Highway 15 and Township Road 550 will remain, and will be upgraded as development warrants and as per future transportation assessment and analysis.

3.4.5 Range Road 220

Range Road 220 is currently an 8m wide gravel rural road that is contained within the original 20m (66 ft.) government road allowance and is operated and maintained by Strathcona County. The MDP identifies this with an arterial roadway designation, however at this time there is a minimal volume of traffic. As development progresses towards this part of the plan area, transportation analysis and consultation with Strathcona County will determine the required width of the roadway, cross section, right-of-way requirements and any access opportunities.

3.4.6 Dangerous Goods Routes

Highway 15 is the only designated dangerous goods route in proximity to the plan area.

3.5 MUNICIPAL SERVICES

The City of Fort Saskatchewan Conceptual Servicing Study, Final Report, June 2006, was prepared by Stantec Consulting Ltd. to provide a conceptual level servicing study for the Alsten Lands Outline Plan, as well as the area contained within this ASP. The conceptual study addressed water, sanitary and storm servicing issues within these areas. As part of the review and update to the ASP in 2013/2014, a review of the Servicing Study was completed, and an

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

alternate servicing concept was developed by Scheffer Andrew Ltd. The revised servicing options are detailed in Section 4.8.

3.6 EMERGENCY SERVICES

3.6.1 Fire, Police, Ambulance and Disaster Services

The City of Fort Saskatchewan's Disaster Services Agency provides fire and disaster services to the plan area, while emergency medical services are provided by Alberta Health Services. The plan area is covered by the City's Fire Hall at the corner of Highway 15 and 101 Street (approximately 2 miles southwest of the plan area), where part-time fire members, under the direction of a full-time Fire Chief, respond to incidents involving the need for fire suppression, rescue, (motor vehicle collisions, low angle rope rescue), clean up of environment spills or mutual aid to surrounding communities. Two fully equipped ambulances are also located at the main fire station to provide emergency medical services to the City.

Additionally, Strathcona County's Heartland Hall (Station #4) is located approximately 2 miles north-east of the plan area along Highway 15. This station provides full-time protection for heavy industry in the area, with advanced industrial level fire and dangerous goods support.

The RCMP provides police services to the City of Fort Saskatchewan from the central station located west of Highway 15 on 99 Avenue. Additionally, Heartland Hall also serves as a satellite office for the RCMP, special constables and bylaw officers working in north Strathcona County.

The City of Fort Saskatchewan Disaster Services Agency works closely with industrial and municipal partners and all other stakeholders to prevent incidents from occurring that could negatively impact our personal safety.

3.6.2 Emergency Preparedness

The City of Fort Saskatchewan is a member of Northeast Region Community Awareness and Emergency Response (CAER), a partnership of more than 40 community-minded industries and municipalities dedicated to emergency response and education initiatives in the region.

A key initiative for the organization is the Community Notification Program. This program consists of (1) an information line residents can call to hear current information about site activities, and (2) notification system Northeast Region CAER members can use to contact residents to notify them of more serious situations in their immediate vicinity.

3.7 INDUSTRIAL RISK ASSESSMENT

The City of Fort Saskatchewan retained the services of Doug McCutcheon and Associates Consulting to undertake an Industrial Risk Assessment (IRA) for the plan area in 2009. The objective of the IRA was to identify and assess what types and levels of risk are in proximity to

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

the plan area as a result of existing and potential heavy industrial activities. The purpose of the assessment was to provide recommendations as to the type and intensity of uses that could be appropriate in the plan area. Summary and selected excerpts from this analysis are provided below.

The plan area is in proximity to, and potentially impacted by, industrial activity from several companies in the area, including Sherritt Gordon, Marsulex, Shell Canada and Dow Chemical. In addition, nearby railway and roadway infrastructure are used to transport numerous hazardous materials to and from these sites. The IRA characterizes the risk associated with these into five separate activities that could create incidents that would be typical for these industries, including (1) toxic release, (2) flammable release, (3) explosion damage, (4) pipeline incidents and (5) a boiling liquid expanding vapour explosion (BLEVE).

The analysis for risk involves the consequence of an incident and the probability of it happening. The result is calculated in terms of the potential for fatalities and then compared to an acceptable level of risk as defined through the Major Industrial Accidents Council of Canada (MIACC) criteria for risk based land use planning.

The MIACC risk acceptability criteria describes the level of risk for a member of the public who is inadvertently exposed to an industrial incident must be better than a 1×10^{-6} (1 in a million) chance of a fatality. However as one moves closer to the risk source, the level of risk increases, until it reaches the maximum allowable of 1×10^{-4} (100 in a million) chance of a fatality.

The IRA provides a breakdown of the probability that an industrial accident would occur in proximity to the plan area, and concludes that these values are within an acceptable range for light industrial and commercial zoning according to the MIACC criteria. Sound design, collection of leaks, fire protection systems, operational procedures, emergency planning and other activities will serve to effectively manage the risk to that acceptable level.

The IRA concludes that each of the five scenarios outlined above would have varying degrees of impact on the plan area, as each can create consequences that can potentially cause fatalities. In order to illustrate these, the IRA provides “risk contours” that define the risk level based on distances of 0 km, 1.5 km and 3.5 km from the property line of adjacent industry. For example a 1×10^{-4} (100 in a million) risk contour corresponds to 0 km at the property line of industry. At a distance of 1.5 km from the property line would be the 1×10^{-5} (10 in a million) risk contour. At 3.5 km from the heavy industry property line is the 1×10^{-6} (1 in a million) risk contour.

Based on the MIACC risk based land-use criteria, the IRA recommends that allowable land uses in the plan area be as follows:

- 0 km to 1.5 km (1×10^{-4} to 1×10^{-5}): light to medium industrial land uses such as manufacturing, warehousing and open space (e.g. parkland, golf courses, etc.)
- 1.5 km to 3.5 km (1×10^{-5} to 1×10^{-6}): commercial and office type uses

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Thus, in order to minimize risk, commercial and office type uses should be no closer than 1.5 km to the property line of heavy industry, and light or medium industrial uses to be primarily between 1.5 km and heavy industry.

The IRA recommends the 1.5 km and 3.5 km as appropriate separation distances for managing land uses in the plan area. However, there is an opportunity to incorporate a limited amount of highway commercial uses within the 0 km to 1.5 km risk contour provided that certain safety measures are implemented in the design, construction and operation of the buildings. For example, new development could be designed and built (e.g. structural, mechanical, HVAC, etc.) to have a calculated risk equal to or lower than the 1×10^{-5} risk criteria at the 1.5 km distance. Site/development-specific risk assessment would demonstrate to the City's satisfaction that the lesser setback is warranted by the incorporation of additional safety measures. Other safety measures may include provision of in-place sheltering, shielding, evacuation programs and ventilation shut-off systems.

As part of the Josephburg Road North Industrial Area Structure Plan review and update, the City of Fort Saskatchewan retained the services of Doug McCutcheon and Associates Consulting, to undertake a Risk Review of the proposed amendments to the plan area in 2014.

The Risk Review reiterates that medium to light industrial uses are appropriate for the plan area and the Highway 15 Vicinity Overlay. Should uses such as office buildings be incorporated into the Highway 15 Vicinity Overlay, safety measures such as; HVAC control and isolation systems using monitors to initiate shutdown, windows that cannot open, and doors capable of sealing tightly, should be incorporated into the building design in order to provide an effective means of sheltering in place should a toxic release happen.

The Risk Review identifies no concerns around acceptable levels of risk in relation to utilizing a trickle water system to service medium to light industrial uses; keeping in mind that a separate fire water system will be utilized for the areas serviced by the trickle water system.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.0 Land Use Concept, Objectives and Policies

4.1 DEVELOPMENT CONCEPT

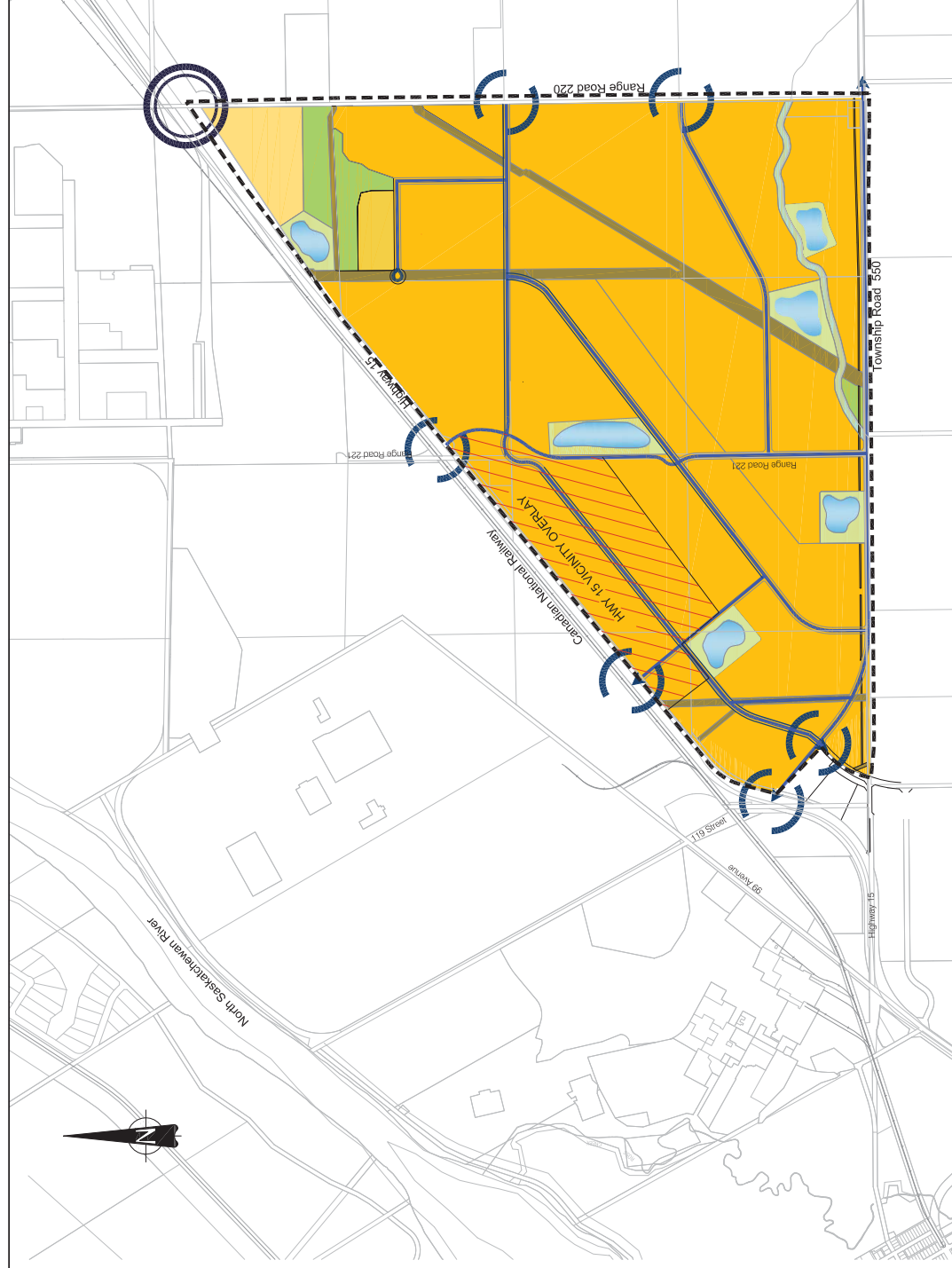
A Development Concept is shown on **Figure 5.0 – Land Use Concept**, illustrating a conceptual land use and development framework for the preparation of more detailed Outline Plans and design briefs that need to be approved by the City of Fort Saskatchewan prior to consideration of detailed subdivision and zoning applications.

The Development Concept and associated goals, objectives and policies for the Josephburg Road North Industrial ASP have been determined with regards to the following:

- Policies contained within the City of Fort Saskatchewan MDP, the City of Fort Saskatchewan's Alberta's Industrial Heartland ASP and other relevant plans and studies;
- Natural and man-made constraints;
- Existing and planned improvements to the transportation network;
- Utility servicing constraints and opportunities;
- Economic and strategic development goals of the City of Fort Saskatchewan;
- Environmental considerations;
- Long range planning practices that support logical, cost-effective and beneficial development.

Figure 5.0
Land Use Concept

-  Medium Industrial District
-  HWY 15 Vicinity Overlay
-  Municipal Reserve
-  Future Road Right-of-Way
-  Pipeline/ Utility Corridor
-  Public Utility Lot
-  Stormwater Management Facilities
-  Proposed All Direction Access
-  Potential Interchange
-  ASP Boundary
-  Roadway



SCALE 1:20000



CITY OF
FORT SASKATCHEWAN
ALBERTA

Josephburg Road North Industrial Area Structure Plan

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.2 INDUSTRIAL DEVELOPMENT

Goal: To create a functional and sustainable industrial area in which future development is complementary to, and compatible with, the surrounding heavy industry and agricultural uses, roadway network, and important site elements.

Objective	ASP Policy	Implementation
4.2.1 To provide for medium industrial land that will be available to meet the diverse needs of prospective industries.	4.2.1(a) Lands in the ASP designated for medium industrial development will be accommodated through application of the IM-Medium Industrial District.	4.2.1(a) Figure 5.0 – Land Use Concept will guide the future application of medium industrial land uses.
	4.2.1(b) To allow flexibility in the size of parcels in order to accommodate the space requirements of respective users.	4.2.1(b) The size of the parcels required to accommodate user demands shall be pursued at the Outline Plan, rezoning and subdivision phases.
	4.2.1(c) To provide industrial and business opportunities to serve the employment needs of the Industrial Heartland, the City of Fort Saskatchewan and the Capital Region.	4.2.1(c) Uses which are permitted within the existing IM-Medium Industrial District will be applied.
4.2.2 Locate and orient industrial parcels along roadways to take advantage of the high visibility and convenient access opportunities.	4.2.2 To ensure internal roadway circulation increases visibility and provides appropriate accesses to the industrial parcels.	4.2.2 <ul style="list-style-type: none"> • Figure 5.0 – Land Use Concept illustrates industrial uses with access and frontage along the internal roadways. • The internal roadway circulation pattern will be refined and confirmed at the Outline Plan, rezoning and subdivision phases with regards to the development and staging pattern and logical extension of infrastructure, and may require the completion of detailed

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
		Transportation Impact Assessments (TIA).
4.2.3 To apply an appropriate buffer width to protect the existing drainage course from encroaching industrial development.	4.2.3 To protect the existing drainage course through the plan area.	4.2.3 Appropriate buffers, setbacks and/or landscaping requirements will be determined at the Outline Plan, subdivision or development permit stage at the discretion of Planning & Development Services.
4.2.4 To encourage industrial land uses that support the Alberta's Industrial Heartland, are complementary and compatible with Heavy Industry and have adequate infrastructure to meet current and future industrial needs.	4.2.4 Development shall be intended primarily for uses that are complimentary to heavy industrial land uses in proximity, with secondary emphasis on limited highway commercial uses.	4.2.4 Development shall comply with the IM-Medium Industrial District of the Land Use Bylaw.

Discussion

The land use identified for the entire ASP, consisting of approximately 590 ha (1,459 ac) ha (ac) of land, is for medium industrial purposes. Medium industrial lands are situated as a buffer between the existing heavy industrial development north of Highway 15 and the agricultural lands to the south and east, which is in Strathcona County's boundaries. Note that the lands subject to the Medium Industrial - Highway 15 Vicinity Overlay may permit uses that differ from those traditionally found in medium industrial areas. The Land Use Bylaw currently specifies a minimum parcel size of 0.4 ha (0.99 ac) for medium industrial development.. Parcel sizes will be identified at the Outline Plan stage, approved through the subdivision process and confirmed at time of legal plan endorsement. The plan intends to allow landowners the flexibility to create a variety of parcel sizes, which are appropriate to the district and would facilitate the creation of economical and marketable industrial land development.

Medium industrial uses are located such that access and frontage are along the internal roadway system to increase site visibility and provide for appropriate access opportunities. Where possible direct access to either Township Road 550 or Range Road 220 may be possible, and would require consultation and/or approval with Strathcona County and the City of Fort Saskatchewan.

The existing drainage course in the south-east portion of the plan area has been heavily modified and re-channeled over many years. As it carries drainage flows from beyond the plan area (Strathcona County) to the North Saskatchewan River, it should be protected from abutting development. The plan identifies and provides policy for its protection and the application of

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

appropriate buffering and setbacks, determined through the Outline Plan and subdivision application processes. A more detailed analysis may be required prior to subdivision approval.

While the focus of the plan is to facilitate primarily industrial development, a limited amount of commercial oriented land uses may be accommodated within a Medium Industrial – Highway 15 Vicinity Overlay designation. The plan illustrates conceptually the location where the Overlay may be applied in consideration of visibility and accessibility from Highway 15 and where access will not interfere with the flow of traffic intended for industrial purposes. In order to accommodate commercial oriented land uses within the Medium Industrial - Highway 15 Vicinity Overlay it may be necessary to prepare a risk assessment to support proposed commercial oriented development.

The conceptual locations for medium industrial activities are illustrated on ***Figure 5.0 – Land Use Concept***.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.3 MEDIUM INDUSTRIAL – HIGHWAY 15 VICINITY OVERLAY

Goal: To provide opportunities for limited commercial oriented land uses, adjacent to Highway 15, which are compatible and complementary to heavy industrial development in proximity to the plan.

Objective	ASP Policy	Implementation
<p>4.3.1</p> <p>To provide opportunities for limited commercial oriented land uses immediately adjacent to Highway 15.</p>	<p>4.3.1(a)</p> <ul style="list-style-type: none"> • Medium Industrial will be the primary land use in this area. • A Highway 15 Vicinity Overlay, with special provisions, will allow for the development of specific commercial oriented uses on lands designated for Medium Industrial. 	<p>4.3.1(a)</p> <ul style="list-style-type: none"> • Figure 5.0 – Land Use Concept will conceptually guide the future application of medium industrial uses, with provisions for limited commercial oriented land uses. • An update to the Highway 15 Vicinity Overlay in the Land Use Bylaw shall be prepared to reflect the amendments to this plan.
	<p>4.3.1(b)</p> <p>Commercial oriented development will only be considered within the area identified as “Highway 15 Vicinity Overlay”, and shall not exceed ten percent (10%) of the gross developable area.</p>	<p>4.3.1(b)</p> <ul style="list-style-type: none"> • Development shall comply with the Highway 15 Vicinity Overlay of the Land Use Bylaw. • The Highway 15 Vicinity Overlay shall be limited to 10% of the gross developable area.
<p>4.3.2</p> <p>To prohibit commercial developments which are not compatible with heavy industrial uses.</p>	<p>4.3.2</p> <ul style="list-style-type: none"> • Consideration of commercial oriented uses shall ensure compatibility, from a risk management perspective, with adjacent heavy industrial activities. 	<p>4.3.2</p> <p>Planning & Development Services shall review all commercial oriented development against the Highway 15 Vicinity Overlay and at the Development Permit stage.</p>
<p>4.3.3</p> <p>To implement risk management procedures to ensure that commercial oriented development does not restrict the operation or expansion of heavy industrial uses in proximity to the plan area.</p>	<p>4.3.3</p> <ul style="list-style-type: none"> • Appropriate safety measures may be required for new buildings in the plan area to ensure that the operation or expansion of adjacent heavy industrial uses will not be 	<p>4.3.3</p> <p>The requirement for risk assessments/analysis of risk management programs/implementation of safety features shall be determined by Planning and Development</p>

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
	<p>compromised by commercial oriented uses.</p> <ul style="list-style-type: none"> • Risk Assessments may be required for any commercial oriented developments to ensure a calculated risk equal to or lower than the 1×10^{-5} risk criteria at the 1.5 km distance. • Additional safety measures may be required and could include provision of in-place sheltering, shielding, evacuation programs and ventilation shut-off systems. 	Services and in consultation with Emergency Services.

Discussion

The land uses parallel and adjacent to the Highway 15 corridor are medium industrial in nature, allowing uses similar to those described in Section 4.2. However, the plan includes provisions for a limited amount of commercial oriented uses under a new Highway 15 Vicinity Overlay. The Overlay would permit a specific range of commercial type uses, at specific locations, within the medium industrial area adjacent to Highway 15. The commercial oriented uses are anticipated to include such uses as animal service facility (minor/major); business/office support services; commercial school; recreational vehicle/heavy equipment sales & rental; and professional services – office, which would require good visibility and accessibility to Highway 15. However, consideration of appropriate uses will ensure they are of the type that does not employ a significant number of people on site and are compatible with heavy industry.

The designation of the Highway 15 Vicinity Overlay within the plan area requires the implementation of a new overlay in the Land Use Bylaw to specifically address the type, intensity, location and risk analysis of compatible uses within this designation.

The areas suitable for these commercial oriented uses are illustrated paralleling Highway 15, and would be accessible only via the internal circulation network, with no direct access to Highway 15 permitted. The plan proposes approximately 63.0 ha, or approximately 11.8% of the gross developable area for potential commercial purposes. The plan area is in proximity to significant heavy industrial activity, both existing and proposed, to the north. The type and intensity of commercial uses adjacent to Highway 15 must be compatible with heavy industry and must be designed and operated to minimize the risk associated with the continued operation of these heavy industrial facilities.

Section 3.7 Industrial Risk Assessment outlines, at a minimum, the requirements for development of commercial oriented uses within the overlay. The intent is for site developments/buildings to be constructed and operated such that they provide safe and efficient evacuation of occupants and/or provide “shelter in place” protection or ventilation shut-off systems against potential risks

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

associated with the heavy industry. Other regulations may be incorporated to ensure adjacent heavy industrial uses will not be compromised by the commercial oriented uses.

Site or development specific risk assessment analysis will be required for each new development within the Overlay to ensure compliance with the requirements of this section and to demonstrate to the City's satisfaction the uses proposed are compatible with heavy industry.

The conceptual location of the Highway 15 Vicinity Overlay and commercial oriented activities are illustrated on **Figure 5.0 – Land Use Concept**.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.4 UTILITY RIGHT OF WAYS

Goal: To ensure the integrity of existing pipeline and utility corridors by incorporating the right of ways, where feasible, into the development fabric of the plan area.

Objective	ASP Policy	Implementation
4.4.1 Protect pipeline and utility right-of-ways from encroachment by abutting uses.	4.4.1(a) Minor utility rights-of-way will be incorporated into development sites and protected by easement.	4.4.1(a) Easements to protect minor utility rights-of-way will be required as a condition of subdivision approval for the affected lands.
	4.4.1(b) Major utility rights-of-ways not already owned by the operator will be dedicated to the City as public utility lots.	4.4.1(b) Public utility lots to accommodate major utility rights-of-way will be required as a condition of subdivision approval for the affected lands.
4.4.2 To create appropriate buffers between operating pipelines and industrial uses.	4.4.2 Require minimum setbacks from industrial land uses and pipeline and utility corridors.	4.4.2 The minimum setback shall be in accordance with the guidelines of the Alberta Energy Regulators and relevant regulations specified in the Land Use Bylaw.
4.4.3 To recognize the potential for additional pipelines within or adjacent to existing corridors within the plan area.	4.4.3 At the Outline Plan stage determine the need for future pipeline infrastructure.	4.4.3 Outline Plans will be circulated to local industry to determine requirements for future pipeline infrastructure.
4.4.4 To integrate, where feasible, portions of the major utility rights-of-way and Storm Water Management Facilities as open space elements.	4.4.4 Portions of the major utility rights-of-way and the SWMF may incorporate multi-use trails and landscaping to enhance these lands as open space areas.	4.4.4 The provision of multi-use trails and associated landscaping will be reviewed at the Outline Plan stage and may be required as a condition of subdivision approval for the abutting lands.

Discussion

The plan area contains numerous pipeline and utility right-of-ways which convey high pressure flammable or volatile petrochemical products and high voltage electricity. These right-of-ways are currently legally registered in the name of the owner/operator of the respective utility and must be

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

protected from encroachment by future uses. In the past, incidents have been caused by third party activities near to the pipelines, and as such these potential impacts must be taken into consideration by respecting existing right-of-ways and development setback distances.

The alignment and location of future pipelines or above ground power transmission lines is not known at this time. As there are a significant number of existing pipeline corridors currently traversing through the plan area, possible future pipelines within or adjacent to these may be considered. Through the Outline Plan and subdivision application stages adjacent industry should be notified to determine if their long range infrastructure requirements affect the land uses proposed for the area. In addition, during the development permit application stage, pipeline operators should be notified, in order to ensure that the specifically proposed development activity does not impact, nor is impacted by the pipeline operations.

Incorporating naturalized landscaping and/or multi-use trails along utility right-of-ways further encourages alternative forms of circulation by providing workers in the area opportunities to walk, cycle or roller-blade at various times during the workday. It also serves to better integrate these corridors into the plan area, while still allowing ease of access to the utility operators for maintenance purposes.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.5 ENVIRONMENTAL MANAGEMENT

Goal: To protect environmentally sensitive areas, where considered sustainable, and promote environmentally responsible and sustainable development practices.

Objective	ASP Policy	Implementation
4.5.1 To encourage the retention, incorporation and enhancement of sustainable wetland areas into the ASP.	4.5.1 Use wetland areas, where feasible, for stormwater management, thereby reducing infrastructure and servicing costs.	4.5.1 As Figure 5.0 – Land Use Concept conceptually illustrates the existing wetlands and drainage course may be integrated into the future stormwater management system, where feasible and environmentally sustainable, to achieve the ASP policy.
4.5.2 To provide adequate buffering between industrial land uses, the existing treed area in the north-east and drainage channel in the south-east part of the ASP.	4.5.2 Require as part of the preparation of the Outline Plan and Design Brief that buffers are integrated adjacent to industrial uses to protect and enhance existing natural features.	4.5.2 The location, width and type of buffer will be determined as part of the preparation of the detailed Outline Plan and Design Brief and to the satisfaction of Planning & Development Services.
4.5.3 To protect the existing treed area in the north-east corner of the plan area.	4.5.3 The treed area will be conserved as a natural area and passive open space.	4.5.3 The tree stand will be acquired by the City of Fort Saskatchewan through municipal reserve dedication, which will be achieved as a condition of subdivision of the affected lands.
4.5.4 Municipal Reserve Dedication	4.5.4 Take 10% of the Gross Developable Area to be subdivided in the form of municipal reserve land or cash-in lieu.	4.5.4 The amount, form (land or cash-in lieu) and location of municipal reserves will be confirmed through Outline Plan and subdivision applications.

Discussion

A preliminary ecological assessment concluded the presence of three wetland areas within the plan area. Although each of these areas have been heavily modified over time, the opportunity for enhancement as storm water management facilities provides an ecologically sound and sustainable method of storm water treatment on site.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

The large contiguous tree stand in the north-east corner of the plan appears to be a sustainable and a permanent feature on the landscape. A variety of wildlife species were observed throughout the stand, either directly or indirectly, including migratory birds and the presence of deer were indicated. In consideration of this, the plan designates the treed area for preservation as municipal reserve, with the exact boundary and area to be determined through the Outline Plan and subdivision process. The location of existing underground pipeline infrastructure in proximity to the treed area should be considered.

Municipal reserves will be primarily required as cash-in lieu at the time of subdivision. However, the preparation of Outline Plans may demonstrate the need for small parks in strategic locations throughout the plan to provide amenity and recreation opportunities.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.6 TRANSPORTATION

Goal: To provide a functional and effective transportation system that supports development of industrial and limited highway commercial uses within the Plan area and to ensure that adequate land area is protected to facilitate major, future transportation improvements (i.e. Hwy 15 / Range Road 220 interchange);

Objective	ASP Policy	Implementation
4.6.1 Provide safe, effective and functional transportation linkages consistent with the City of Fort Saskatchewan's Transportation Master Plan and the Strathcona Area Industrial Heartland Transportation Study.	4.6.1(a) Transportation access and internal circulation in the plan area will be generally consistent with the network of roadways as conceptually shown on Figure 5.0 – Land Use Concept and Figure 6.0 – Transportation Network . Where warranted, and supported through the preparation of a suitable traffic study, existing road rights-of-way may be utilized as part of the future internal road network.	4.6.1(a) Roadway alignments shown on Figure 5.0 – Land Use Concept and Figure 6.0 – Transportation Network are conceptual. Ultimate alignments and cross-sections will be determined through the preparation of detailed Outline Plans and Design Briefs and to the satisfaction of Planning & Development Services, Project Management and, where warranted, Strathcona County and Alberta Transportation.
	4.6.1(b) Access to the plan area from abutting roadways will be generally as illustrated on Figure 5.0 – Land Use Concept and Figure 6.0 – Transportation Network	4.6.1(b) <ul style="list-style-type: none"> • Access locations to Highway 15 and Township Road 550 will be reviewed and approved at the Outline Plan and subdivision stage. • Strathcona County has jurisdiction of Range Road 220 and will review any applications to access from the plan area.
4.6.2 To protect land adjacent to the Highway 15 / Range Road 220 intersection for future right-of-way requirements long-term construction of an interchange.		
	4.6.2(a)	4.6.2(a)

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
	An interchange has been identified in the Capital Region Integrated Growth Management Plan's 35-Year Plan.	The City of Fort Saskatchewan will work with Alberta Transportation and Strathcona County in determining when the interchange is required and the amount of land needed.
	4.6.2(b) Future roadway networks adjacent to the plan area, within the Industrial Heartland, will be further defined.	4.6.2(b) The Heartland Association is currently working on their Transportation Master Plan. Recommendations of this report will be taken into consideration as development progresses in the plan area.
4.6.3 Ensure noise resulting from heavy traffic and along major roadways is taken into consideration and addressed where necessary.	4.6.3 Ensure land uses backing on to Township Road 550 or Range Road 220 provide appropriate landscaping, berms or other enhancements (e.g. building treatments) to assist in noise attenuation and buffering.	4.6.3 Appropriate landscaping, berming and fencing, as required by the Land Use Bylaw, shall be provided along Township Road 550 or Range Road 220 to provide for visual and noise attenuation.
4.6.4 To recognize future pedestrian and bicycle circulation opportunities in the plan area.	4.6.4 A multi-use trail network shall be developed within the plan area utilizing portions of the major utility rights-of-way, stormwater management facilities, the roadway network and the northeast treed area.	4.6.4 The location, alignment and cross-section of a multi-use trail network will be determined through the preparation of detailed Outline Plans and Design Briefs and required as a condition of subdivision approval.
4.6.5 Ensure roadways are built to a standard acceptable to the City of Fort Saskatchewan's Engineering and Service Standards.	4.6.5 A combination of rural and urban roadway cross-sections will be provided. Within the Medium Industrial - Highway 15 Vicinity Overlay urban road cross-sections are preferred.	4.6.5 Roadway standards and subsequent rights-of-way will be determined during the preparation of the Outline Plan and Design Brief and will be dedicated through subdivision.
4.6.6	4.6.6	4.6.6

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
To facilitate the installation of water and wastewater services, in a flexible manner.	Roadway cross-sections (with adjacent easement areas as necessary) are to be designed in order to accommodate the installation of any deferred infrastructure.	Roadway standards and subsequent rights-of-way will be determined during the preparation of the Outline Plan and Design Brief, and will be dedicated through subdivision.

Discussion

The circulation pattern within the plan area provides for the safe and efficient movement of traffic throughout the plan area and beyond. The proposed hierarchy of roadways includes collector and local industrial roadways. Construction of these roadways will be to either a rural standard (e.g. ditches) or full urban standard with curb and gutter, and will be determined at the Outline Plan and Design Brief stage, to the satisfaction of the City. The road network within the area subject to the Highway 15 overlay is intended to be urban in nature, with curb and gutter in order to reflect the more commercial nature of the area. In order to promote walkability, the provision of a 3.0m multi-use trail on at least one side of most roads in the plan area will allow pedestrians the choice to circulate throughout the plan and amenity areas (storm water facilities and open spaces) in a relatively safe and efficient manner.

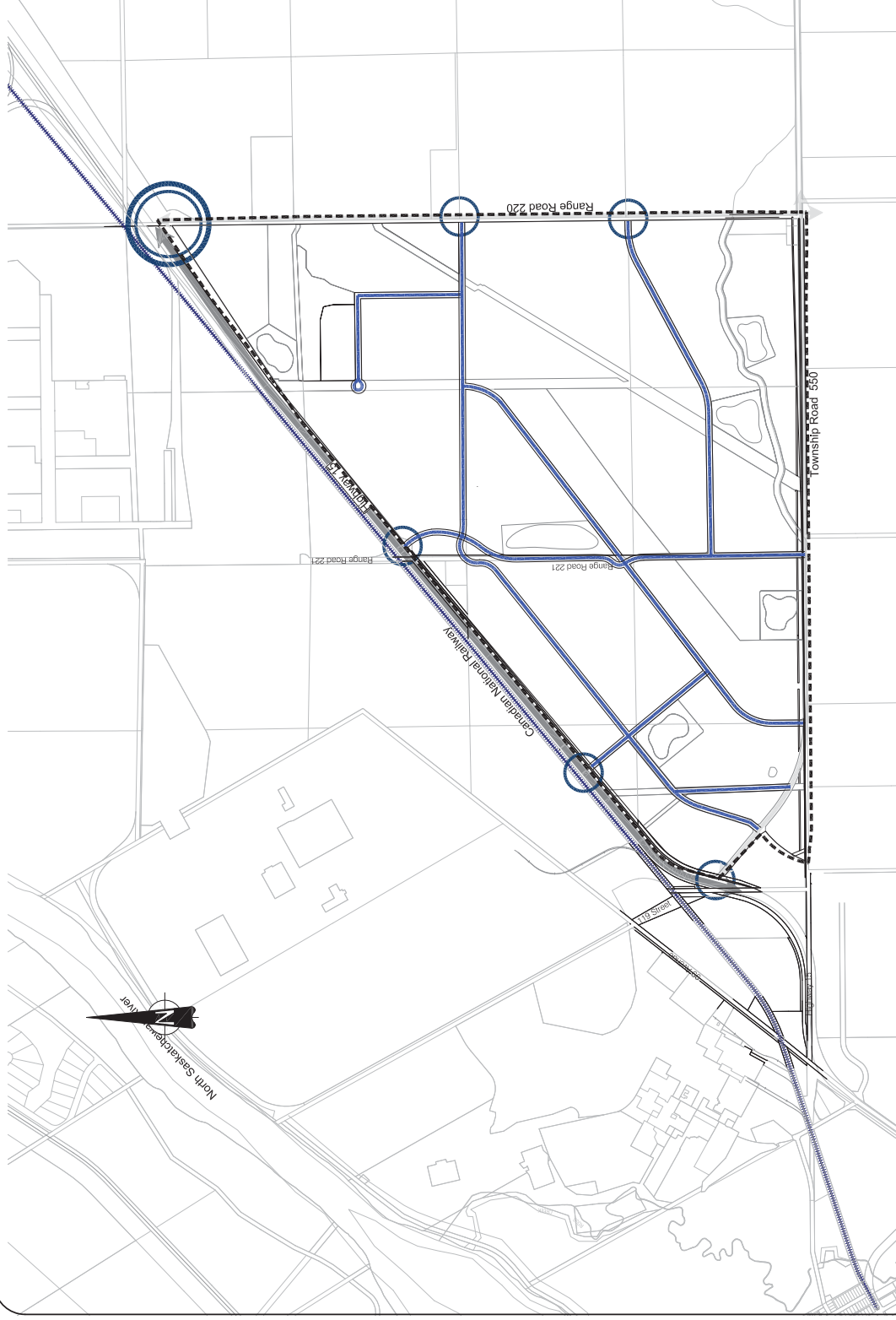
The plan identifies the location of the three major all-directional access points into the plan from Highway 15. The southernmost access is proposed at the intersection of the future re-aligned Township Road 550 and 119 Street. The second access is located at the existing location across from the Dow main gate, while the third access is at the intersection of Range Road 221 (which may be realigned and improved) and the northerly access to the Dow site. Each of these all-directional access points occurs at existing intersection locations, and will provide the main access routes to the plan area.

However, detailed traffic impact assessments will be required through Outline Plans prepared for all lands in this area, which will confirm the exact access type and location, intersection requirements/improvements, traffic signalizations, internal road alignments and cross-sections, to the satisfaction of the City of Fort Saskatchewan.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

Figure 6.0
Transportation
Network



SCALE 1:20000



Josephburg Road North Industrial Area Structure Plan

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Vehicular access to the plan area from Township Road 550 and Range Road 220 is conceptually illustrated **Figure 5.0 – Land Use Concept** and **Figure 6.0 – Transportation Network**. The purpose is to permit all-directional access directly into the plan at regular intervals (i.e. 400 m or greater). Township Road 550 and Range Road 220 are not anticipated to carry significant traffic volumes, and consideration may be given to providing individual parcels direct access at specific locations, at the discretion of the City of Fort Saskatchewan and with regards to detailed traffic assessment analysis. As Range Road 220 is the responsibility of Strathcona County, any proposal which require access to this roadway will require their review.

The Province of Alberta has identified the intersection of Range Road 220 and Highway 15 as the location of a future interchange within the next 35 years. Within the next 10 years, this intersection has been identified for intersection improvements to maintain and improve traffic flow to the Industrial Heartland. Given the long-term horizon of the interchange, it is difficult to determine the extent of land required for a right-of-way and construction. However to protect for this possibility, the lands north of the tree stand (proposed as municipal reserve) have been acknowledged as future road right of way. The objective is to ensure sufficient land is set aside for the Outline Plan stage or the interchange is deemed a priority.

Adjacent to the plan area are lands within Strathcona County primarily used for agricultural purposes, containing few residential dwellings. However, as the plan area builds out, improvements to Township Road 550 and/or Range Road 220 will increase the level of traffic and noise impacting these areas, and industrial development may create negative visual impacts. Consideration for landscaping or noise attenuation within the plan area to mitigate this shall be considered as the lands and roadways develop.

Individuals employed in the plan area may seek opportunities for exercise and passive or active recreation before, during or after work. The inclusion of a 3.0m multi-use trail on at least one side of most roadways, the opportunity to create a separate multi-use trail system, the enhanced landscaping of storm water management facilities and the use of utility or pipeline corridors will provide the necessary means for employees to recreate throughout the day. The specific location and alignment of multi-use trails within utility or pipeline corridors, along roadways and within storm water management facilities will be determined through the Outline Plan, subdivision and development agreement stages.

It is anticipated that the plan area will be accessible via a series of collector and local industrial roadways. The collector roadway network is flexible in nature and is conceptually illustrated in **Figure 6.0 – Transportation Network**, while the local roadway network will be confirmed through the Outline Plan and subdivision processes. Deviations from the conceptual roadway network are to be supported by a suitable traffic study, prepared to the satisfaction of the City of Fort Saskatchewan. The appropriate roadway cross-sections will be confirmed through the development of individual Outline Plans, detailed transportation impact assessments and to the satisfaction of the City of Fort Saskatchewan. Urban roadway cross-sections will be the predominant requirement within the plan area subject to the Highway 15 overlay, with the remainder of the plan area anticipated to consist of rural roadway cross-sections.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.7 SUSTAINABLE DEVELOPMENT PRACTICES

Goal: To promote sustainable development practices within the plan area.

Objective	ASP Policy	Implementation
4.7.1 To consider alternative development standards which promote innovation and high levels of environmental and economic performance.	4.7.1 Minimize environmental impacts through the application of sustainable development practices (i.e. bio-swales as a complement to storm sewers; reduced street lighting levels to reduce energy consumption and light pollution; alternative road construction to accommodate recycled materials, grey water re-use, etc.)	4.7.1 Application of alternative development standards will be reviewed through the preparation of detailed Outline Plans and Design Briefs, and finalized through subdivision approval and associated servicing agreements.

Discussion

Sustainable development practices are intended to be used for site and facility design to take advantage of synergies between various building systems and industrial processes. For development within the plan area, sustainable development guidelines could be applied, where appropriate, at the Outline Plan, subdivision, development agreement or development permitting processes.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.8 MUNICIPAL SERVICING

Goal: To ensure that municipal utility services are provided in accordance with the City of Fort Saskatchewan standards and extended in a logical and coordinated manner to meet the needs of present and future growth;

Objective	ASP Policy	Implementation
<p>4.8.1 To ensure that suitable contributions are made, be they financial or physical, to shared infrastructure within each servicing basin, so as to accommodate the ultimate servicing concepts as outlined within the ASP.</p>	<p>4.8.1(a) A suitable level of service for Sanitary, Storm and Water services will be finalized through subdivision approval and associated servicing agreements with regards to the ultimate servicing concepts outlined within the ASP and the Outline Plans.</p> <p>4.8.1(b) Development within the plan area west of Range Road 221 shall be fully serviced; while development within the plan area east of Range Road 221 shall have a reduced level of servicing. Areas subject to a reduced level of servicing are to be served by a trickle water feed and a low pressure sanitary system. Fire protection will be provided via dry hydrants connected to suitably sized stormwater management facilities.</p> <p>4.8.1(c) A combination of overland storm drainage conveyance and underground systems will be utilized.</p>	<p>4.8.1(a) The ultimate sanitary, storm and water infrastructure systems shall be determined through the preparation of detailed design briefs during the preparation of the Outline Plans.</p> <p>4.8.1(b) Proponents of a subdivision, shall accommodate the ultimate servicing requirements by providing for suitable rights-of-way, and contributing to the overall construction of the system by either constructing municipal infrastructure, or contributing financially, through a municipal levy, for the future installation of municipal infrastructure.</p> <p>4.8.1(c) Design briefs will guide the location and extent of overland conveyance, as well that of the underground system. Underground systems are to be utilized within the plan area subject to the Highway 15 Overlay; while a combination of overland conveyance and underground systems may be used for the remainder of the lands.</p>
	<p>4.8.1(d) Should the end user desire a level of service in conformance with the ultimate servicing concept and</p>	<p>4.8.1(d) Should the end user desire a level of service in conformance with the ultimate servicing plan, and</p>

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
	proposed Outline Plan, the end user shall be responsible for extending the necessary infrastructure.	proposed Outline Plan, they shall provide suitable design drawings to the City and construct the infrastructure to City standards. The end user may recover costs from other benefitting end users, when and if they tie into the infrastructure.
	4.8.1(e) All shallow utility infrastructure required to provide service to development will be located underground, unless otherwise directed by the City of Fort Saskatchewan.	4.8.1(e) Underground utility services will be required as a condition of subdivision and executed through the associated servicing agreements.
4.8.2 To require more detailed servicing information at the Outline Plan and Design Brief stages.	4.8.2 Development and servicing shall proceed in accordance with approved Outline Plans and Design Briefs prepared for the associated lands.	4.8.2 Development shall proceed having regards to the Servicing Concepts as outlined within the ASP and as per Outline Plans and Design Briefs approved for the associated lands. The costs of preparing any required reports shall be borne by the benefiting developer(s).
4.8.3 Provide for a flexible servicing and phasing scheme having regard for the economical and efficient extension of City services and utilities.	4.8.3 Development and servicing should occur sequentially, through the extension of storm, sanitary and water systems; flexibility in the sequence of development and servicing shall be accommodated where warranted by the proponent of a subdivision and where supported by the City.	4.8.3 Development shall occur when it is feasible to do so, balancing the needs of the proponent and that of the City. Development shall adhere to the servicing concepts outlined in the ASP, Outline Plans and Design Briefs.
4.8.4 To utilize natural systems to provide surface drainage and stormwater management, where possible	4.8.4 To re-create natural drainage patterns, where feasible, and utilize such patterns to provide storm	4.8.4 The precise locations, sizes, configuration and number of stormwater sites will be determined

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
	drainage services within the plan area.	at the time of Outline Plan and subdivision plan.
4.8.5 Utilize stormwater management facilities to provide water for fire protection purposes.	4.8.5 Stormwater management facilities shall provide sufficient fire water to service all of the plan area which is not fully serviced as identified in Figure 7.0 – Conceptual Water Servicing .	4.8.5(a) A system of dry hydrants, connected to the stormwater management facilities, shall be defined in the Outline Plan and Design Briefs, to the satisfaction of the City. 4.8.5(b) A system of dry hydrants, connected to suitably sized stormwater management facilities, shall be required as part of any development, and shall be in accordance with the Outline Plan and Design Briefs approved by the City.
4.8.6 To encourage the joint use of utility pipeline corridors, transmission lines and other utility right-of-ways.	4.8.6 Use the pipeline / utility corridors as multiple use corridors to accommodate oil, natural gas and product pipelines, utilities such as electrical transmission lines and communications infrastructure.	4.8.6 Pipeline and utility companies shall be notified during the subdivision application and development review process.

Discussion

Servicing required for the development of the ASP lands will be paid for and constructed either by the developer, or by the City upon satisfactory collection of municipal levies. Servicing will be provided to a full urban standard with municipal water, sanitary and storm sewers for those areas subject to the Medium Industrial - Highway 15 Vicinity Overlay; with full water and sanitary servicing being provided to the remainder of the plan area west of Range Road 221.

For the remainder of the plan area east of Range Road 221, servicing shall be provided at a reduced level with a trickle water service, low pressure sanitary sewers, and a stormwater collection system consisting of underground and surface conveyance. Installation of infrastructure will be provided in accordance with approved studies (i.e. design brief) and to City of Fort Saskatchewan approved standards.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

The 2006 City of Fort Saskatchewan Conceptual Servicing Study Final Report provided a conceptual level servicing for these lands, in conjunction with the Alsten Lands Outline Plan to the south-west. In 2013/2014 the initial water, sanitary and stormwater servicing concepts for the plan area were reviewed and alternate servicing concepts, blending full and reduced servicing levels were prepared. Further, more detailed technical reports will be required through subsequent stages of development (i.e. Outline Plans, subdivision, etc.) in order to adequately address the provision of water, sanitary and stormwater services.

Water Servicing Concept

It is the City's intent through the Medium Industrial Off-Site Levy program to construct a combined 450mm and 350mm watermain to support this plan area, and meet fire flow demands in the Alsten Development. The 450mm watermain will follow a route along the north boundary of the Alsten Developments (just behind the Heartland Center) with the 350mm line to extend north and connect to the existing watermain at the intersection of 119th Street and 99th Avenue as per **Figure 7.0 – Conceptual Water Servicing**. The 2006 City of Fort Saskatchewan Conceptual Servicing Study Final Report determined that all of the Alsten Lands Outline Plan area and approximately one quarter section of land in this ASP can be serviced with the construction of the above noted 450mm water main. The October 2014 City of Fort Saskatchewan Josephburg Road North Industrial Water Modelling Study - Update Technical Memorandum identifies that the plan area west of Range Road 221 can be fully serviced. Approximately 189.44 ha within the plan area will be fully serviced with municipal water, which includes the approximately 63.0 ha Highway 15 Vicinity Overlay, as identified in **Figure 5.0 – Land Use Concept**. Capacity is limited for the development of the remaining lands; as a result, the majority of the ASP lands are to be serviced with a trickle water feed service to provide potable water only.

Figure 7.0 – Conceptual Water Servicing conceptually illustrates the possible water servicing concept for the ASP lands, based on the completion of infrastructure in the Alsten Lands Outline Plan.

Sanitary Servicing Concept

The Alberta Capital Region Wastewater Commission (ACRWC) pumping station and siphon is located over 2 km west of the plan area, along River Road and north of 104 Street. All sanitary flows in the City of Fort Saskatchewan are directed to this location.

The existing sanitary sewer system in the Eastgate Business Park is adequate to accommodate all of the light and medium industrial development within the Alsten Lands Outline Plan. These flows are directed to an existing 1200mm trunk sewer at the intersection of Highway 15 and 112 Street.

For development to commence in the Josephburg Road North Industrial ASP, it will be necessary to construct a lift station and a 450m forcemain connecting to the existing 1200mm pipe at 99 Avenue and 112 Street. The plan area will be served by a combined gravity and low pressure

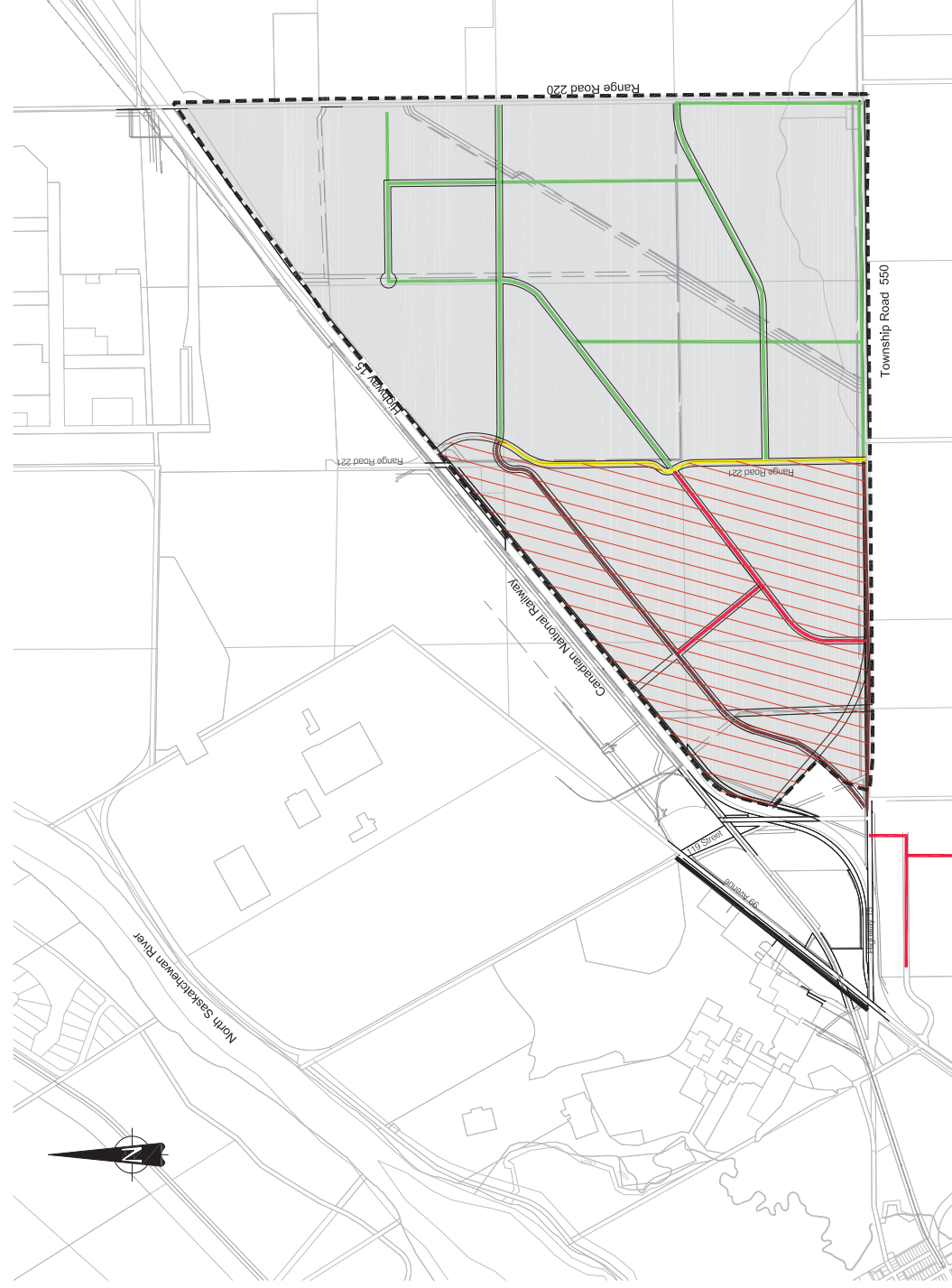
JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

system. The gravity system will serve the area to be fully serviced with municipal water west of Range Road 221, while the low pressure system will service the remainder of the plan area east of Range Road 221.

Figure 8.0 – Conceptual Sanitary Servicing conceptually illustrates the possible sanitary sewer servicing concept for the ASP lands. All development must comply with the ACRWC principles and guidelines.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank



SCALE 1:20000



JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Storm Servicing Concept

A heavily modified, minor tributary of Ross Creek (in the south-eastern part of the plan, crossing to the south side of Township Road 550) forms the principal drainage feature through the plan area. The east side of the plan area slopes from 630m in the west to 626m near the middle. The west half is very flat with an elevation of approximately 626m. Runoff in the west flows away from the high point in all directions whereas to the north runoff collects in a depression. The runoff to the east and south is ultimately collected in a ditch that runs along the south side of Township Road 550. This ditch carries flows east to a tributary of Ross Creek. The runoff on the east half of the plan area collects in the low lying areas, however under large rainfall events, the runoff would be directed to the ditch to the south and eventually into the tributary of Ross Creek.

Three wetland areas have been identified within the plan area. Although these wetland areas have been heavily modified over time, are deemed to have low ecological value and retain water only on an intermittent basis, their incorporation into the proposed development as stormwater management facilities may be a highly desirable means with which to retain the wetlands on the landscape, given the proposed industrial development. However, development would require regulatory approvals or compensation under the Water Act, Public Lands Act and Environmental Protection Act.

The proposed development concept identifies the general location and conceptual size of seven (7) storm water management facilities (SWMF) within the plan area. These SWMF's will be sized in order to supply fire water for future development within the portion of the plan area serviced by a trickle water system and not fully serviced by municipal water services, as identified in **Figure 7.0 – Conceptual Water Servicing**. The SWMF will be outfitted with a dry hydrant system, acceptable to the City of Fort Saskatchewan. It is proposed that a piped outlet system will convey any discharges from each SWMF to Ross Creek.

The storm runoff from the developed plan area will discharge into Ross Creek through a proposed outfall near an existing storm outfall (near Highway 15 and 99 Avenue). During detailed design stage when more accurate information is available, the possibility of utilizing the existing outfall for these areas needs to be explored.

Consideration for alternative methods of overland storm water conveyance may be possible to reduce the cost of the storm sewer system by using drainage ditches and strategically placed public utility lots instead of underground infrastructure. A determination will be made upon the preparation and review of appropriate design briefs by the landowners and at the discretion of the City of Fort Saskatchewan.

Figure 9.0 – Conceptual Storm Servicing conceptually illustrates the proposed storm sewer servicing concept for the plan area.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

Figure 8.0
Conceptual Sanitary
Services



LEGEND

- ↑ PROPOSED FULL SERVICE
SANITARY GRAVITY SEWER SYSTEM
- PROPOSED 150mm SANITARY LOW
PRESSURE SEWER SYSTEM
- - - SERVING BOUNDARY
- ⊗ PROPOSED SANITARY LIFT STATION
- PROPOSED OFFSITE FORCEMAIN
- ▨ FULLY SERVICED



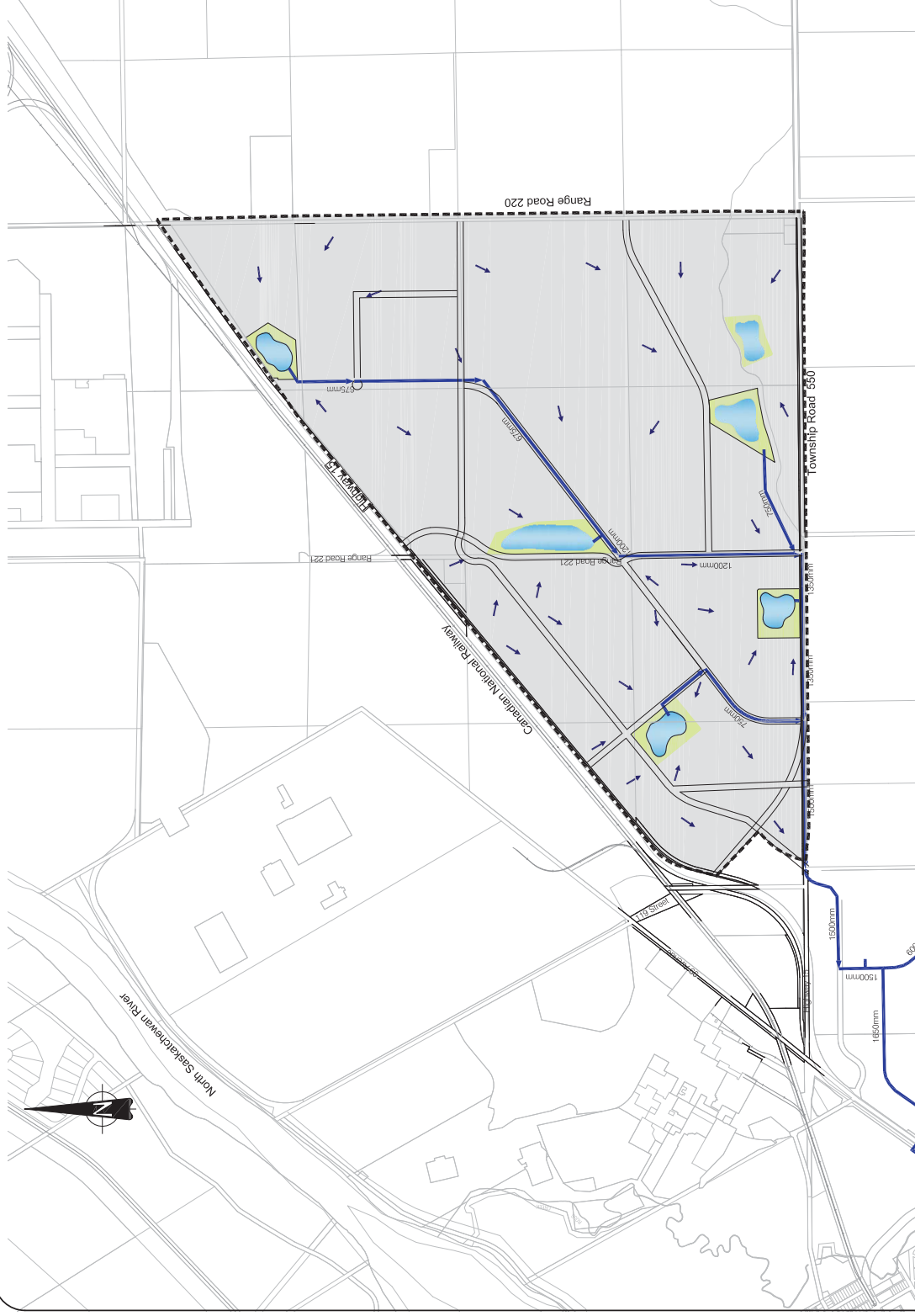
CITY OF
FORT SASKATCHEWAN
ALBERTA

Josephburg Road North Industrial
Area Structure Plan

SCALE 1:20000

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Figure 9.0
Conceptual
Storm Servicing



LEGEND

- 1350mm
- Proposed Storm Trunk
(conceptual alignment)
- Stormwater Management
Facility
- General Direction of
Stormwater Flow
- ASP Boundary



CITY OF
FORT SASKATCHEWAN
ALBERTA

Josephburg Road North Industrial
Area Structure Plan

SCALE 1:20000

Scheffer Andrew Ltd.
 Planners & Engineers

2014-10-27

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Shallow Utilities (Power, Gas, Telecommunications)

Shallow utilities such as power, gas and telecommunications systems will be extended from existing locations, at the cost of the developer, and subject to approvals by the appropriate utility agencies.

Pipeline / Utility Corridors

In recognition of the proximity of the plan area to heavy industry and the existing pipeline and utility corridors within the plan area, utility companies shall be notified during the Outline Plan, subdivision application and development review process. Cooperation and communications with these companies will provide them with the ability to adequately plan their long term infrastructure and land right-of-way requirements, while integrating with the land uses proposed within the plan area.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.9 HIGHWAY 15 CORRIDOR DESIGN GUIDELINES

Goal: To control the appearance and quality of development along Highway 15, recognizing its importance as a gateway to the City of Fort Saskatchewan and the Alberta's Industrial Heartland.

Objective	ASP Policy	Implementation
4.9.1 Promote a high standard of design and aesthetics within the public and private lands abutting Highway 15.	4.9.1(a) The City of Fort Saskatchewan should endeavor to develop and implement a statutory plan overlay in the Land Use Bylaw, consistent with Section 6.8 - Industrial, and more specifically policy 6.8.4 of the MDP, with respect to design guidelines for industrial uses along Highway 15 to ensure a higher standard of appearance for the major gateway into Fort Saskatchewan and the Alberta's Industrial Heartland.	4.9.1(a) The statutory plan overlay design guidelines should address, but not be limited to, such considerations as building design, setbacks, location and screening of open storage areas, parking lots, landscaping, signage and lighting.
	4.9.1(b) All infrastructure required to distribute and service the development should be located underground.	4.9.1(b) Underground services will be required as a condition of subdivision and executed through associated servicing agreements.
		4.9.1(c) Roadway standards will be reviewed and included as conditions at the time of subdivision approval and executed through servicing agreements.

Discussion

Policy 6.8.4 of the Municipal Development Plan provides direction for the City to work with developers and landowners to develop and implement design guidelines regarding such items as architectural treatment of building, setbacks, landscaping, lighting, screening, storage, parking and signage for the Highway 15 corridor. This would ensure that the siting, form, and unified architectural treatment of commercial and industrial development improves the visual quality of road(s) that it fronts and considers the environment in which it is located. Special attention will be given to development along Highways 15. Wetlands, stands of trees, and other natural habitat

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

should be preserved where possible. Parking and loading areas should be paved and screened, have landscaped buffers, and provided with signage where appropriate.

The use of aerial (overhead) power servicing is discouraged as it is unattractive and may pose as an impediment to end users within the plan area. Roadway cross-sections within the Highway 15 Vicinity Overlay are to be urban in nature as it coincides with the level of servicing within the Overlay area, and is more aesthetically pleasing; thereby promoting that these lands be used for their highest and best use.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.10 RISK MANAGEMENT

Goal: To adopt a Risk Management Framework which prohibits land uses within the plan area that might be detrimentally impacted by hazardous land uses located on lands in adjacent industrial areas.

Objective	ASP Policy	Implementation
4.10.1 To restrict industrial uses and industrial activities associated with explosive or hazardous materials within the plan area.	4.10.1 The IH Heavy Industrial district will not be applied to lands within the plan area.	4.10.1 Planning & Development Services will refuse support for applications to rezone lands within the plan area to the IH designation.
4.10.2 To implement risk management practices and procedures for limited highway commercial land uses.	4.10.2 Appropriate safety measures, such as in-place sheltering, shielding, public warning systems, evacuation programs and ventilation shut-off systems may be required for new buildings in the plan area.	4.10.2 A Risk Management Framework shall be developed at the Outline Plan and Design Brief stage by the developer/landowner, in consultation with Emergency Services and adjacent heavy industrial landowners.
4.10.3 To restrict public institution uses where people are dependent upon others for evacuation such as hospitals, schools, or senior citizen homes.	4.10.3 No public institution uses shall be approved with the ASP boundaries.	4.10.3 The Development Officer will refuse any applications for any uses that, in the opinion of the Development Officer, is deemed a public institutional use.

Discussion

The Municipal Development Plan supports the continued growth and expansion of Heavy Industrial development in Fort Saskatchewan, and promotes a risk management approach to ensure that appropriate separation distances are defined between Heavy Industrial Development and less intensive industrial development or potentially non-compatible uses. The limited highway commercial land uses will not be able to accommodate day cares, institutional uses, recreation facilities, or uses where overnight accommodation may occur.

Section 4.3 recognizes the opportunities for limited highway commercial development along Highway 15. Reducing risk has been initiated through the completion of the Industrial Risk Assessment outlined in Section 3.7, whereby 1.5 km and 3.5 km setback distance from heavy industrial to various land uses are recommended. The opportunity for a limited amount of highway commercial uses within the 1.5 km setback distance may be acceptable provided that certain safety measures are implemented in the design and operation of the buildings and that

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

site/development specific risk assessments demonstrate to the City's satisfaction that this lesser setback distance is warranted.

For example, new development may require designs (e.g. structural, mechanical, HVAC, etc.) that reduce the calculated risk equal to or lower than the 1×10^{-5} risk criteria at the 1.5 km distance. The site/development-specific risk assessment could determine that although the site is at or closer than 1.5 km of industry, it actually functions as if it was located farther out because of the incorporation of additional safety measures.

Historically the City has required developers of new industrial subdivisions to provide public warning systems in accordance with City standards, and will continue to do so of future industrial subdivisions.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.11 IMPLEMENTATION

Goal: To adopt and use the Area Structure Plan as a policy framework for development and to assist with the preparation of more detailed Outline Plans and design briefs for City approval prior to consideration of rezoning and subdivision applications.

Objective	ASP Policy	Implementation
4.11.1 To ensure lands within the ASP boundaries are able to meet local and regional demands for industrial land.	4.11.1(a) Servicing of ASP lands shall be permitted to be flexible.	4.11.1(a) Reduced levels of service shall be permitted in the interim until such time as the owner of a parcel requires a higher level of service.
	4.11.1(b) To require more detailed servicing information at the Outline Plan and Design Brief stages.	4.11.1(b) Development and servicing shall proceed in accordance with Outline Plans and Design Briefs prepared for the associated lands.
	4.11.1(c) Provide for a flexible servicing and phasing scheme having regard for the economical and efficient extension of City services and utilities.	4.11.1(c) Development and servicing should occur sequentially, through the extension of storm, sanitary and water systems; flexibility in the sequence of development and servicing shall be accommodated where warranted by the proponent of a subdivision and supported by the City.
	4.11.1(d) Require on-site and off-site costs associated with servicing new developments with roadways, utilities and other infrastructure to be borne by the development.	4.11.1(d) Development charges and levies will be in accordance with executed servicing agreements associated with each development.
4.11.2 Undertake amendments to the Land Use Bylaw	4.11.2(a) A Land Use Bylaw amendment will recognize the addition of the Limited Highway Commercial Overlay designation within the plan area.	4.11.2(a) Administration shall draft a Limited Highway Commercial Overlay for Council consideration.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
	4.11.2(b) Individual landowners must apply for zoning prior to subdivision and/or development.	4.11.2(b) Administration will review all rezoning applications to ensure conformity with this plan.
	4.11.2(c) The City of Fort Saskatchewan, at its discretion, may pre-zone the lands within the ASP to the IM - Medium Industrial District following approval of an Outline Plan.	4.11.2(c) Administration shall investigate the need to pre-zone the lands within the plan area, and determine if it is in the best interest of the City.
4.11.3 Ensure alignment and consistency between all municipal statutory documents.	4.11.3 Amendments to The City of Fort Saskatchewan Alberta's Industrial Heartland ASP, Bylaw C19-00 will recognize amendments made to this plan.	4.11.3(a) The City shall amend Map 2 of The City of Fort Saskatchewan Alberta's Industrial Heartland ASP to reflect the Land Use Concept as found in this plan. 4.11.3(a) The City shall amend The City of Fort Saskatchewan Alberta's Industrial Heartland ASP objective and policies to reflect those found in this plan.
4.11.4 To ensure the ASP remains relevant to the planning and development issues that may change over time.	4.11.4 Planning & Development Services will monitor the effectiveness of the ASP in an ongoing manner.	4.11.4 Planning & Development Services will monitor the ASP and undertake a review, and update if necessary, at 5 year intervals.
4.11.4 Facilitate open discussion with Strathcona County regarding access points from the plan area roads onto County roads.	4.11.4 Inter-municipal cooperation	4.11.4 The City of Fort Saskatchewan will enter into agreement with Strathcona County regarding identified access points from the plan area onto Range Road 220 and TWP RD 550 within one year of adoption of this plan and any access and/or County road improvement matters identified must be resolved to the satisfaction of both parties prior

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
		to any new development proposals being accepted within the plan area.

Discussion

The ASP is meant to serve as a planning framework for development within the plan area, and should be reviewed periodically, updated and amended having regard to changes in economic, social or physical (i.e. man-made) conditions that may arise. Amendments are required to the Land Use Bylaw to ensure consistency with this plan and land uses proposed. Additional technical and supporting studies will be required regarding, but not limited to, transportation and servicing, within the plan area. The phasing of development is proposed based upon the logical extension of municipal infrastructure, transportation links and economics and is conceptually illustrated on **Figure 10.0 – Conceptual Development Phasing**.

Economic conditions, market demand and the logical and cost-effective extension of roadways, municipal services and other infrastructure will determine the phasing of Outline Plans, subdivision and development. If, as a result of proposed Outline Plans, subdivision or development proposals, the need to construct services and other municipal infrastructure in advance of the logical phasing of development is proposed, review will required by the City provided the developer finances the required costs.

Figure 10.0
Conceptual
Development Staging



LEGEND

-  General Direction of Development
-  ASP Boundary



CITY OF
FORT SASKATCHEWAN
ALBERTA

Josephburg Road North Industrial Area Structure Plan

SCALE 1:20000

Scheffer Andrew Ltd.
Planners & Engineers

2014-10-27

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Appendix 1 – Land Ownership

	Titled Owner	Legal Description	Area (ha)
1	Private Corporate	Lot 1, Block 1, Plan 082 0100	1.79
2	Private Corporate	SW 1/4 2-55-22-W4M	54.93
3	Private Corporate	SE 1/4 2-55-22-W4M	65.195
4	Private Corporate	SW 1/4 1-55-22-W4M	65.894
5	Private Corporate	SE 1/4 1-55-22-W4M	40.436
6	Private Corporate	Lot 1, Plan 952 2270	23.49
7	Private Non-Corporate	SE 1/4 1-55-22-W4M	2.02
8	Private Non-Corporate	SE 1/4 12-55-22-W4M	64.7
9	Private Non-Corporate	NE 1/4 12-55-22-W4M	28.48
10	Private Corporate	Pt of Lot 1, Plan 912 2507	0.82*
11	Private Corporate	SW 1/4 12-55-22-W4M	44.7*
12	Private Corporate	NW & NE 1/4 2-55-22-W4M	62.9*
13	Private Corporate	NW 1/4 1-55-22-W4M	64.7
14	Private Non-Corporate	NE 1/4 1-55-22-W4M	64.7
15	Private Corporate	Power Line ROW OT, Plan 110RS	2.48
16	Private Non-Corporate	SE 1/4 1-55-22-W4M	0.543
17	Private Non-Corporate	SE 1/4 1-55-22-W4M	0.203
18	Private Corporate	Lot A, Plan 590KS	1.005
19	Private Corporate	Lot B, Plan 590KS	1.21
20	Private Corporate	Lot C, Plan 590KS	0.924
		TOTAL	591

* Represents area within the ASP, not as indicated on the certificate of title

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Appendix 2 – Land Use Statistics

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN		
	Area (ha.)	% of GDA
Gross Area	590.3	
Existing Roadways and Widening (RR 221, Hwy 15)	9.2	
*Hwy 15 / Rge Rd 220 Interchange Right-of-Way	15.5	
Pipeline & Utility Rights-of-Way	27.3	
Public Utility Lot (Drainage Channel)	5.4	
Gross Developable Area	532.9	100%
Stormwater Management Facilities	32.4	6.1%
Industrial Collector Roadways	34.0	6.4%
**Municipal Reserve		
NE Treed Area	13.6	2.6%
Net Developable Area	452.9	85.0%
(areas below are inclusive of future local industrial roads which will be determined through Outline Plans)		
Medium Industrial	389.9	73.2%
Medium Industrial – Highway 15 Vicinity Overlay	63.0	11.8%
*Area is approximate. Exact area and extent of land required for the interchange to be determined in conjunction with Alberta Transportation, Strathcona County and the City of Fort Saskatchewan prior to approval of an Outline Plan for the adjacent lands.		
** Exact MR area to be confirmed at time of detailed Outline Plan submission. Balance of MR owing to be provided as cash-in lieu, unless the City determines through the Outline Plan stages that smaller park areas are required.		

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Appendix 3 – References

The following studies were reference in support of the Josephburg Road North Industrial ASP:

- 1. City of Fort Saskatchewan Municipal Development Plan, 1999-2010, Bylaw C12-99 (Rescinded)**
- 2. City of Fort Saskatchewan Municipal Development Plan, 2010-2030, Bylaw C16-10**
- 3. City of Fort Saskatchewan Land Use Bylaw, Bylaw C6-08 (Rescinded)**
- 4. City of Fort Saskatchewan Land Use Bylaw, Bylaw C10-13**
- 5. The City of Fort Saskatchewan Alberta's Industrial Heartland ASP, Bylaw C19-00**
- 6. Strathcona Area Industrial Heartland Transportation Study Update – Final Report**
 - a. Prepared by Stantec Consulting Ltd, November 2007, for Strathcona County.
- 7. City of Fort Saskatchewan Conceptual Servicing Study Final Report**
 - a. Prepared by Stantec Consulting Ltd., October 2006
- 8. Ecological Assessment, Josephburg Road North Industrial ASP, Section 1-55-22-4 and portions of Sections 2-55-22-4 and 12-55-22-4, Fort Saskatchewan**
 - o Prepared by Stantec Consulting Ltd, August 15, 2008
- 9. Industrial Risk Assessment, Final Report**
 - o Prepared by Doug McCutcheon and Associates Consulting, March 2009
- 10. Josephburg ASP Amendment, Risk Review**
 - o Prepared by Doug McCutcheon and Associates Consulting, June 2009
- 11. Alsten Lands Outline Plan**
 - o Prepared by Durrance Projects Ltd; Trans America Group; Ecomark Ltd; Associated Engineering Ltd; & BK Hydrology Services, July 2007
- 12. City of Fort Saskatchewan Strategic Plan: 2020 Vision – Clarity for the Future**
- 13. Strathcona County Land Use Bylaw, Bylaw 8-2001**
- 14. Heartland Transmission Project;**
 - a. www.heartlandtransmissionarea.ca

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

15. Capital Region Integrated Growth Management Plan – Final Report on Core Infrastructure

- Prepared by ISL Engineering and Land Services, November 2007

16. Josephburg Road North Industrial ASP – Transportation Impact Assessment

- Prepared by Stantec Consulting Ltd., December 2009

17. Growing Forward – The Capital Region Growth Plan

- Capital Region Board, March 2009

18. The Capital Region Growth Plan Addendum

- Capital Region Board, October 2009 & December 2009

19. Integrated Regional Transportation Master Plan

- Capital Region Board, September 2011

20. Josephburg Road North Industrial Water Modelling Study - Update

- Prepared by Associated Engineering, October 2014



Josephburg Road North Industrial Area Structure Plan

BYLAW ~~C13-09~~C9-15

SCHEDULE A

Prepared For:
The City of Fort Saskatchewan

Prepared By:
Stantec Consulting Ltd.
File: 1161 70800

Adopted ~~as Bylaw C13-09~~ January
25, 2011

Reviewed and Updated for the City
of Fort Saskatchewan by Scheffer
Andrew Ltd. in 2014

Adopted as Bylaw C9-15

This Page Left Intentionally Blank

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Table of Contents

1.0 INTRODUCTION	1.4
1.1 INTRODUCTION	1.4
1.2 PURPOSE	1.4
1.3 LAND OWNERSHIP	1.5
1.4 REGIONAL CONTEXT	1.9
1.5 ORIENTATION	1.9
2.0 STATUTORY AND POLICY CONTEXT	2.11
2.1 CITY OF FORT SASKATCHEWAN MUNICIPAL DEVELOPMENT PLAN (2010 – 2030)	2.11
2.2 ALBERTA'S INDUSTRIAL HEARTLAND AREA STRUCTURE PLAN	2.13
2.3 CITY OF FORT SASKATCHEWAN LAND USE BYLAW	2.14
2.4 CITY OF FORT SASKATCHEWAN STRATEGIC PLAN	2.15
2.5 STRATHCONA COUNTY STATUTORY PLANS	2.15
3.0 SITE CHARACTERISTICS AND DEVELOPMENT CONSIDERATIONS	3.17
3.1 TOPOGRAPHY AND VEGETATION	3.17
3.2 PRELIMINARY ECOLOGICAL ASSESSMENT	3.17
3.3 EXISTING AND SURROUNDING USES	3.23
3.3.1 Existing Land Uses	3.23
3.3.2 Surrounding Land Uses	3.24
3.3.3 Natural Constraints	3.24
3.3.4 Man-Made Constraints	3.25
3.3.5 Parks, Recreation, Open Space and Trails	3.31
3.4 TRANSPORTATION	3.31
3.4.1 Highway 15	3.31
3.4.2 Fort Saskatchewan By-Pass	3.32
3.4.3 Township Road 550	3.33
3.4.4 Range Road 221	3.33
3.4.5 Range Road 220	3.33
3.4.6 Dangerous Goods Routes	3.33
3.5 MUNICIPAL SERVICES	3.33
3.6 EMERGENCY SERVICES	3.34
3.6.1 Fire, Police, Ambulance and Disaster Services	3.34
3.6.2 Emergency Preparedness	3.34
3.7 INDUSTRIAL RISK ASSESSMENT	3.35
4.0 LAND USE CONCEPT, OBJECTIVES AND POLICIES	4.37
4.1 DEVELOPMENT CONCEPT	4.37
4.2 INDUSTRIAL DEVELOPMENT	4.40
4.3 MEDIUM INDUSTRIAL – HIGHWAY 15 VICINITY OVERLAY	4.44
4.4 UTILITY RIGHT OF WAYS	4.47

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.5 ENVIRONMENTAL MANAGEMENT	4.49
4.6 TRANSPORTATION.....	4.51
4.7 SUSTAINABLE DEVELOPMENT PRACTICES	4.59
4.8 MUNICIPAL SERVICING.....	4.60
Water Servicing Concept	4.64
Sanitary Servicing Concept.....	4.65
Storm Servicing Concept	4.69
Figure 8 4.72	
Shallow Utilities (Power, Gas, Telecommunications).....	4.76
Pipeline / Utility Corridors.....	4.76
4.9 HIGHWAY 15 CORRIDOR DESIGN GUIDELINES.....	4.77
4.10 RISK MANAGEMENT	4.79
4.11 IMPLEMENTATION	4.81
APPENDIX 1 – LAND OWNERSHIP	4.87
APPENDIX 2 – LAND USE STATISTICS	4.90
APPENDIX 3 – REFERENCES	4.92
1.0 INTRODUCTION	1.4
1.1 INTRODUCTION	1.4
1.2 PURPOSE	1.5
1.3 LAND OWNERSHIP	1.5
1.4 REGIONAL CONTEXT	1.9
1.5 ORIENTATION	1.9
2.0 STATUTORY AND POLICY CONTEXT.....	2.11
2.1 CITY OF FORT SASKATCHEWAN MUNICIPAL DEVELOPMENT PLAN (2010 – 2030).....	2.11
2.2 ALBERTA'S INDUSTRIAL HEARTLAND AREA STRUCTURE PLAN	2.14
2.3 CITY OF FORT SASKATCHEWAN LAND USE BYLAW	2.15
2.4 CITY OF FORT SASKATCHEWAN STRATEGIC PLAN.....	2.16
2.5 STRATHCONA COUNTY STATUTORY PLANS	2.16
3.0 SITE CHARACTERISTICS AND DEVELOPMENT CONSIDERATIONS	3.17
3.1 TOPOGRAPHY AND VEGETATION.....	3.17
3.2 PRELIMINARY ECOLOGICAL ASSESSMENT.....	3.17
3.3 EXISTING AND SURROUNDING USES.....	3.23
3.3.1 Existing Land Uses	3.23
3.3.2 Surrounding Land Uses	3.24
3.3.3 Natural Constraints	3.24
3.3.4 Man-Made Constraints.....	3.25
3.3.5 Parks, Recreation, Open Space and Trails.....	3.32
3.4 TRANSPORTATION.....	3.32
3.4.1 Highway 15	3.32

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

3.4.2	Fort Saskatchewan By-Pass.....	3.33
3.4.3	Range Road 221.....	3.34
3.4.4	Range Road 220.....	3.34
3.4.5	Dangerous Goods Routes.....	3.35
3.5	MUNICIPAL SERVICES.....	3.35
3.6	EMERGENCY SERVICES.....	3.35
3.6.1	Fire, Police, Ambulance and Disaster Services.....	3.35
3.6.2	Emergency Preparedness.....	3.36
3.7	INDUSTRIAL RISK ASSESSMENT.....	3.36
4.0	LAND USE CONCEPT, OBJECTIVES AND POLICIES.....	4.39
4.1	DEVELOPMENT CONCEPT.....	4.39
4.2	INDUSTRIAL DEVELOPMENT.....	4.42
4.3	MEDIUM INDUSTRIAL – HIGHWAY 15 VICINITY OVERLAY.....	4.46
4.4	UTILITY RIGHT OF WAYS.....	4.49
4.5	ENVIRONMENTAL MANAGEMENT.....	4.51
4.6	TRANSPORTATION.....	4.53
4.7	SUSTAINABLE DEVELOPMENT PRACTICES.....	4.61
4.8	MUNICIPAL SERVICING.....	4.62
	Water Servicing Concept.....	4.67
	Sanitary Servicing Concept.....	4.67
	Storm Servicing Concept.....	4.72
	Figure 8 – 4.75	
	Shallow Utilities (Power, Gas, Telecommunications).....	4.79
	Pipeline / Utility Corridors.....	4.79
4.9	HIGHWAY 15 CORRIDOR DESIGN GUIDELINES.....	4.79
4.10	RISK MANAGEMENT.....	4.82
4.11	IMPLEMENTATION.....	4.83
APPENDIX 1	LAND OWNERSHIP.....	4.91
APPENDIX 2	LAND USE STATISTICS.....	4.93
APPENDIX 3	REFERENCES.....	4.95

List of Figures

Follows Page

1.0	PLAN AREA	1.5
2.0	LAND OWNERSHIP	1.9
3.0	SITE CONTOURS	3.1977
4.0	NATURAL SITE FEATURES AND MAN-MADE CONSTRAINTS	3.24199
5.0	LAND USE CONCEPT	4.23755
6.0	TRANSPORTATION NETWORK	4.45314
7.0	CONCEPTUAL WATER SERVICING	4.63149

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

8.0	CONCEPTUAL SANITARY SERVICING	4.67553
9.0	CONCEPTUAL STORM SERVICING	4.56975
10.0	CONCEPTUAL DEVELOPMENT PHASING	4.67973

List of Tables

Page

1	Pipeline Information Summary	3.42755
2	Utility Right-of-Way Information Summary	3.42977

1.0 Introduction

1.1 INTRODUCTION

The purpose of the Josephburg Road North Industrial Area Structure Plan (ASP) is to provide a policy framework for orderly planning in the area, respond to future subdivision and development proposals, and establish a preliminary servicing concept for the Josephburg Road North Industrial area. The objectives of this ASP are to:

- Prepare a policy document and a future development concept for the plan area;
- Establish the general location of land uses and transportation routes to support development within the plan area;
- Conceptually identify public utilities and storm water management facilities (SWMF) necessary to support development;
- Outline preliminary and conceptual servicing requirements for the proposed development concept;
- Determine the appropriate sequencing and phasing of development based on servicing, access and market demand; and,
- Prompt the creation of a Boundary Road Accord Agreement between the City of Fort Saskatchewan and Strathcona County for Range Road 220 when warranted.

1.2 PURPOSE

The ASP-plan area covers approximately 590 ha (1,459 ac). As shown in **Figure 1.0 - Plan Area**, the plan area is located within the municipal boundaries of the City of Fort Saskatchewan. The plan area boundaries are as follows:

Northern Boundary: Highway 15

Eastern Boundary: Range Road 220 (Strathcona County)

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Western Boundary: Highway 15

Southern Boundary: Township Road 550 (Strathcona County)

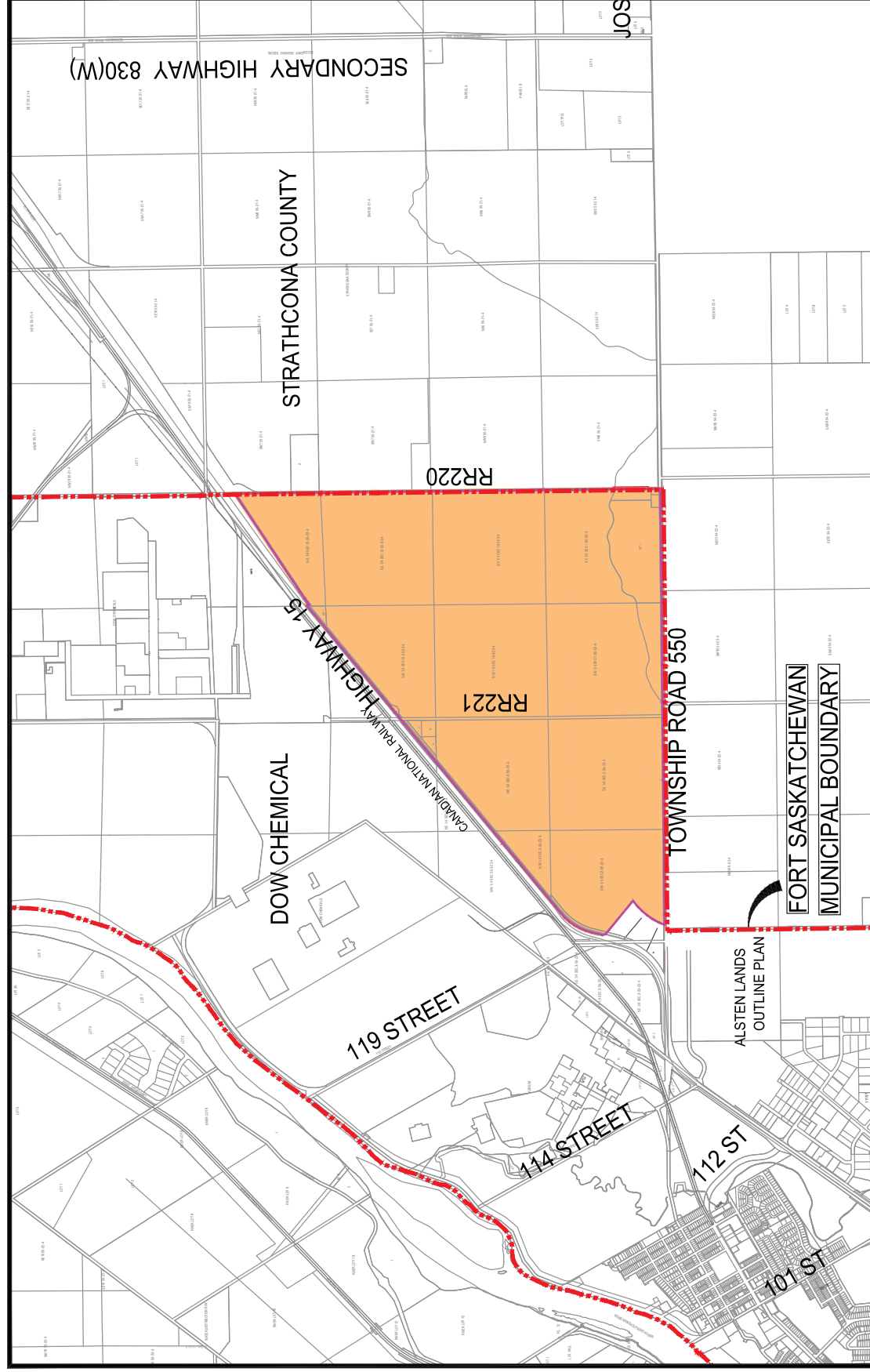
1.3 LAND OWNERSHIP

Most of the parcels within the Plan area are un-subdivided (i.e. full quarter sections) or have been severed by the Highway 15 right-of-way or the existing drainage ditch in the southeast portion of the Plan area. Three smaller parcels (ranging from 0.5 ha to 5 ha) are registered in the southeast corner of the plan area (Range Road 220), and are currently titled to private landowners. Most of the land in the ~~Plan-plan~~ is held by private developers, while a private landowner currently owns approximately 2½ quarter sections adjacent to Range Road 220.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

Figure 1.0
Plan Area



Josephburg Road North Industrial Area Structure Plan Bylaw C13-09

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Figure 2.0 - Land Ownership and **Appendix 1 – Land Ownership** show the land ownership within the ASP-plan area.

1.4 REGIONAL CONTEXT

Fort Saskatchewan is centered within Alberta's Industrial Heartland Region, and is home to numerous industrial petrochemical and related industries, providing an important link to the Athabasca Oil Sands of northeastern Alberta and international markets. It is one of Canada's largest processing centres for petroleum, petrochemicals and chemicals, and employs a large, skilled workforce. The City of Fort Saskatchewan is home to ~~17,500~~21,795-22,808 residents, and the majority of land uses include residential, light/medium/heavy industrial, commercial, business and agricultural uses. Highway 21 provides one of the main access routes to Fort Saskatchewan while Highway 15 provides a second link to the Capital Region, and access to northern and eastern Alberta.

The Alberta's Industrial Heartland region lies within portions of ~~four~~five municipalities: The City of Fort Saskatchewan, the City of Edmonton, the Counties of Strathcona, Sturgeon and Lamont. It is designated for long-term heavy and medium industrial growth, and is already home to over ~~30~~50 world-class companies, with still more facilities proposed.

The City of Fort Saskatchewan is a member of the Capital Region Board (CRB), along with 23 other municipalities in the Capital Region. Member municipalities are subject to the Capital Region Growth Plan (CRGP), which is a regional growth management document. The City of Fort Saskatchewan, has been identified as being a priority growth area within the CRGP.

1.5 ORIENTATION

This document contains four (4) sections and 3 appendices.

- Section 1 provides the Introduction, purpose and regional context of the plan;
- Section 2 describes the Statutory and Policy Context;
- Section 3 outlines the Site Characteristics and Development Considerations;
- Section 4 Describes the Land Use Concept, Objectives and Policies;
- Appendix 1 contains background information on Land ownership;
- Appendix 2 provides the proposed Land Use Statistics;
- Appendix 3 contains a listing of technical studies used in support of the preparation of the development and servicing concepts.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

[illegible]

Area Structure Plan

Bylaw C13-09



JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

2.0 Statutory and Policy Context

The ASP has been prepared in context of its geographical location within the Edmonton Capital Region and the Heartland Industrial Area.

It has been planned having regard for the City of Fort Saskatchewan's statutory plans, policies and design principles which govern land development. These include the City of Fort Saskatchewan Municipal Development Plan (MDP), the Fort Saskatchewan Alberta's Industrial Heartland Complementary Area Structure Plan, and Land Use Bylaw ~~G6-08~~C10-13. Other relevant policies and design principles are further identified and summarized in the following sub-sections as they relate to the creation of an industrial park within the ~~ASP-plan~~ area.

It must be noted that the ASP underwent a review and update in 2013/2014, in order to ensure alignment with the revised MDP, LUB and CRGP. This review and update involved amendments to the proposed land uses and servicing concepts within the plan area, in order to provide for greater flexibility and encourage development within the plan area.

Future applicants seeking amendments to the ASP or applying for rezoning, subdivisions or development permits are required to consult the actual documents for specific guidance on detailed requirements as they apply to particular properties.

The following documents provide important direction and a policy context for the ASP.

2.1 CITY OF FORT SASKATCHEWAN MUNICIPAL DEVELOPMENT PLAN (2010 – 2030)

The Municipal Development Plan (MDP) was adopted in September 2010 as Bylaw C16-10, and is intended to express the community's vision, goals, objectives and policies to direct its physical, social and economic development from 2010 to 2030.

Several of the policies contained within the MDP have been used in the preparation of this ASP, ensuring conformity with Council direction with respect to the development of these lands for industrial purposes. The relevant policies include:

Section 6.8 ➔ Industrial ➔ General Policies for all Industrial Areas

Section 6.8.1 Implement the Industrial Heartland Area Structure Plan (ASP) and the Josephburg Road North Industrial ASP, which set the main policy direction for the development of industrial areas.

Section 6.8.3 Encourage the utilization of eco-industrial planning principles, seeking enhanced environmental and economic performance through collaboration in managing environmental and resource issues, including energy, water and

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

materials, as outlined in the Industrial Heartland ASP and the Eco-Industrial Master Plan Strategy.

Section 6.8.4 *Require, through provisions of the Land Use Bylaw and other municipal bylaws, quality development to be maintained through the application of standards for siting and design of buildings, landscaping, screening of storage and parking areas, and signage.*

Section 6.8.7 *Permit interim agricultural uses that will not preclude the future use of the land for industrial purposes.*

Section 6.8.8 *Ensure that developments mitigate visual impacts from public roads.*

Section 6.8 ➔ Industrial ➔ Light and Medium Industrial

Section 6.8.15 *Complete and implement the Josephburg Road North Industrial Area Structure Plan (ASP).*

Section 6.8.16 *Ensure compatibility between the Light and Medium industrial designation and surrounding urban development by directing only Light Industrial uses to those lands within proximity of the General Urban Area.*

Section 6.8.17 *Proactively work to ensure a supply of fully serviced Light and Medium Industrial lands.*

Section 6.8.18 *Ensure that Light and Medium Industrial uses meet current industrial Risk standards and guidelines.*

Section 6.8.19 *Use a risk management approach based on principles of: risk reduction at the source; risk reductions through land use controls and prescribed mitigation measures; emergency preparedness; emergency response; and, risk communication.*

Section 7.2 ➔ Community Design ➔ Sustainable Design Policies

Section 7.2.3 *Work with the development community to provide incentives for the development of buildings and developments that meet green building standards.*

Section 8.2 ➔ Mobility ➔ Pedestrians and Cyclists

Section 8.2.2 *Plan for pedestrian and cyclist facilities as part of development and redevelopment proposals, ensuring the provision of adequate walking and cycling paths and lanes, and adequate cycle facilities such as secure storage, changing rooms, and showers where appropriate/feasible.*

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Section 8.4 ➡☐ Mobility ➡☐ Roadways

- Section 8.4.1 *Use the Transportation Master Plan as a reference to ensure that adequate rights-of-way are preserved and incorporated into future development areas.*
- Section 8.4.2 *Limit vehicular accesses along Highway 15 and 21 to provide an expressway standard.*
- Section 8.4.3 *Ensure that Transportation Impact Studies are prepared to evaluate the impacts of major development on the safe and efficient movement of pedestrians, cyclists, public transit and vehicles.*

Section 10.2 ➡☐ Parks and the Natural Environment ➡☐ Municipal, School and Environmental Reserves

- Section 10.2.3 *Require that environmental reserve, municipal and school reserves be identified through the ASP and ARP processes, to the satisfaction of the Municipality.*
- Section 10.2.4 *Ensure that industrial ASP and ARP processes consider preservation of land for parkland, and that municipal reserves are established for this purpose.*

Section 10.4 ➡☐ Parks and the Natural Environment ➡☐ Pathways and Trails

- Section 10.4.1 *Facilitate the continued development of community and regional trail systems that provide connectivity for both leisure and commuting purposes.*

Section 11.4 ➡☐ Infrastructure and Resource Management ➡☐ Stormwater Management

- Section 11.4.2 *Maximize retention of stormwater or require enhancement to natural wetlands where possible to ensure a high quality of stormwater effluent.*
- Section 11.4.3 *Minimize effective impervious area for all new development to reduce development related stormwater run-off.*

Section 13.3 ➡☐ Responsive Local Economy ➡☐ Economic Diversification

- Section 13.3.1 *Support economic diversification by ensuring there are sufficient commercial and industrial lands available to suit a variety of business opportunities.*

2.2 ALBERTA'S INDUSTRIAL HEARTLAND AREA STRUCTURE PLAN

The City of Fort Saskatchewan Alberta's Industrial Heartland Area Structure Plan (ASP) was approved under Bylaw C19-00 in August 2001. At the same time, the municipalities of Lamont County, Strathcona County and Sturgeon County approved similar, complimentary ASP's for

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

lands within their jurisdictions. Together, these four ASP documents refer to the area now marketed internationally as the Alberta's Industrial Heartland.

The purpose of these four ASP's is to guide future industrial development within each municipality and the region as a whole. More specifically the plans:

- Encourage more efficient use of the land resource;
- Establish complementary land use policies for industrial, transition and environmental areas in the Heartland Region;
- Reduce environmental impacts;
- Reduce land use conflicts and establish buffers and transition areas around heavy industry;
- Facilitate efficient provision and extension of transportation and utility infrastructure and joint use of these services;
- Encourage the use of eco-industrial principles, e.g. co-generation.

The land use concept for the ASP identifies the entire plan area as being within the Medium Industrial Policy Area. This policy area is intended to serve as a transition zone between heavy industrial uses and lighter/non-industrial land uses. The land use concept for the ASP identifies the entire plan area as being within the Medium Industrial Policy Area. This policy area is intended to serve as a transition zone between heavy industrial and non-industrial land uses.

Important guidelines outlined in this ASP, and which dictate development is as follows:

- This area will accommodate medium industrial activities that could support the activities associated with the heavy industry activities throughout the Heartland;
- Activities could include: heavy industrial and agricultural equipment sales, rentals and repairs; laboratories; temporary indoor storage; veterinary clinics or hospitals; minor eating establishments; contracting services; vehicle repair and service stations and greenhouses/nurseries;
- Access to and from Highway 15 will be centralized in one or two intersections;
- To maintain the point of entry into the Heartland Area, regulations for the development will require a high standard of site design, signage, open space, architectural control and landscaping.

2.3 CITY OF FORT SASKATCHEWAN LAND USE BYLAW

The plan area is, in large part, currently zoned as IR -- Industrial + Reserve, but is agricultural and presently under cultivation. The IR is district is "generally intended to reserve those areas of the City which are rural in character or land use but are intended for future industrial development until such time as a subdivision plan has been accepted in principle or approved for other specific uses not permitted in this district." Approximately 47 ha (117 acres), adjacent to Highway 15 and Range Road 221 is currently zoned as IM - Medium Industrial District; this district "is generally

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

intended to establish an area of industrial uses where site regulations ~~requirerequire~~ a high standard of site design, open space, and landscaping. ~~..T~~This district ~~will accomodate~~ uses that do not cause any external, objectionable or dangerous conditions beyond the outer limit of the site and will normally be appliedis intended to serve as a District to buffer between heavier industrialheavier industrial Land Use Districts fromand other Lland Uuses Districts."

In addition to the lands zoned as IM and IR, there are approximately 23.5 ha (58 acres) of land at the intersection of Range Road 220 and Township Road 550, that are currently zoned as IL - Light Industrial-District. This district is "intended to provide for light industrial uses that do not adversely affect adjacent land uses or cause any external, objectionable or dangerous conditions outside of any building on the light industrial site. This district is also intended to ..and may serve as a buffer between heavier industrial and other land uses."

In addition to the zoning currently in place, a portion of the plan area adjacent to Highway 15 is subject to the Highway 15 Vicinity Overlay. T;-the purpose of the Overlay "is to provide for limited highway commercial uses adjacent to Highway 15" as identified in **Figure 5.0 – Land Use Concept** of the ASP.

The entire ASP area is currently zoned IR – Industrial Reserve, but is agricultural and presently under cultivation. The district is "intended to reserve those areas of the municipality which are rural in character or land use but intended for future industrial development until such time as a subdivision plan has been accepted in principle or approved for other specific uses not permitted in this district".

Lands that are adjacent to, but not within, the plan area and are within the City of Fort Saskatchewan are currently zoned as IL-Light Industrial, IM-Medium Industrial or IH-Heavy Industrial.

2.4 CITY OF FORT SASKATCHEWAN STRATEGIC PLAN

The City of Fort Saskatchewan's Strategic Plan: 2020 Vision – Clarity for the Future, identifies economic development as a significant strategic priority. Within that priority, it is the City's objective to attract medium industrial activities by providing an adequate amount of serviced and available industrial land for sale to prospective industries. The preparation of this ASP provides for the proper planning and development of the industrial area in supporting the City in achieving their strategic objective of creating a sizeable medium industrial land base.

2.5 STRATHCONA COUNTY STATUTORY PLANS

Associated statutory plans and the Land Use Bylaw within Strathcona County to the east and south of the ASP-plan area include the Municipal Development Plan Bylaw 1-2007, Strathcona County Alberta's Industrial Heartland ASP Bylaw 65-2001 and the Land Use Bylaw 8-2001.

The MDP identifies lands south and east of the plan area as being under the Agriculture Large Holdings Policy Area or the Agri-Industrial Transition Policy Area. The Strathcona County Alberta's Industrial Heartland ASP identifies the lands adjacent to Highway 15, north-east of the plan area, as being within the Strathcona Transition Policy Area.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

The Land Use Bylaw has zoned these lands adjacent to the plan area as AG Agriculture: General. The ASP-plan area is also in proximity to the Josephburg Airport (within 1.5 miles). As such, the A-O Airport Vicinity Protection Overlay applies to some of these lands.

Although the airport is within Strathcona County, a portion of the eastern ASP-plan area is affected by the conical and inner-horizontal surfaces and take-off/approach surface of the runway. Therefore, it should be recognized that the airport may influence the height and type of development permitted within the plan area. **~~Section 9.16—Development in the vicinity of the Strathcona Airport in the City of Fort Saskatchewan~~** MDP recognizes this and provides policy direction to ensure that developments and plans do not negatively impact existing airport operations and any future plans for expansion.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

3.0 Site Characteristics and Development Considerations

3.1 TOPOGRAPHY AND VEGETATION

As shown in **Figure 3.0 - Site Contours**, the topography of the land within the ASP-plan area is primarily flat, with gradual sloping from east to west. The highest point is at an elevation of 631.1 m (within the eastern portion of the N.E.1/4 SEC.1-55-22-4), while the lowest elevation is 624.1 m (within the eastern portion of the S.W.1/4 SEC.2-55-22-4). The maximum elevation difference across the entire plan area is approximately 7.0 m.

The majority of the land within the ASP-plan area has been cleared of vegetation and is under cultivation. However, there are several depressional areas loosely scattered throughout the plan area and a large tree stand in the north-east corner. Stantec Consulting Ltd. undertook a preliminary ecological assessment of these features, described in Section 3.2 below, to provide a preliminary baseline inventory of site features.

3.2 PRELIMINARY ECOLOGICAL ASSESSMENT

The ASP is located within the Parkland Natural Region and Central Parkland Subregion. The Parkland Natural Region is a transition vegetation zone, characterized by fescue grassland interspersed with pockets of aspen forest and mixed wood forest. The Central Parkland Subregion typically consists of groves of poplar intermixed with grasslands dominated by rough fescue. As part of the ecological assessment, a site reconnaissance was conducted in the summer 2008. This investigation identified a total of 3 wetlands (defined as having standing water and/or the presence of hydrophilic vegetation), a tree stand and a drainage course. Two of the wetlands, identified as Wetland (W1) and Wetland (W2) on **Figure 4.0 – Natural Site Features and Man-Made Constraints**, have been classified as intermittent bodies of water. Both wetlands are at low-lying elevations and are absent of distinct hydrophilic vegetation such as cattails, sedges, rushes and distinctive soils usually characteristic of higher order wetland systems. It is likely that these areas contain open water during portions of the wet season and would seldom hold water for prolonged periods of time. No wildlife was observed at either of these locations. Therefore, given their low-lying elevations, it is suggestive that the primary function of these wetland features would be as a surficial run-off storage feature.

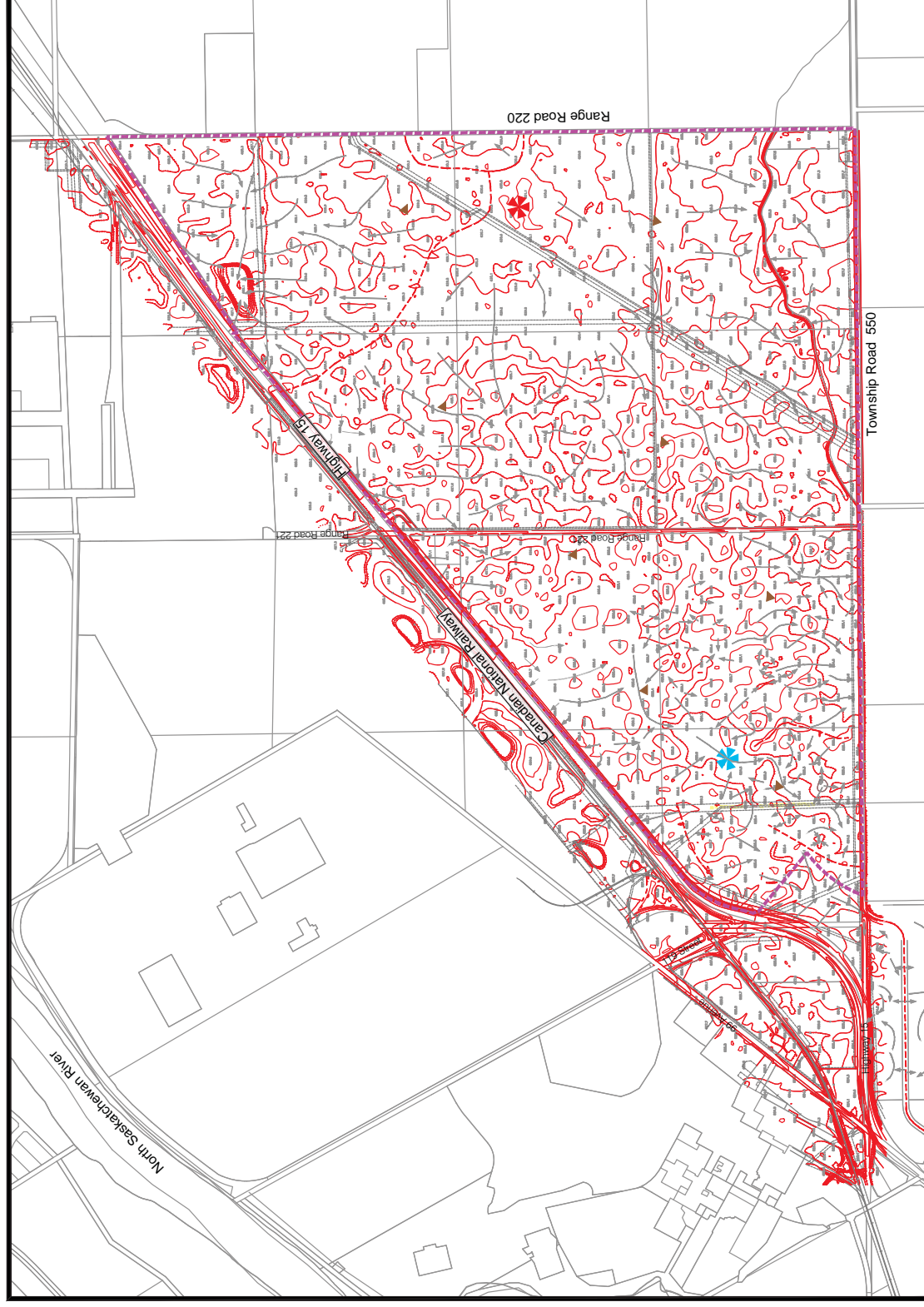
The third wetland, Wetland (W3), did not appear on the landscape until after 1996 and is a large, open water feature with steep banks and no outlying fringe of vegetation, suggestive of a man-made feature. However, various waterfowl species were identified using the area.

The existing tree stand is located in a portion of the NE and SE ¼ Sec 12-55-22-W4M, and is identified as “Tree Stand”. This area has a large contiguous stand that contained healthy mature balsam poplar with a thick willow understory, a meadow-like interior consisting of reed canary

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

Figure 3.0
Site Contours



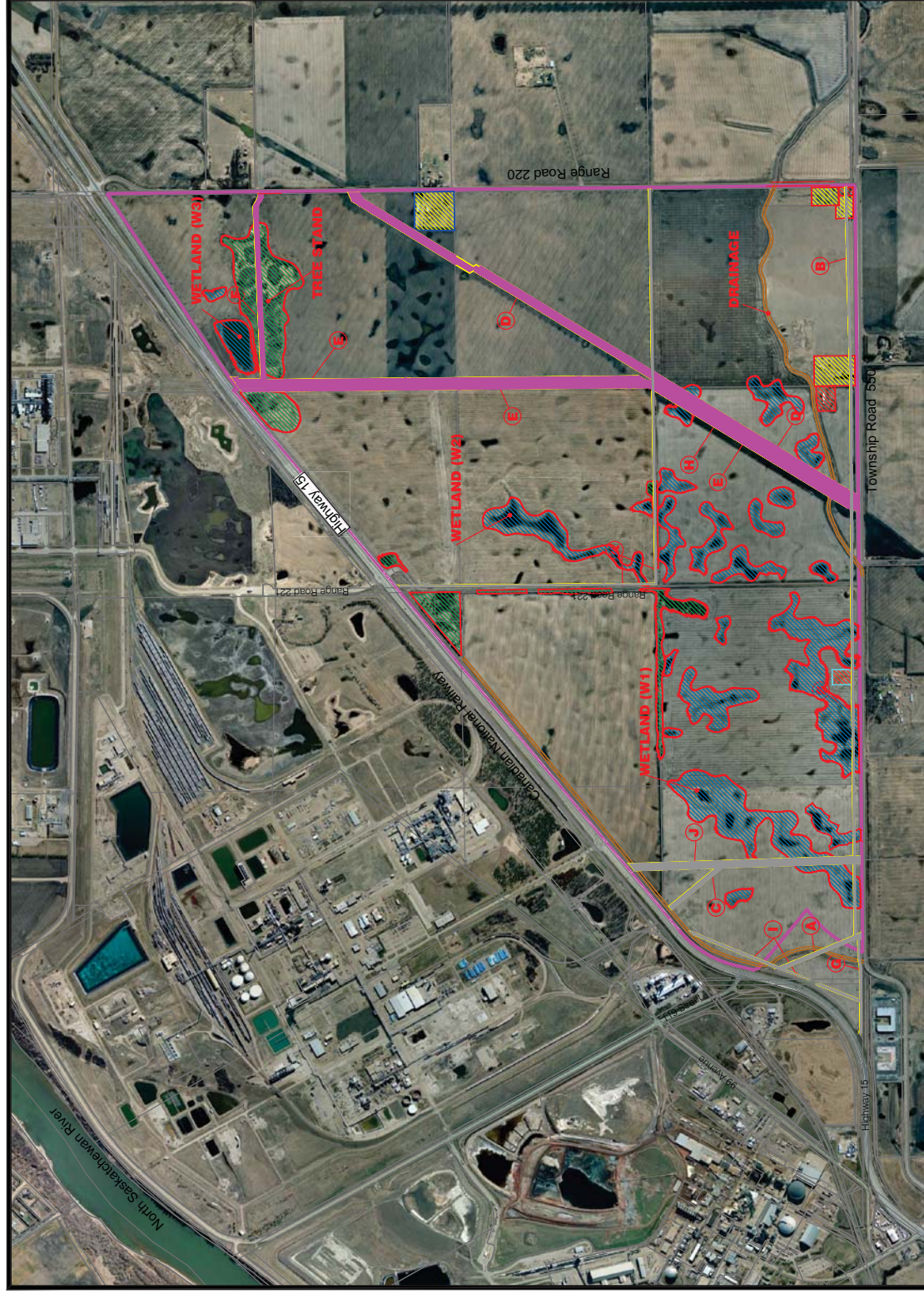
- ASP Boundary
- Existing Drainage Divide
- Existing Drainage Direction
- LOW POINT = 624.1 m
- HIGH POINT = 631.1 m

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Figure 4.0

Natural Site Features

and Man-Made Constraints



Legend

- A. Gasline R/W 4729hw
- B. Gas Pipeline R/W 3014KS
- C. Products Pipeline R/W 1913TR
- D. Pipeline R/W 952 5272; Petroleum Products Pipeline 822 1189; Oil Pipeline R/W 3859NY
- E. Oil Pipeline R/W 5275TR; Gas Pipeline R/W 792 2509
- F. Light Hydrocarbons Liquids Pipeline R/W 5073TR



ASP Boundary

Vegetation

Residential Uses

Low Lying Areas

Agricultural Use

Pipeline/ Utility Corridor

Existing Drainage Channel

Josephburg Road North Industrial

Area Structure Plan

Bylaw C13-09

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

grass. Surrounding the stand were intermittent depressional areas that could potentially hold water for portions of the year. The preliminary site visit suggests that the stand is sustainable and a permanent feature on the landscape. A review of historical air photos revealed that the woodlot has been present on the landscape as a depressional area since 1949 and it appears vegetation has been associated with it since 1962. In addition, a variety of wildlife species were observed throughout the stand, either directly or indirectly, including migratory birds and the presence of deer were indicated.

The drainage course runs from the southwest corner of the SW ¼ 1-55-22-W4M to the east of the SE ¼ 1-55-22-W4M, and has been evident on the landscape since 1949. The channel appears to be a natural creek, which has been heavily modified over time. The historical air photo review indicates that the drainage channel likely contains water during parts of the year. Further investigation of the channel would be required prior to any development if additional modifications are proposed.

Based on the ecological assessment, Wetlands (W1) and (W2) have been heavily modified through agricultural practices over time, while Wetland (W3) appears to be a man-made feature. If (W1), (W2) or the drainage course, are to be conserved, it is recommended that a buffer be implemented around the perimeter to protect wildlife habitat and water quality within the wetland. Due to historical agricultural activities, it is also recommended that restoration activities occur within the buffer and/or riparian area to improve habitat quality. However, as Wetlands (W1) and (W2) are considered to have low ecological value, they may not require compensation under the Provincial Wetland Restoration/Compensation Guide (Alberta Environment 2007). If compensation is required it should be done in accordance with Alberta Environment policies and procedures.

3.3 EXISTING AND SURROUNDING USES

3.3.1 Existing Land Uses

The existing land uses within the ASP-plan area are identified in **Figure 4.0 – Natural Site Features and Man-Made Constraints**. The majority of the plan area is agricultural land, with few permanent structures. There are no other land uses in the plan. Specific land uses within the plan area include:

Agriculture

The majority of land in the ASP-plan area is under agricultural cultivation. There are no horticultural or intensive livestock operations within the ASP-plan area.

Residential

There are five existing dwellings within the plan area, each located on separately titled parcels. Associated with each parcel are also numerous outbuildings (i.e. grain silos, workshops, garages,

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

~~etc.~~). Two of the homes have access directly to Range Road 220, while the other three access directly to Township Road 550.

3.3.2 Surrounding Land Uses

Land uses to the east and south are within the jurisdiction of Strathcona County, while land uses to the north and west are within the City of Fort Saskatchewan.

Agriculture

The predominant land use to the south and east are agricultural in nature. Directly adjacent to the ASP-plan area are five existing farmsteads accessing either Range Road 220 or Josephburg Road.

Industrial

Heavy industrial uses are located to the north and west of the ASP-plan area. These uses are petro-chemical in nature and include multi-national firms such as Dow Chemical Canada Inc, Agrium Inc and Sherritt International. These industries cover a large number of hectares and are accessed directly from Highway 15.

To the south-west of the ASP-plan area is the ~~recently-approved~~ Alsten Lands Outline Plan area, containing a mixture of light and medium industrial lands on parcels ranging from 1.0 ha to 2.0 ha in area. Servicing of the first phase of development began in the summer of 2008, with a total plan area consisting of approximately 123 ha (304 ac).

3.3.3 Natural Constraints

There are few natural constraints to development of these lands for development purposes. The constraints as they are present today include:

Drainage Course

As detailed in Section 3.2, there is an existing drainage course which traverses from the southwest corner of the SW ¼ 1-55-22-W4M to the east of the SE ¼ 1-55-22-W4M. The channel is a tributary of Ross Creek, originating further east in Strathcona County, and has been heavily modified and re-channeled over time. The drainage course continues for approximately 1 mile into the southeastern portion of the ASP-plan area, eventually crossing to the south side of Township Road 550 (outside of the plan area) and continuing west until teeing into Ross Creek approximately 1km south of the North Saskatchewan River.

Based on the preliminary findings of the Ecological Assessment outlined in Section 3.2, further investigation will be required if modifications are proposed to the drainage course (i.e. with respect to compensation). At a minimum, appropriate setbacks to the channel should be observed based upon the minimum requirements of the Municipal Government Act and the Subdivision and

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Development Regulations at the time of detailed Outline Plan preparation. As required by the City, geotechnical investigations should be undertaken prior to subdivision to determine the suitability of the lands adjacent to the channel for more intensive industrial development.

Wetlands/Treed Area

As discussed in Section 3.2, there is an existing tree stand and three wetland areas existing in the ASP-plan area. The wetland areas have been heavily modified over time and are deemed to have low ecological value. The tree stand has been deemed to be a sustainable and permanent feature on the landscape and is recommended for retention.

3.3.4 Man-Made Constraints

The significant man-made constraints within the ASP-plan area are shown on **Figure 4.0 – Natural Site Features and Man-Made Constraints** and are detailed below:

Oil, Gas and Petro-Chemical Pipelines

There are numerous pipeline right-of-ways that pass through the ASP-plan area. In total there are seven (7) distinct pipeline corridors, containing a total of 22 separate pipelines. **Table 1 – Pipeline Information Summary** provides additional details regarding the pipelines.

The pipeline corridor identified as “A” contains three operating ATCO Gas and Pipelines Ltd natural gas pipelines, is situated within an approximately 15m right-of-way and is located in the extreme southwest corner of the ASP-plan area.

Pipeline corridor “B” contains two operating ATCO Gas and Pipelines Ltd natural gas pipelines and is located within a 6m wide right-of-way. This right-of-way is approximately 20m north of the north boundary of Josephburg Road and extends east to west for approximately 2 miles along the south portion of the plan area.

Pipeline corridor “C” contains three operating Keyera Energy Ltd high pressure pipelines within a right-of-way that varies in width from 6m to 18m. A portion of the corridor is within an existing power line right of way, and then bends to the northwest where it crosses to the north side of Highway 15.

The largest pipeline corridor (identified as “D” and “E”) originates in the south central part of the plan area and is approximately 66m in width, diverging into two separate corridors (48m in width to the north and 32m in width to the north-east). This corridor contains pipelines belonging to Nova Chemicals, BP Canada Energy Company, Alberta Ethane Development Company, Suncor, Praxair, Shell Canada and Access Pipeline. All of the pipelines contain products that are under extremely high pressures.

Pipeline corridor “F” contains two operating BP Canada Energy Company high pressure pipelines within an 18m wide right-of-way. The corridor enters the plan area from east of Range Road 220,

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

crossing through the centre of the treed area, before bending sharply to the north and crossing Highway 15.

Pipeline right-of-way “G” contains one operating Praxair high pressure pipeline within a 5m wide right-of-way. The corridor enters the southwest corner of the plan area from south of Josephburg Road, heading west before crossing to the north side of Highway 15.

Air Products is currently in the application process to receive approval for the construction of two pipeline segments through the plan area, beginning at their existing facilities in the refinery row area of Strathcona County just outside Edmonton to end at the Williams Energy (Canada) Inc. facility north of the ASP. A significant portion of the right-of-way proposed is adjacent to the existing “D” and “E” pipeline corridors, along with a lateral portion adjacent to the “G” corridor. The pipelines would be to transport hydrogen gas containing no hydrogen sulphide. These alignments will be confirmed upon approval by the ERCB and will be respected within any Outline Plan or detailed subdivisions which are affected.

As the ASP is adjacent to heavy industry and contains a significant number of existing pipelines, it is important to recognize the potential for additional pipeline infrastructure within or adjacent to existing corridors within the plan area.

Historically, the City has provided policy direction regarding the joint use of utility corridors. This ASP continues to encourage the joint use of utility pipeline corridors, transmission lines, and other utility right-of-ways and structures of a compatible nature to minimize adverse visual, environmental, or safety impacts, as well as to minimize the fragmentation of properties. This initiative would be consistent with the City's MDP, which provides the policy direction under Section 9.13 Joint Use of Utility Corridors as follows: Encourage the joint use of utility pipeline corridors, transmission lines, and other utility right-of-ways and structures of a compatible nature to minimize adverse visual, environmental, or safety impacts, as well as minimize the fragmentation of properties.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Table 1: Pipeline Information Summary

License #	Licensee	Status / Substance	Operating Pressure (kPa)	Utility Right of Way Plan	Map Reference ID
1826-59	ATCO Gas and Pipelines (South)	Abandoned/ Natural Gas	0		
4932-1	ATCO Gas and Pipelines (South)	Operating/ Natural Gas	4,450		
1826-61	ATCO Gas and Pipelines (South)	Operating/ Natural Gas	3,450	Gasline R/W 4729HW	Pipeline Corridor "A"
1826-24	ATCO Gas and Pipelines (South)	Operating/ Natural Gas	3,450		
9453-1	ATCO Gas and Pipelines (South)	Operating/ Natural Gas	3,450	Gas Pipeline R/W 3014KS	Pipeline Corridor "B"
1826-25	ATCO Gas and Pipelines (South)	Operating/ Natural Gas	3,450	Gas Pipeline R/W 3014KS	Pipeline Corridor "B"
8594-6	Keyera Energy Ltd	Operating/ High Vapour Pressure Products	4,960		
8478-6	Keyera Energy Ltd	Operating/High Vapour Pressure Products	6,230	Products Pipeline R/W 1913TR	Pipeline Corridor "C"
8588-7	Keyera Energy Ltd	Operating/High Vapour Pressure Products	4,930		
34335-1	Suncor Energy Inc	Operating/Carbon Dioxide	7,290		
27896-3	Praxair Canada Inc.	Operating/Miscellaneous Gases	6,800	Pipeline R/W 952 5272	
19780-23	Shell Canada Limited	Operating/Low Vapour Pressure Products	9,930	Petroleum Products Pipeline R/W 822 1189	Pipeline Corridor "D"
19780-4	Shell Canada Limited	Operating/Low Vapour Pressure Products	9,930	Oil Pipeline R/W 3859NY	
46674-30	Access Pipeline Inc.	To be constructed/Low Vapour Pressure Products	9,930		
43179-1	Nova Chemicals Corporation	Operating/High Vapour Pressure Products	9,930		
14763-36	Nova Chemicals Corporation	Operating/High Vapour Pressure Products	9,930		
9570-1	BP Canada Energy Company	Operating/High Vapour Pressure Products	9,930	Oil Pipeline R/W 5278TR	Pipeline Corridor "E"
13023-16	Alberta Ethane Development Company	Operating/High Vapour Pressure Products	9,930	Gas Pipeline R/W 792 2504	
16967-1	Nova Chemicals Corporation	Operating/High Vapour Pressure Products	9,930		

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

License #	Licensee	Status / Substance	Operating Pressure (kPa)	Utility Right of Way Plan	Map Reference ID
22037-1	BP Canada Energy Company	Operating/Low Vapour Pressure Products	9,930	Light Hydrocarbons Liquids Pipeline R/W 5053TR	Pipeline Corridor "F"
9570-11	BP Canada Energy Company	Operating/High Vapour Pressure Products	9,930		
27896-2	Praxair Canada Inc	Operating/Miscellaneous Gases	6,800	Pipeline R/W 952 1455	Pipeline Corridor "G"

Powerline / Telephone Rights-of-Ways

There are two existing powerline right-of-ways within the southwestern portion of the [ASP-plan](#) area, and as described in **Table 2 – Utility Right of Way Information Summary**. Powerline right-of-way "H" is situated within a 30.5m wide corridor and contains a series of 2 parallel above ground transmission towers which originate at an existing sub-station at the northwest corner of the 114 Street / Highway 15 intersection. The transmission lines extend from the sub-station east through the Alsten Lands Outline Plan, then turn northwards, crossing Township Road 550 and entering the [ASP-plan](#) area approximately 400m east of Range Road 223 and in the approximate centre of the S.W.¼ Sec. 2-55-22-W4, where they proceed northwards and cross to the north side of Highway 15 and continue to the northeast paralleling the CN Railway mainline.

The second powerline right-of-way, identified as "I", is an underground powerline contained within a 9.2m wide corridor in the extreme southwestern corner of the [ASP-plan](#) area. The right-of-way enters the plan area from the corner of Lot 1, Block 1, Plan 0820100 and travels in a northeast direction through the S.W.¼ Sec. 2-55-22-W4, ultimately crossing Highway 15 and continuing north.

It is forecasted that there will be a major increase in the demand for electricity as industrial development continues in the Heartland area. As such, ~~it has been determined that expansion of the electrical transmission system is was expanded required~~ to meet this growing demand. ~~The expansion of the transmission system involved the This will involve the~~ construction of a new high voltage transmission line connecting power generation in the Keephills / Wabamun area (west of Edmonton) to the Heartland region. ~~The transmission line runs within the transportation utility corridor (TUC) through southeast Edmonton and along the western boundary of Strathcona County (west of Sherwood Park), the transmission line then extends north from the TUC into Sturgeon County finally terminating at the Heartland substation. The transmission system is now energized and is capable of supply electricity to development in the region.~~

~~At this time, AltaLink and EPCOR have been directed by the Alberta Electric System Operator to begin preparing necessary documentation and applications. Although no specific routing has been confirmed, four potential routes have been identified, of which two indicate that the transmission line may potentially cross within the ASP area.~~

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Right-of-way “J” is an existing buried, telephone cable within a 5m wide right-of-way. The corridor begins in the ASP-plan area at the intersection of Range Road 221 and Highway 15, traveling southward on the west side of N.W.¼ Sec. 1-55-22-W4 for approximately 1km, eventually heading east 1.6km and out of the plan area.

Table 2: Utility Right of Way Information Summary

Owner	Utility Right of Way Plan	Map Reference ID
Altalink Management Ltd	Powerline Right of Way 6661KS	Utility Corridor “H”
Altalink Management Ltd	Powerline R/W 1110RS	Utility Corridor “I”
Alberta Government Telephones	Buried Cable R/W	Utility Corridor “J”

Utility Right-of-Ways

The plan area does not contain any municipal infrastructure improvements such as sanitary, storm or water mains.

~~The developing Alsten Lands Outline Plan light and medium industrial park to the southwest of the ASP-plan area will eventually construct some of the infrastructure required to support servicing within the plan area. Approximately 189.44 ha of the plan area west of Range Road 221 can be fully serviced; while the remaining plan area east of Range Road 221 will have a reduced level of servicing such as a trickle feed water supply, and a low pressure sanitary sewer network. The developing Alsten Lands Outline Plan light and medium industrial park to the southwest of the ASP area will eventually construct some of the infrastructure required to service a~~

~~small portion of the plan area. This area of the ASP area will be serviceable by the sanitary sewer and water line capacity constructed with the Alsten lands. As development progresses further east, significant infrastructure improvements and upgrades will be required to provide a fully serviceable industrial park.~~

Highways and Roadways

The ASP-plan area is bound on the north and west by Highway 15, a four lane divided urban expressway and part of the provincially designated high load corridor system. Access to ~~the~~ Highway 15 from the subject-landsplan area is currently available from three separate intersections located at Range Roads 220, 221 and Township Road 550.

The south boundary of the plan area is Township Road 550, an improved, two lane rural road under the City of Fort Saskatchewan’s municipal jurisdiction. The roadway terminates slightly west of the plan area where it intersects with Highway 15. The eastern boundary of the plan area is Range Road 220, an unimproved two lane rural roadway. The municipal boundary between the

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

City of Fort Saskatchewan and Strathcona County is on the west side of the Range Road 220 right-of-way. All access to Range Road 220 is controlled solely by Strathcona County. under the jurisdiction of Strathcona County. The only internal roadway is Range Road 221, again an unimproved two lane rural roadway, extending north-south in the ASP.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Josephburg Airport

A majority of the eastern and central portions of the ASP-plan area are affected by Strathcona County's Josephburg Airport Vicinity Overlay zone. This overlay zone applies restrictions to areas in proximity to the airport to buildings and structures and ensures continuing flight safety and air navigation for the airport by prohibiting uses within the zone that would cause excessive radio interference, conflict with aircraft movements, create a fire or explosives hazard, accumulate any material or waste edible by, or attractive to, birds or development that may create glare or lighting that interferes with lights necessary for aircraft landing or taking off.

3.3.5 Parks, Recreation, Open Space and Trails

There are currently no developed parks, recreation facilities or trail systems within the ASP-plan area. There is potential for extending the City's primary trail network into the plan area, as identified in the Recreation, Culture and Parks Facilities Master Plan document. Further details should be considered through development of subsequent Outline Plans.

3.4 TRANSPORTATION

The following discussion outlines the existing transportation characteristics of the ASP-plan area including highways and roadways.

3.4.1 Highway 15

Highway 15 is a four lane divided urban expressway with a concrete median separating opposing traffic for the majority of its length adjacent to the ASP-plan area. The Highway is the main transportation route into the City from the north, intersecting with Highway 21 within the City limits. At the Highway 21/Highway 15 junction, Highway 15 continues to the northeast, accommodating the major transportation into the Heartland area with a 70km/h posted speed limit. In 2005 the Average Annual Daily Traffic west of Range Road 220, as it traveled parallel to the ASP-plan area, was in excess of 9,300 vehicles. There are ~~three~~ four existing traffic signals impacting the ASP-plan area along Highway 15 at the intersections with Township Road 550, the Dow main gate (east of 119 Street) ~~and, Range Road 221, and while the intersection at Range Road 220 is controlled by a stop sign.~~

The Strathcona Area Industrial Heartland Transportation Study Update was completed for Strathcona County by Stantec in ~~2008~~ 2007. Although the area studied was for industrial lands within the boundaries of Strathcona County, the intersection of Highway 15 and Range Road 220 is the eastern boundary of the City and the Highway is an important link into the region, and as such was impacted by this study.

The study, as it affects this ASP, was intended to develop a conceptual major internal road network for the area and establish the characteristics of the road network (major intersection configurations, right-of-ways). The issues identified in the study concluded that heavy traffic

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

volumes on Highway 15 in peak hours cause significant delay at signals through Fort Saskatchewan, and that maintaining reasonable traffic flows through the City is desired.

Key findings and recommendations highlighted in the study indicated the intersection of Highway 15 with Range Road 220 ~~(currently operating under stop sign conditions on the north and south)~~ was a location for potential major intersection improvements. The intersection improvements identified for the intersection of Highway 15 with Range Road 220 have since been completed. Eventually as development evolves, a grade separated interchange at the Highway 15/Range Road 220 intersection would be required to provide adequate capacity for the long-term Daily Operational and major Turnaround traffic demands.

Based on the study, it was recommended that at a minimum traffic signals through the study area as well as through the City be controlled through a centralized traffic control system. Although not a consideration of the study, a more robust approach would be the development of a free-flow by-pass route around the City.

Strathcona County has also undertaken a Functional Planning Study of the Highway 15 corridor between Range Road 220 and Highway 830 north in order to determine medium and long-term needs of the highway, inclusive of future grade-separated intersection locations and their possible footprints. This study is ongoing.

3.4.2 Fort Saskatchewan By-Pass

The Capital Region 10-Year Provincial Highways Plan, a follow up on the Capital Region Integrated Growth Management Plan Report, was released by the Alberta Government to the public in August 2008. The document outlines the province's ~~current~~ transportation funding policy, the municipal grant programs available and the 10-Year highways plan. The plan recognizes the province's priorities with respect to Capital Region highway improvements. Within and in proximity to the ASP-plan area, the plan outlines the following improvements that directly or indirectly impact the ASP-plan area:

- Intersection upgrade at the Highway 15 / Range Road 220 intersection;
- The Fort Saskatchewan by-pass highway, consisting of a new 4-lane roadway and two new interchanges; one at the south end of the City intersecting with Highway 21 and another at the intersection of the proposed by-pass with Highway 15 and Secondary Highway 830 east of the City.

The proposed Fort Saskatchewan by-pass south of the ASP-plan area presents possible opportunities to access the south part of the plan, thus creating additional prospects for end users and increasing accessibility. The by-pass alignment ~~is~~remains ~~still~~ conceptual at this time; however traffic modeling undertaken by the City indicates that the bypass will be required by 2022. As development progresses eastwards in the ASP-plan area a more detailed right-of-way may be available, and therefore prior to approval of an Outline Plan(s) for the eastern portion of

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

the ~~ASP-plan~~ area, the alignment of the by-pass should be confirmed to ensure consistency with regional transportation facilities.

It should be noted that the Capital Region Board Integrated Regional Transportation Master Plan identifies a new river crossing south of Fort Saskatchewan, connecting Highway 28 with Highway 21 and Township Road 540. This river crossing and roadway may, at some future point, constitute part of the Fort Saskatchewan by-pass. For more information see Figure 3 of the Integrated Regional Transportation Master Plan.

3.4.3 Township Road 550

Township Road 550 is currently a paved two-lane, rural cross-section roadway, forming the southerly boundary of the plan area. The roadway has been designated as an arterial roadway classification by the MDP, however at this time it operates as a rural roadway.

3.4.4 Range Road 221

Range Road 221 is an 8m wide paved rural road that is contained within the original 20m (66 ~~ft.~~) government road allowance. The ASP development concept proposes to re-align the majority of Range Road 221, ultimately requiring that the majority of the right-of-way be closed and consolidated with adjacent parcels. The existing intersection locations at Highway 15 and Township Road 550 will remain, and will be upgraded as development warrants and as per future transportation assessment and analysis.

3.4.5 Range Road 220

Range Road 220 is currently an 8m wide gravel rural road that is contained within the original 20m (66 ~~ft.~~) government road allowance and is operated and maintained by Strathcona County. The MDP identifies this with an arterial roadway designation, however at this time there is a minimal volume of traffic. ~~At this time Strathcona County, in partnership with adjacent industry, is currently designing improvements to this roadway.~~ As development progresses towards this part of the plan area, transportation analysis and consultation with Strathcona County will determine the required width of the roadway, cross section, right-of-way requirements and any access opportunities.

3.4.6 Dangerous Goods Routes

Highway 15 is the only designated dangerous goods route in proximity to the ~~ASP-plan~~ area.

3.5 MUNICIPAL SERVICES

The City of Fort Saskatchewan Conceptual Servicing Study, Final Report, June 2006, was prepared by Stantec Consulting Ltd. to provide a conceptual level servicing study for the Alsten

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Lands Outline Plan, as well as the area contained within this ASP. The conceptual study addressed water, sanitary and storm servicing issues within these areas. As part of the review and update to the ASP in 2013/2014, a review of the Servicing Study was completed, and an alternate servicing concept was developed by Scheffer Andrew Ltd. The revised servicing options are detailed in Section 4.8.

The City of Fort Saskatchewan Conceptual Servicing Study, Final Report, June 2006, was prepared by Stantec Consulting Ltd. to provide a conceptual level servicing study for the Alsten Lands Outline Plan, as well as the area contained within this ASP. The conceptual study addressed water, sanitary and storm servicing issues within these areas. The servicing options outlined in the study are detailed in Section 4.8

3.7.3.6 EMERGENCY SERVICES

3.7.13.6.1 Fire, Police, Ambulance and Disaster Services

The City of Fort Saskatchewan's ~~Community and Protective Disaster~~ Services Agency provides fire and disaster services to the ASP-plan area, while emergency medical services ~~is~~ are provided by Alberta Health Services. The ASP-plan area is covered by the City's Fire Hall at the corner of Highway 15 and 101 Street (approximately 2 miles southwest of the ASP-plan area), where part-time fire members, under the direction of a full-time Fire Chief, respond to incidents involving the need for fire suppression, rescue, (motor vehicle collisions, low angle rope rescue), clean up of environment spills or mutual aid to surrounding communities. Two fully equipped ambulances are also located at the main fire station to provide emergency medical services to the City.

Additionally, Strathcona County's Heartland Hall (Station #4) is located approximately 2 miles north-east of the plan area along Highway 15. This station provides full-time protection for heavy industry in the area, with advanced industrial level fire and dangerous goods support.

The RCMP provides police services to the City of Fort Saskatchewan from the central station located west of Highway 15 on 99 Avenue. Additionally, Heartland Hall also serves as a satellite office for the RCMP, special constables and bylaw officers working in north Strathcona County.

The City of Fort Saskatchewan Disaster Services Agency works closely with industrial and municipal partners and all other stakeholders to prevent incidents from occurring that could negatively impact our personal safety.

3.7.23.6.2 Emergency Preparedness

The City of Fort Saskatchewan is a member of Northeast Region Community Awareness and Emergency Response (CAER), a partnership of more than 40 community-minded industries and municipalities dedicated to emergency response and education initiatives in the region.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

A key initiative for the organization is the Community Notification Program. This program consists of (1) an information line residents can call to hear current information about site activities, and (2) notification system Northeast Region CAER members can use to contact residents to notify them of more serious situations in their immediate vicinity.

3.83.7 INDUSTRIAL RISK ASSESSMENT

The City of Fort Saskatchewan retained the services of Doug McCutcheon and Associates, Consulting to undertake an Industrial Risk Assessment (IRA) for the plan area [in 2009](#). The objective of the IRA was to identify and assess what types and levels of risk are in proximity to the plan area as a result of existing and potential heavy industrial activities. The purpose of the assessment was to provide recommendations as to the type and intensity of uses that could be appropriate in the plan area. Summary and selected excerpts from this analysis are provided below; ~~however the complete document should be referenced for specific details.~~

The ~~ASP~~ plan area is in proximity to, and potentially impacted by, industrial activity from several companies in the area, including Sheritt Gordon, Marsulex, Shell Canada and Dow Chemical. In addition, nearby railway and roadway infrastructure are used to transport numerous hazardous materials to and from these sites. The IRA characterizes the risk associated with these into five separate activities that could create incidents that would be typical for these industries, including (1) toxic release, (2) flammable release, (3) explosion damage, (4) pipeline incidents and (5) a boiling liquid expanding vapour explosion (BLEVE).

The analysis for risk involves the consequence of an incident and the probability of it happening. The result is calculated in terms of the potential for fatalities and then compared to an acceptable level of risk as defined through the Major Industrial Accidents Council of Canada (MIACC) criteria for risk based land use planning.

The MIACC risk acceptability criteria describes the level of risk for a member of the public who is inadvertently exposed to an industrial incident must be better than a 1×10^{-6} (1 in a million) chance of a fatality. However as one moves closer to the risk source, the level of risk increases, until it reaches the maximum allowable of 1×10^{-4} (100 in a million) chance of a fatality.

The IRA provides a breakdown of the probability that an industrial accident would occur in proximity to the plan area, and concludes that these values are within an acceptable range for light industrial and commercial zoning according to the MIACC criteria. Sound design, collection of leaks, fire protection systems, operational procedures, emergency planning and other activities will serve to effectively manage the risk to that acceptable level.

The IRA concludes that each of the five scenarios outlined above would have varying degrees of impact on the plan area, as each can create consequences that can potentially cause fatalities. In order to illustrate these, the IRA provides “risk contours” that define the risk level based on distances of 0 km, 1.5 km and 3.5 km from the property line of adjacent industry. For example a 1×10^{-4} (100 in a million) risk contour corresponds to 0 km at the property line of industry. At a

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

distance of 1.5 km from the property line would be the 1×10^{-5} (10 in a million) risk contour. At 3.5 km from the heavy industry property line is the 1×10^{-6} (1 in a million) risk contour.

Based on the MIACC risk based land-use criteria, the IRA recommends that allowable land uses in the plan area be as follows:

- 0 km to 1.5 km (1×10^{-4} to 1×10^{-5}): light to medium industrial land uses such as manufacturing, warehousing and open space (e.g. parkland, golf courses, ~~ete~~etc.)
- 1.5 km to 3.5 km (1×10^{-5} to 1×10^{-6}): commercial and office type uses

Thus, in order to minimize risk, commercial and office type uses should be no closer than 1.5 km to the property line of heavy industry, and light or medium industrial uses to be primarily between 1.5 km and heavy industry.

The IRA recommends the 1.5 km and 3.5 km as appropriate separation distances for managing land uses in the plan area. However, there is an opportunity to incorporate a limited amount of highway commercial uses within the 0 km to 1.5 km risk contour provided that certain safety measures are implemented in the design, construction and operation of the buildings. For example, new development could be designed and built (e.g. structural, mechanical, HVAC, ~~ete~~etc.) to have a calculated risk equal to or lower than the 1×10^{-5} risk criteria at the 1.5 km distance. Site/development-specific risk assessment would demonstrate to the City's satisfaction that the lesser setback is warranted by the incorporation of additional safety measures. Other safety measures may include provision of in-place sheltering, shielding, evacuation programs and ventilation shut-off systems.

As part of the Josephburg Road North Industrial Area Structure Plan review and update, the City of Fort Saskatchewan retained the services of Doug McCutcheon and Associates Consulting, to undertake a Risk Review of the proposed amendments to the plan area in 2014.

The Risk Review reiterates that medium to light industrial uses are appropriate for the plan area and the Highway 15 Vicinity Overlay. Should uses such as office buildings be incorporated into the Highway 15 Vicinity Overlay, safety measures such as; HVAC control and isolation systems using monitors to initiate shutdown, windows that cannot open, and doors capable of sealing tightly, should be incorporated into the building design in order to provide an effective means of sheltering in place should a toxic release happen.

The Risk Review identifies no concerns around acceptable levels of risk in relation to utilizing a trickle water system to service medium to light industrial uses; keeping in mind that a separate fire water system will be utilized for the areas serviced by the trickle water system.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.0 Land Use Concept, Objectives and Policies

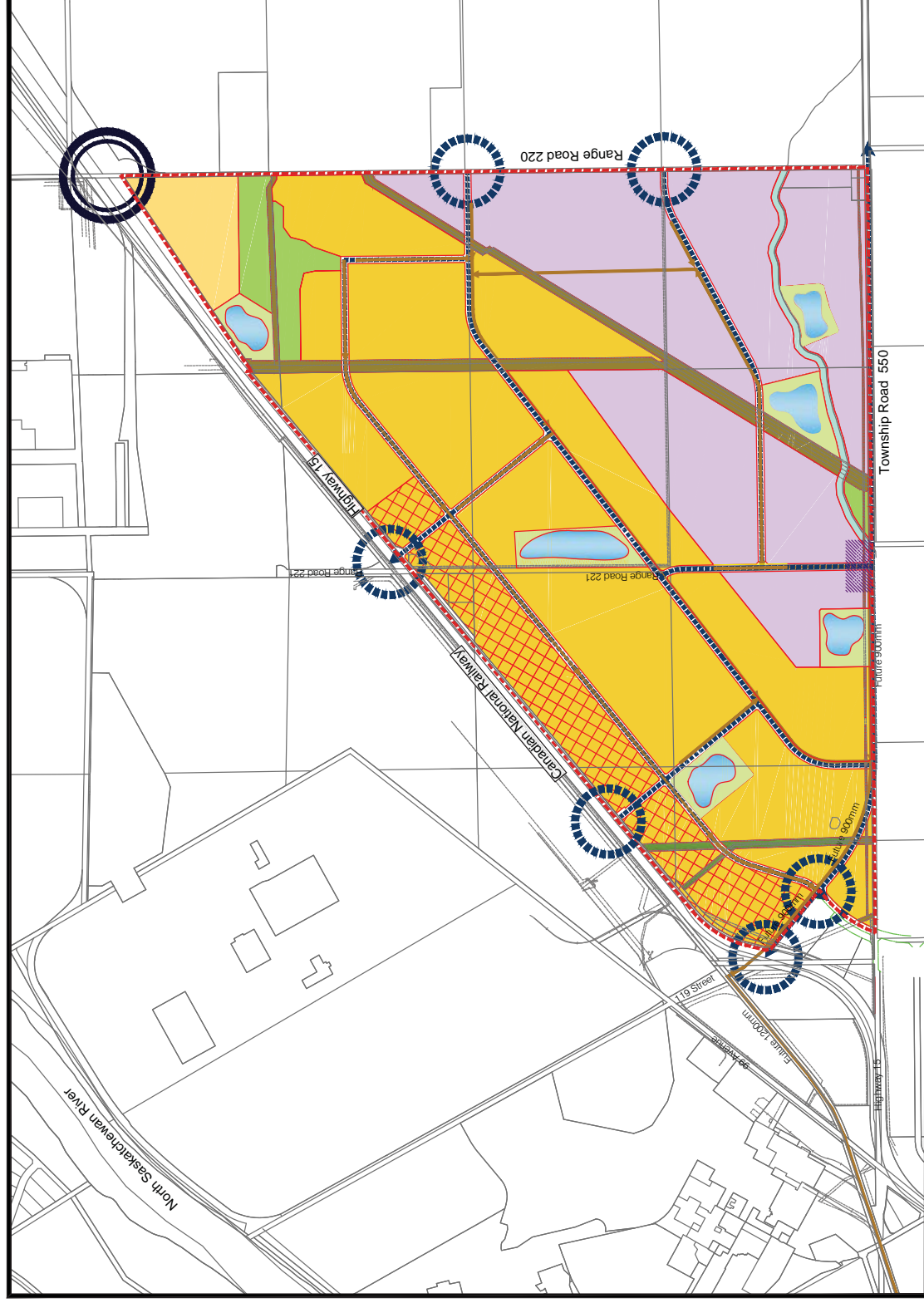
4.1 DEVELOPMENT CONCEPT

A Development Concept is shown on **Figure 5.0 – Land Use Concept**, illustrating a conceptual land use and development framework for the preparation of more detailed Outline Plans and design briefs that need to be approved by the City of Fort Saskatchewan prior to consideration of detailed subdivision and zoning applications.

The Development Concept and associated goals, objectives and policies for the Josephburg Road North Industrial ASP have been determined with regards to the following:

- Policies contained within the City of Fort Saskatchewan MDP, the City of Fort Saskatchewan's Alberta's Industrial Heartland ASP and other relevant plans and studies;
- Natural and man-made constraints;
- Existing and planned improvements to the transportation network;
- Utility servicing constraints and opportunities;
- Economic and strategic development goals of the City of Fort Saskatchewan;
- Environmental considerations;
- Long range planning practices that support logical, cost-effective and beneficial development.

Figure 5.0
Land Use Concept



Josephburg Road North Industrial Area Structure Plan

Bylaw C13-09

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.2 INDUSTRIAL DEVELOPMENT

Goal: To create a functional and sustainable industrial area in which future development is complementary to, and compatible with, the surrounding heavy industry and agricultural uses, roadway network, and important site elements.

Objective	ASP Policy	Implementation
4.2.1 <u>To provide for medium industrial land that will be available to meet the diverse needs of prospective industries. To provide for light and medium industrial land that will be available to meet the diverse needs of prospective industries.</u>	4.2.1(a) <u>Lands in the ASP designated for medium industrial development will be accommodated through application of the IM-Medium Industrial District. Lands in the ASP designated for light and medium industrial development will be accommodated through application of the IL-Light Industrial and IM-Medium Industrial Districts.</u>	4.2.1(a) Figure 5.0 – Land Use Concept <u>will guide the future application of medium industrial land uses. will guide the future application of light and medium industrial land uses.</u>
	4.2.1(b) To allow flexibility in the size of parcels in order to accommodate the space requirements of respective users.	4.2.1(b) The size of the parcels required to accommodate user demands shall be pursued at the Outline Plan, rezoning and subdivision phases.
	4.2.1(c) To provide industrial and business opportunities to serve the employment needs of the Industrial Heartland, the City of Fort Saskatchewan and the Capital Region.	4.2.1(c) <u>Uses which are permitted within the existing IM-Medium Industrial District will be applied. Uses which are permitted within the existing IL-Light Industrial and IM-Medium Industrial Districts will be applied.</u>
4.2.2 Locate and orient industrial parcels along roadways to take advantage of the high visibility and convenient access opportunities.	4.2.2 To ensure internal roadway circulation increases visibility and provides appropriate accesses to the industrial parcels.	4.2.2 <ul style="list-style-type: none"> Figure 5.0 – Land Use Concept illustrates industrial uses with access and frontage along the internal roadways. The internal roadway circulation pattern will be refined and confirmed at the Outline Plan, rezoning and subdivision

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
		phases with regards to the development and staging pattern and logical extension of infrastructure, and may require the completion of detailed Transportation Impact Assessments (TIA).
<p>4.2.3</p> <p>To promote a local Service Oriented node centrally to the plan area.</p>	<p>4.2.3</p> <p>Service-oriented development may occur within the central portion of the ASP area.</p> <p>Uses must only serve the plan area and not be intended to serve a larger area.</p>	<p>4.2.3</p> <ul style="list-style-type: none"> • Figure 5.0—Land Use Concept illustrates the location of the potential Service Centre Node. • The provisions of the IL Light Industrial District will guide the consideration of uses within this node. Typical uses may include small eating establishments, retail stores (general) or other related uses. • Consideration for site-specific direct control development regulations may be given.
<p>4.2.43</p> <p>To apply an appropriate buffer width to protect the existing drainage course from encroaching industrial development.</p>	<p>4.2.34</p> <p>To protect the existing drainage course through the plan area.</p>	<p>4.2.34</p> <p>Appropriate buffers, setbacks and/or landscaping requirements will be determined at the Outline Plan, subdivision or development permit stage at the discretion of the Planning and Strategic Initiatives Department & Development Services.</p>
<p>4.2.45</p> <p>To encourage industrial land uses that support the Alberta's Industrial Heartland, are complementary and compatible with Heavy Industry and have adequate infrastructure to meet current and future industrial needs.</p>	<p>4.2.45</p> <p>Development shall be intended primarily for uses that are complimentary to heavy industrial land uses in proximity, with secondary emphasis on limited highway commercial uses.</p>	<p>4.2.45</p> <p>Development shall comply with the IM-Medium Industrial District of the Land Use Bylaw. Development shall comply with the IL-Light Industrial and IM-Medium Industrial Districts of the Land Use Bylaw.</p>

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Discussion

~~Approximately 168 ha (415 ac) of the gross developable land in the plan area is proposed for light industrial purposes. These uses are located within the south and south-eastern portions of the plan area and are along internal roads and on either side of the existing drainage channel. Light industrial uses provide a compatible and sensitive land use transition to existing agricultural uses in proximity to the plan and are intended to accommodate uses that do not cause any external, objectionable or dangerous conditions outside of any building on the site and will normally be applied to sites adjacent to major roadways on the periphery of industrial areas. Typical uses permitted in this district include automotive and equipment repair and rental (minor), automotive/recreational vehicle sales and rental, contractor, gas bar, service station, storage facility and vehicle wash.~~

~~The land use identified for the entire ASP, consisting of approximately 590 ha (1,459 ac) ha (ac) of land, is for medium industrial purposes. Medium industrial lands are situated as a buffer between the existing heavy industrial development north of Highway 15 and the agricultural lands to the south and east, which is in Strathcona County's boundaries. Note that the lands subject to the Medium Industrial - Highway 15 Vicinity Overlay may permit uses that differ from those traditionally found in medium industrial areas. The primary land use identified, consisting of approximately 233 ha (575 ac) of land, is for medium industrial purposes, in the plan area. Medium industrial lands are situated as a buffer between the existing heavy industrial development north of Highway 15 and the balance of the plan area and are intended to accommodate uses that do not cause any external, objectionable or dangerous conditions beyond the outer limits of the site and are normally located so as to buffer heavier industrial districts from other land uses. Typical uses permitted are similar to the IL District but also include automotive and equipment repair and rental (major), general industrial uses, outdoor storage facility, recycling depot, warehouse sales and warehouse distribution and storage.~~

~~The Land Use Bylaw currently specifies a minimum parcel size of 0.4 ha (0.99 ac) for medium industrial development. The Land Use Bylaw currently specifies a minimum parcel size of 0.2 ha (0.49 ac) for light industrial development and 0.4 ha (0.99 ac) for medium industrial development.~~ Parcel sizes will be identified at the Outline Plan stage, approved through the subdivision process and confirmed at time of legal plan endorsement. The plan intends to allow landowners the flexibility to create a variety of parcel sizes, which are appropriate to the district and would facilitate the creation of economical and marketable industrial land development.

~~Medium industrial uses are located such that access and frontage are along the internal roadway system to increase site visibility and provide for appropriate access opportunities. Light and medium industrial uses are located such that access and frontage are along the internal roadway system to increase site visibility and provide for appropriate access opportunities.~~ Where possible direct access to either Township Road 550 or Range Road 220 may be possible, and would require consultation and/or approval with Strathcona County and the City of Fort Saskatchewan.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

~~A local Service Oriented node is identified in the south central portion of the plan area, at the intersection of Township Road 550 and Range Road 221. The intent of the node is to promote a central location where a concentration of service-oriented businesses can locate and serve the everyday needs of employees of the plan area. Typical uses could include small restaurants, coffee shops or other retail stores under the provisions of the IL Light Industrial District. Consideration for site specific direct control zoning may be considered if the uses proposed are compatible with adjacent light and medium industrial lands uses, and will be at the discretion of the City.~~

The existing drainage course in the south-east portion of the plan area has been heavily modified and re-channelled over many years. As it carries drainage flows from beyond the plan area (Strathcona County) to the North Saskatchewan River, it should be protected from abutting development. The plan identifies and provides policy for its protection and the application of appropriate buffering and setbacks, determined through the Outline Plan and subdivision application processes. A more detailed analysis may be required prior to subdivision approval.

While the focus of the plan is to facilitate primarily industrial development, a limited amount of commercial oriented land uses may be accommodated within a Medium Industrial – Highway 15 Vicinity Overlay designation. The plan illustrates conceptually the location where the Overlay may be applied in consideration of visibility and accessibility from Highway 15 and where access will not interfere with the flow of traffic intended for industrial purposes. In order to accommodate commercial oriented land uses within the Medium Industrial - Highway 15 Vicinity Overlay it may be necessary to prepare a risk assessment to support proposed commercial oriented development.

~~The conceptual locations for medium industrial activities are illustrated on~~
~~The conceptual locations for light and medium industrial activities are illustrated on~~ **Figure 5.0 – Land Use Concept.**

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.3 MEDIUM INDUSTRIAL – HIGHWAY 15 VICINITY OVERLAY

Goal: To provide opportunities for limited commercial oriented land uses, adjacent to Highway 15, which are compatible and complementary to heavy industrial development in proximity to the ~~Plan~~plan.

Objective	ASP Policy	Implementation
<p>4.3.1</p> <p>To provide opportunities for limited commercial oriented land uses immediately adjacent to Highway 15.</p>	<p>4.3.1(a)</p> <ul style="list-style-type: none"> Medium Industrial will be the primary land use in this area. A Highway 15 Vicinity Overlay, with special provisions, will allow for the development of specific commercial oriented uses on lands designated for Medium Industrial. 	<p>4.3.1(a)</p> <ul style="list-style-type: none"> Figure 5.0 – Land Use Concept will conceptually guide the future application of medium industrial uses, with provisions for limited commercial oriented land uses. The Planning and Strategic Initiatives Department shall prepare <u>An update to the a new</u> Highway 15 Vicinity Overlay for <u>in</u> the Land Use Bylaw <u>shall be prepared to reflect the amendments to this plan,</u> which will require Council approval, prior to or concurrent with this plan.
	<p>4.3.1(b)</p> <p>Commercial oriented development will only be considered within the area identified as “Highway 15 Vicinity Overlay”, and shall not exceed approximately ten percent (10%) of the gross developable area.</p>	<p>4.3.1(b)</p> <ul style="list-style-type: none"> Development shall comply with the Highway 15 Vicinity Overlay of the Land Use Bylaw and to the satisfaction of the Planning and Strategic Initiatives Department. Planning and Strategic Initiatives Department shall ensure <u>The Highway 15 Vicinity Overlay commercial-oriented development shall be limited to</u> does not exceed 10% of the Gross Developable Area.
<p>4.3.2</p> <p>To prohibit commercial developments which are not</p>	<p>4.3.2</p> <ul style="list-style-type: none"> Consideration of commercial oriented uses shall ensure compatibility, from a risk 	<p>4.3.2</p> <p>Planning and <u>& Strategic Development Initiatives Services</u> shall review all commercial</p>

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
compatible with heavy industrial uses.	management perspective, with adjacent heavy industrial activities.	oriented development against the Highway 15 Vicinity Overlay and at the Outline Plan, Rezoning, Subdivision and/or Development Permit stage.
<p>4.3.3</p> <p>To implement risk management procedures to ensure that commercial oriented development does not restrict the operation or expansion of heavy industrial uses in proximity to the plan area.</p>	<p>4.3.3</p> <ul style="list-style-type: none"> • Appropriate safety measures may be required for new buildings in the plan area to ensure that the operation or expansion of adjacent heavy industrial uses will not be compromised by commercial oriented uses. • Risk Assessments may be required for any commercial oriented developments to ensure a calculated risk equal to or lower than the 1 x 10⁻⁵ risk criteria at the 1.5 km distance. • Additional safety measures may be required and could include provision of in-place sheltering, shielding, evacuation programs and ventilation shut-off systems. 	<p>4.3.3</p> <p>The requirement for risk assessments/analysis of risk management programs/implementation of safety features shall be directed <u>determined</u> by the <u>Planning and Strategic Development Initiatives Services</u> and <u>Community and Protective Services Departments</u> in <u>consultation with Emergency Services</u>.</p>

Discussion

The land uses parallel and adjacent to the Highway 15 corridor are medium industrial in nature, allowing uses similar to those described in Section 4.2. However, the plan includes provisions for a limited amount of commercial oriented uses under a new Highway 15 Vicinity Overlay. The Overlay would permit a specific range of commercial type uses, at specific locations, within the medium industrial area adjacent to Highway 15. The commercial oriented uses are anticipated to include such uses as animal service facility (minor/major); business/office support services; commercial school; recreational vehicle/heavy equipment sales & rental; and professional services – office, which would require good visibility and accessibility to Highway 15. However, consideration of appropriate uses will ensure they are of the type that does not employ a significant number of people on site and are compatible with heavy industry.

The designation of the Highway 15 Vicinity Overlay within the plan area requires the implementation of a new overlay in the Land Use Bylaw to specifically address the type, intensity, location and risk analysis of compatible uses within this designation.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

The areas suitable for these commercial oriented uses are illustrated paralleling Highway 15, and would be accessible only via the internal circulation network, with no direct access to Highway 15 permitted. ~~The plan proposes approximately 63.0 ha, or approximately 11.8% of the gross developable land area for potential commercial purposes. The plan proposes approximately 53.0 ha, or approximately 10% of the gross developable land area for commercial purposes. It is a policy of this plan that commercial uses within the Overlay will not exceed this amount.~~

The plan area is in proximity to significant heavy industrial activity, both existing and proposed, to the north. The type and intensity of commercial uses adjacent to Highway 15 must be compatible with heavy industry and must be designed and operated to minimize the risk associated with the continued operation of these heavy industrial facilities.

Section 3.7 Industrial Risk Assessment outlines, at a minimum, the requirements for development of commercial oriented uses within the overlay. The intent is for site developments/buildings to be constructed and operated such that they provide safe and efficient evacuation of occupants and/or provide “shelter in place” protection or ventilation shut-off systems against potential risks associated with the heavy industry. Other regulations may be incorporated to ensure adjacent heavy industrial uses will not be compromised by the commercial oriented uses.

Site or development specific risk assessment analysis will be required for each new development within the Overlay to ensure compliance with the requirements of this section and to demonstrate to the City’s satisfaction the uses proposed are compatible with heavy industry.

The conceptual location of the Highway 15 Vicinity Overlay and commercial oriented activities are illustrated on **Figure 5.0 – Land Use Concept**.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.4 UTILITY RIGHT OF WAYS

Goal: To ensure the integrity of existing pipeline and utility corridors by incorporating the right of ways, where feasible, into the development fabric of the plan area.

Objective	ASP Policy	Implementation
4.4.1 Protect pipeline and utility right-of-ways from encroachment by abutting uses.	4.4.1(a) Minor utility rights-of-way will be incorporated into development sites and protected by easement.	4.4.1(a) Easements to protect minor utility rights-of-way will be required as a condition of subdivision approval for the affected lands.
	4.4.1(b) Major utility rights-of-ways not already owned by the operator will be dedicated to the City as public utility lots.	4.4.1(b) Public utility lots to accommodate major utility rights-of-way will be required as a condition of subdivision approval for the affected lands.
4.4.2 To create appropriate buffers between operating pipelines and industrial uses.	4.4.2 Require minimum setbacks from industrial land uses and pipeline and utility corridors.	4.4.2 The minimum setback shall be in accordance with the guidelines of the Energy Resources Conservation Board Alberta Energy Regulators and relevant regulations specified in the Land Use Bylaw.
4.4.3 To recognize the potential for additional pipelines within or adjacent to existing corridors within the plan area.	4.4.3 At the Outline Plan stage determine the need for future pipeline infrastructure.	4.4.3 Outline Plans will be circulated to local industry to determine requirements for future pipeline infrastructure.
4.4.4 To integrate, where feasible, portions of the major utility rights-of-way and Storm Water Management Facilities as open space elements.	4.4.4 Portions of the major utility rights-of-way and the SWMF may incorporate multi-use trails and landscaping to enhance these lands as open space areas.	4.4.4 The provision of multi-use trails and associated landscaping will be reviewed at the Outline Plan stage and may be required as a condition of subdivision approval for the abutting lands.

Discussion

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

The plan area contains numerous pipeline and utility right-of-ways which convey high pressure flammable or volatile petrochemical products and high voltage electricity. These right-of-ways are currently legally registered in the name of the owner/operator of the respective utility and must be protected from encroachment by future uses. In the past, incidents have been caused by third party activities near to the pipelines, and as such these potential impacts must be taken into consideration by respecting existing right-of-ways and development setback distances.

The alignment and location of future pipelines or above ground power transmission lines is not known at this time. As there are a significant number of existing pipeline corridors currently traversing through the plan area, possible future pipelines within or adjacent to these may be considered. Through the Outline Plan and subdivision application stages adjacent industry should be notified to determine if their long range infrastructure requirements affect the land uses proposed for the area. In addition, during the development permit application stage, pipeline operators should be notified, in order to ensure that the specifically proposed development activity does not impact, nor is impacted by the pipeline operations.

Incorporating naturalized landscaping and/or multi-use trails along utility right-of-ways further encourages alternative forms of circulation by providing workers in the area opportunities to walk, cycle or roller-blade at various times during the workday. It also serves to better integrate these corridors into the plan area, while still allowing ease of access to the utility operators for maintenance purposes.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.5 ENVIRONMENTAL MANAGEMENT

Goal: To protect environmentally sensitive areas, where considered sustainable, and promote environmentally responsible and sustainable development practices.

Objective	ASP Policy	Implementation
4.5.1 To encourage the retention, incorporation and enhancement of sustainable wetland areas into the ASP.	4.5.1 Use wetland areas, where feasible, for stormwater management, thereby reducing infrastructure and servicing costs.	4.5.1 As Figure 5.0 – Land Use Concept conceptually illustrates the existing wetlands and drainage course may be integrated into the future stormwater management system, where feasible and environmentally sustainable, to achieve the ASP policy.
4.5.2 To provide adequate buffering between industrial land uses, the existing treed area in the north-east and drainage channel in the south-east part of the ASP.	4.5.2 Require as part of the preparation of the Outline Plan and Design Brief that buffers are integrated adjacent to industrial uses to protect and enhance existing natural features.	4.5.2 The location, width and type of buffer will be determined as part of the preparation of the detailed Outline Plan and Design Brief and to the satisfaction of the Planning & Strategic Initiatives Department <u>Development Services</u> .
4.5.3 To protect the existing treed area in the north-east corner of the plan area.	4.5.3 The treed area will be conserved as a natural area and passive open space.	4.5.3 The tree stand will be acquired by the City of Fort Saskatchewan through <u>Municipal-municipal Reserve-reserve</u> dedication, which will be achieved as a condition of subdivision of the affected lands.
4.5.4 Municipal Reserve Dedication	4.5.4 Take 10% of the Gross Developable Area to be subdivided in the form of municipal reserve land or cash-cash -in lieu.	4.5.4 The amount, form (land or cash- <u>in lieu</u>) and location of municipal reserves will be confirmed through Outline Plan and subdivision applications and to the satisfaction of the Planning & Strategic Initiatives Department .

Discussion

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

A preliminary ecological assessment concluded the presence of three wetland areas within the plan area. Although each of these areas have been heavily modified over time, the opportunity for enhancement as storm water management facilities provides an ecologically sound and sustainable method of storm water treatment on site.

The large contiguous tree stand in the north-east corner of the plan appears to be a sustainable and a permanent feature on the landscape. A variety of wildlife species were observed throughout the stand, either directly or indirectly, including migratory birds and the presence of deer were indicated. In consideration of this, the plan designates the treed area for preservation as ~~Municipal~~ municipal Reserve, with the exact boundary and area to be determined through the Outline Plan and subdivision process. The location of existing underground pipeline infrastructure in proximity to the treed area should be considered.

Municipal reserves will be primarily required as ~~cash-cash~~-in lieu at the time of subdivision. However, the preparation of Outline Plans may demonstrate the need for small parks in strategic locations throughout the plan to provide amenity and recreation opportunities.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.6 TRANSPORTATION

Goal: To provide a functional and effective transportation system that supports development of industrial and limited highway commercial uses within the Plan area and to ensure that adequate land area is protected to facilitate major, future transportation improvements (i.e. Hwy 15 / Range Road 220 interchange);

Objective	ASP Policy	Implementation
4.6.1 Provide safe, effective and functional transportation linkages consistent with the City of Fort Saskatchewan's Transportation Master Plan and the Strathcona Area Industrial Heartland Transportation Study.	4.6.1(a) <u>Transportation access and internal circulation in the ASP plan area will be generally consistent with the network of roadways as conceptually shown on Transportation access and internal circulation in the ASP area will be generally consistent with the network of roadways as conceptually shown on Figure 5.0 – Land Use Concept and Figure 6.0 – Transportation Network. Where warranted, and supported through the preparation of a suitable traffic study, existing road rights-of-way may be utilized as part of the future internal road network.</u>	4.6.1(a) Roadway alignments shown on Figure 5.0 – Land Use Concept and Figure 6.0 – Transportation Network are conceptual. Ultimate alignments and cross-sections will be determined through the preparation of detailed Outline Plans and Design Briefs and to the satisfaction of Planning and <u>Strategic Initiatives Development Services, and Public Works and Engineering Departments Project Management and, where warranted, Strathcona County and Alberta Transportation.</u>
	4.6.1(b) Access to the plan area from abutting roadways will be generally as illustrated on Figure 5.0 – Land Use Concept and Figure 6.0 – Transportation Network	4.6.1(b) <ul style="list-style-type: none"> Access locations to Highway 15 and Township Road 550 will be reviewed and approved at the Outline Plan and subdivision stage. Strathcona County has jurisdiction of Range Road 220 and will review any applications to access from the plan area.
4.6.2 To protect land adjacent to the Highway 15 / Range Road 220 intersection for future right-of-way	4.6.2(a) Intersection improvements have been identified in Alberta	4.6.2(a) The alignment and construction of the intersection improvements will be determined in consultation with

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
requirements for short-term intersection improvements and long-term construction of an interchange.	Transportation's Capital Region 10-Year Provincial Highways Plan.	the City of Fort Saskatchewan, Strathcona County and Alberta Transportation.
	4.6.2(ba) An interchange has been identified in the Capital Region Integrated Growth Management Plan's 35-Year Plan.	4.6.2(ba) The City of Fort Saskatchewan will work with Alberta Transportation and Strathcona County in determining when the interchange is required and the amount of land needed.
	4.6.2(eb) Future roadway networks adjacent to the plan area, within the Industrial Heartland, will be further defined.	4.6.2(bb) The Heartland Association is currently working on their Transportation Master Plan. Recommendations of this report will be taken into consideration as development progresses in this the <u>ASP plan area</u> .
4.6.3 Ensure noise resulting from heavy traffic and along major roadways is taken into consideration and addressed where necessary.	4.6.3 Ensure land uses backing on to Township Road 550 or Range Road 220 provide appropriate landscaping, berms or other enhancements (e.g. building treatments) to assist in noise attenuation and buffering.	4.6.3 Appropriate landscaping, berming and fencing, as required by the Land Use Bylaw, shall be provided along Township Road 550 or Range Road 220 to provide for visual and noise attenuation.
4.6.4 To recognize future pedestrian and bicycle circulation opportunities in the <u>ASP plan</u> area.	4.6.4 A multi-use trail network shall be developed within the <u>ASP plan</u> area utilizing portions of the major utility rights-of-way, stormwater management facilities, the roadway network and the northeast treed area.	4.6.4 The location, alignment and cross-section of a multi-use trail network will be determined through the preparation of detailed Outline Plans and Design Briefs and required as a condition of subdivision approval.
4.6.5	4.6.5	4.6.5

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
Ensure roadways are built to a standard acceptable to the City of Fort Saskatchewan's Development Guidelines and Engineering and Service Standards.	<u>A combination of rural and urban roadway cross-sections will be provided. Within the Medium Industrial - Highway 15 Vicinity Overlay urban road cross-sections are preferred. Primarily urban roadway cross-sections, based upon City development and engineering standards, will be incorporated into the ASP area.</u>	Roadway standards and subsequent rights-of-way s, whether urban or rural, will be determined during the preparation of the Outline Plan and Design Brief and will be required as part of the dedicated through subdivision conditions.
<u>4.6.6</u> <u>To facilitate the installation of water and wastewater services, in a flexible manner.</u>	<u>4.6.6</u> <u>Roadway cross-sections (with adjacent easement areas as necessary) are to be designed in order to accommodate the installation of any deferred infrastructure.</u>	<u>4.6.6</u> <u>Roadway standards and subsequent rights-of-way, whether urban or rural, will be determined during the preparation of the Outline Plan and Design Brief, and will be required as part of the dedicated through subdivision conditions.</u>

Discussion

The circulation pattern within the ~~ASP-plan~~ area provides for the safe and efficient movement of traffic throughout the plan area and beyond. The proposed hierarchy of roadways includes collector and local industrial roadways. Construction of these roadways will be to either a rural standard (e.g. ditches) or full urban standard with curb and gutter, and will be determined at the Outline Plan and Design Brief stage, to the satisfaction of the City. The road network within the area subject to the Highway 15 overlay is intended to be urban in nature, with curb and gutter in order to reflect the more commercial nature of the area. In order to promote walkability, the provision of a 3.0m multi-use trail on at least one side of most roads in the plan area will allow pedestrians the choice to circulate throughout the plan and amenity areas (storm water facilities and open spaces) in a relatively safe and efficient manner.

The plan identifies the location of the three major all-directional access points into the plan from Highway 15. The southernmost access is proposed at the intersection of the future re-aligned Township Road 550 and 119 Street. The second access is located at the existing location across from the Dow main gate, while the third access is at the intersection of Range Road 221 (which may be realigned and improved) and the northerly access to the Dow site. ~~The second access is located at the existing location across from the Dow main gate, while the third access is at the intersection of the future re-aligned Range Road 221 and northerly access to the Dow site.~~ Each of these all-directional access points occurs at existing intersection locations, and will provide the main access routes to the plan area.

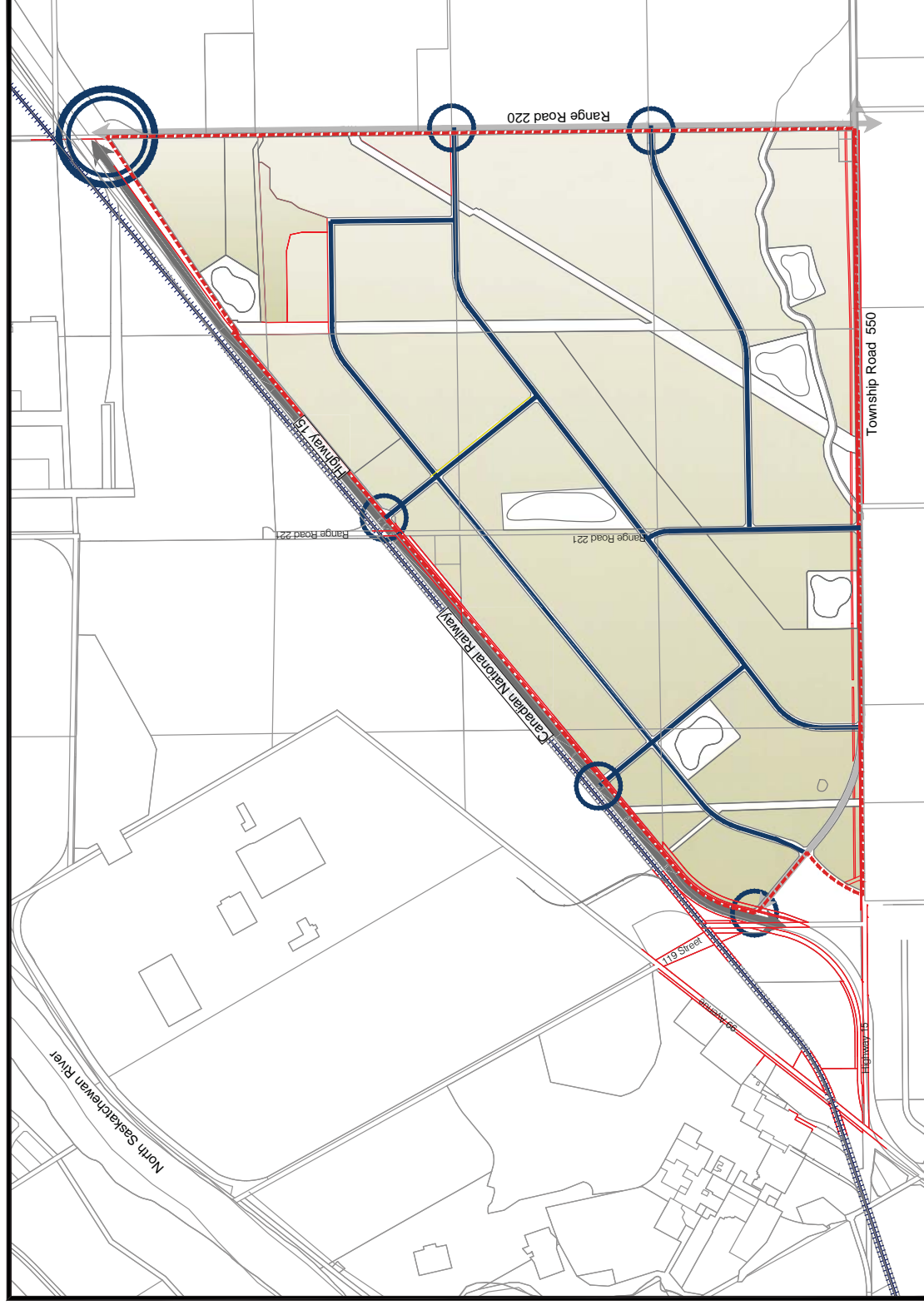
JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

However, detailed traffic impact assessments will be required through Outline Plans prepared for all lands in this area, which will confirm the exact access type and location, intersection requirements/improvements, traffic signalizations, internal road alignments and cross-sections, to the satisfaction of the City of Fort Saskatchewan.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

Figure 6.0
Transportation
Network



- Expressway (4 lane divided)
- Township/Range Road
- Industrial Collector Roadway
- Canadian National Railway
- Proposed All-Directional Access
- Potential Interchange
- ASP Boundary

Josephburg Road North Industrial

Area Structure Plan

Bylaw C13-09

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Vehicular access to the plan area from Township Road 550 and Range Road 220 is conceptually illustrated **Figure 5.0 – Land Use Concept** and **Figure 6.0 – Transportation Network**. The purpose is to permit all-directional access directly into the plan at regular intervals (i.e. 400 m or greater). Township Road 550 and Range Road 220 are not anticipated to carry significant traffic volumes, and consideration may be given to providing individual parcels direct access at specific locations, at the discretion of the City of Fort Saskatchewan and with regards to detailed traffic assessment analysis. As Range Road 220 is the responsibility of Strathcona County, any proposal which require access to this roadway will require their review.

The Province of Alberta has identified the intersection of Range Road 220 and Highway 15 as the location of a future interchange within the next 35 years. Within the next 10 years, this intersection has been identified for intersection improvements to maintain and improve traffic flow to the Industrial Heartland. Given the long-term horizon of the interchange, it is difficult to determine the extent of land required for a right-of-way and construction. However to protect for this possibility, the lands north of the tree stand (proposed as ~~Municipal-municipal Reserve~~) have been acknowledged as future road right of way. The objective is to ensure sufficient land is set aside for the Outline Plan stage or the interchange is deemed a priority.

Adjacent to the plan area are lands within Strathcona County primarily used for agricultural purposes, containing few residential dwellings. However, as the plan area builds out, improvements to Township Road 550 and/or Range Road 220 will increase the level of traffic and noise impacting these areas, and industrial development may create negative visual impacts. Consideration for landscaping or noise attenuation within the plan area to mitigate this shall be considered as the lands and roadways develop. ~~Consideration for landscaping or noise attenuation within the plan area to mitigate this should be considered as the lands and roadways develop.~~

Individuals employed in the plan area may seek opportunities for exercise and passive or active recreation before, during or after work. The inclusion of a 3.0m multi-use trail on at least one side of most roadways, the opportunity to create a separate multi-use trail system, the enhanced landscaping of storm water management facilities and the use of utility or pipeline corridors will provide the necessary means for employees to recreate throughout the day. The specific location and alignment of multi-use trails within utility or pipeline corridors, along roadways and within storm water management facilities will be determined through the Outline Plan, subdivision and development agreement stages.

It is anticipated that the plan area will be accessible via a series of collector and local industrial roadways. The collector roadway network is flexible in nature and is conceptually illustrated in **Figure 6.0 – Transportation Network**, while the local roadway network will be confirmed through the Outline Plan and subdivision processes. Deviations from the conceptual roadway network are to be supported by a suitable traffic study, prepared to the satisfaction of the City of Fort Saskatchewan. The appropriate roadway cross-sections will be confirmed through the development of individual Outline Plans, detailed transportation impact assessments and to the satisfaction of the City of Fort Saskatchewan. Urban roadway cross-sections will be the

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

~~predominant requirement within the plan area subject to the Highway 15 overlay; with the remainder of the plan area anticipated to consist of rural roadway cross-sections. It is anticipated that the plan area will be accessible via a series of collector and local industrial roadways. The collector roadway network is conceptually illustrated while the local roadway network will be confirmed through the Outline Plan and subdivision processes. The appropriate roadway cross-sections will be confirmed through the development of individual Outline Plans, detailed transportation impact assessments and to the satisfaction of the City of Fort Saskatchewan. Although urban roadway cross-section will be the predominant requirement within the plan area, consideration for rural roadway cross-sections may be given at the discretion of the City of Fort Saskatchewan Public Works and Engineering Department.~~

4.7 SUSTAINABLE DEVELOPMENT PRACTICES

Goal: To promote sustainable development practices within the **ASP-plan** area.

Objective	ASP Policy	Implementation
4.7.1 To consider alternative development standards which promote innovation and high levels of environmental and economic performance.	4.7.1 Minimize environmental impacts through the application of sustainable development practices (i.e. bio-swales as a complement to storm sewers; reduced street lighting levels to reduce energy consumption and light pollution; alternative road construction to accommodate recycled materials, grey water re-use, etc etc.)	4.7.1 Application of alternative development standards will be reviewed through the preparation of detailed Outline Plans and Design Briefs, <u>and finalized conditioned at the time of subdivision approval, executed through subdivision approval and associated servicing agreements and implemented through the Land Use Bylaw.</u>

Discussion

Sustainable development practices are intended to be used for site and facility design to take advantage of synergies between various building systems and industrial processes. For development within the plan area, sustainable development guidelines could be applied, where appropriate, at the Outline Plan, subdivision, development agreement or development permitting processes.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.8 MUNICIPAL SERVICING

Goal: To ensure that municipal utility services are provided in accordance with the City of Fort Saskatchewan standards and extended in a logical and coordinated manner to meet the needs of present and future growth;

Objective	ASP Policy	Implementation
<p>4.8.1</p> <p><u>To ensure that suitable contributions are made, be they financial or physical, to shared infrastructure within each servicing basin, so as to</u></p> <p>accommodate<u>accommodate the ultimate servicing concepts as outlined within the ASP. To ensure that the ASP area is serviced to a full urban standard.</u></p>	<p>4.8.1(a)</p> <p><u>A suitable level of service for Sanitary, Storm and Water services, will be</u></p> <p>determined<u>finalized through subdivision approval and associated servicing agreements with by the proponent of a subdivision, but must have</u> regards <u>to the ultimate servicing concepts outlined within the ASP and the Outline Plans. Sanitary, storm and water infrastructure shall be provided in accordance with the City of Fort Saskatchewan's Development Guidelines and Engineering Standards.</u></p> <p>4.8.1(b)</p> <p><u>Development within the plan area west of Range Road 221 shall be fully serviced; while development within the plan area east of Range Road 221 shall have a reduced level of servicing. Areas subject to a reduced level of servicing are to be served by a trickle water feed and a low pressure sanitary system. Fire protection will be provided via dry hydrants connected to suitably sized stormwater management facilities. Unless otherwise determined by the City of Fort Saskatchewan, all development</u></p>	<p>4.8.1(a)</p> <p><u>The ultimate sanitary, storm and water infrastructure systems shall be determined through the preparation of detailed design briefs during the preparation of the Outline Plans.</u></p> <p>Sanitary, storm and water infrastructure shall be determined through the preparation of detailed design briefs and provided in accordance with the City of Fort Saskatchewan's Development Guidelines and Engineering Standards.</p> <p>4.8.1(b)</p> <p><u>Proponents of a subdivision, shall accommodate the ultimate servicing requirements by providing for suitable rights-of-way, and contributing to the overall construction of the system by either constructing municipal infrastructure, or contributing financially, through a municipal levy, for the future installation of municipal infrastructure.</u></p> <p>4.8.1(c)</p> <p>Engineering d<u>Design reports</u>briefs <u>will guide the location and extent of overland</u> coveyance<u>conveyance</u>, <u>as well that of the underground system. Underground systems are to be utilized within the plan area subject to the Highway 15 Overlay;</u></p>

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
	<p>within the plan area shall be serviced to a full urban standard.</p> <p>4.8.1(c) <u>A combination of overland storm drainage conveyance and underground systems will be utilized. The City may consider alternative methods of overland storm drainage conveyance in place of underground systems.</u></p>	<p><u>while a combination of overland conveyance and underground systems may be used for the remainder of the lands. Preparation of engineering design reports supporting the use of alternative methods of storm drainage will guide the location and extent of overland conveyance, to the satisfaction of the City of Fort Saskatchewan.</u></p>
	<p>4.8.1(d) <u>Should the end user desire a level of service in conformance with the ultimate servicing concept and proposed Outline Plan, the end user shall be responsible for extending the necessary infrastructure.</u></p>	<p>4.8.1(d) <u>Should the end user desire a level of service in conformance with the ultimate servicing plan, and proposed Outline Plan, they shall provide suitable design drawings to the City and construct the infrastructure to City standards. The end user may recover costs from other benefitting end users, when and if they tie into the infrastructure.</u></p>
	<p>4.8.1(eb) All shallow utility infrastructure required to provide service to development will be located underground, unless otherwise directed by the City of Fort Saskatchewan.</p>	<p>4.8.1(eb) Underground utility services will be required as a condition of subdivision and executed through the associated servicing agreements.</p>
<p>4.8.2 To require more detailed servicing information at the Outline Plan and Design Brief stages.</p>	<p>4.8.2 Development and servicing shall proceed in accordance with approved Outline Plans and Design Briefs prepared for the associated lands.</p>	<p>4.8.2 <u>Development shall proceed having regards to the Servicing Concepts as outlined within the ASP and as per Outline Plans and Design Briefs approved for the associated lands. The costs of preparing any required reports shall be borne by the benefitting</u></p>

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
		developer(s). Development shall proceed having regards to the 2006 Conceptual Servicing Study and as per Outline Plans and Design Briefs approved for the associated lands. The costs of preparing any required reports shall be borne by the benefiting developer(s).
<p>4.8.3</p> <p><u>Provide for a flexible servicing and phasing scheme having regard for the economical and efficient extension of City services and utilities.</u>Provide a servicing and phasing scheme based on the economical and efficient extension of City services and utilities.</p>	<p>4.8.3</p> <p><u>Development and servicing should occur sequentially, through the extension of storm, sanitary and water systems; flexibility in the sequence of development and servicing shall be</u>accommodated<u>accommodated</u><u>where warranted by the proponent of a subdivision and where supported by the City.</u>Development and servicing shall occur sequentially, through the extension of existing storm, sanitary and water systems.</p>	<p>4.8.3</p> <p><u>Development shall occur when it is feasible to do so, balancing the needs of the proponent and that of the City. Development shall adhere to the servicing concepts outlined in the ASP, Outline Plans and Design Briefs.</u>Development shall occur when it is feasible to do so, and shall proceed according to the 2006 Conceptual Servicing Study, Outline Plans and Design Briefs.</p>
<p>4.8.4</p> <p>To utilize natural systems to provide surface drainage and stormwater management, where possible</p>	<p>4.8.4</p> <p>To re-create natural drainage patterns, where feasible, and utilize such patterns to provide storm drainage services within the ASP <u>plan</u> area.</p>	<p>4.8.4</p> <p>The precise locations, sizes, configuration and number of stormwater sites will be determined at the time of Outline Plan and subdivision plan.</p>
<p>4.8.5</p> <p><u>Utilize stormwater management facilities to provide water for fire protection purposes.</u></p>	<p>4.8.5</p> <p><u>Stormwater management facilities shall provide sufficient fire water to service all of the plan area which is not fully serviced as identified in Figure 7.0 – Conceptual Water Servicing.</u></p>	<p>4.8.5(a)</p> <p><u>A system of dry hydrants, connected to the stormwater management facilities, shall be defined in the Outline Plan and Engineering Design Briefs, to the satisfaction of the City.</u> <u>4.8.5(b)</u></p>

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
		<u>A system of dry hydrants, connected to suitably sized stormwater management facilities, shall be required as part of any development, and shall be in accordance with the Outline Plan and Engineering Design Briefs approved by the City.</u>
4.8. 65 To encourage the joint use of utility pipeline corridors, transmission lines and other utility right-of-ways.	4.8. 65 Use the pipeline / utility corridors as multiple use corridors to accommodate oil, natural gas and product pipelines, utilities such as electrical transmission lines and communications infrastructure.	4.8. 65 Pipeline and utility companies shall be notified during the subdivision application and development review process.

Discussion

Servicing required for the development of the ASP lands will be paid for and constructed either by the developer, or by the City upon satisfactory collection of municipal levies. Servicing will be provided to a full urban standard with municipal water, sanitary and storm sewers for those areas subject to the Medium Industrial - Highway 15 Vicinity Overlay; with full water and sanitary servicing being provided to the remainder of the plan area west of Range Road 221.

For the remainder of the plan area east of Range Road 221, servicing shall be provided at a reduced level with a trickle water service, low pressure sanitary sewers, and a stormwater collection system consisting of underground and surface conveyance. Installation of infrastructure will be provided in accordance with approved studies (i.e. design brief) and to City of Fort Saskatchewan approved standards.

~~Servicing required for the development of the ASP lands will be paid for and constructed by the developers and will be provided to a full urban standard with municipal water, sanitary and storm sewers connecting to existing infrastructure in a logical and coordinated manner. Installation of infrastructure will be provided in accordance with approved studies (i.e. design brief) and to City of Fort Saskatchewan approved standards.~~

The 2006 City of Fort Saskatchewan Conceptual Servicing Study Final Report provided a conceptual level servicing for these lands, in conjunction with the Alsten Lands Outline Plan to the south-west. In 2013/2014 the initial water, sanitary and stormwater servicing concepts for the

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

plan area were reviewed and alternate servicing concepts, blending full and reduced servicing levels were prepared. Further, more detailed technical reports will be required through subsequent stages of development (i.e. Outline Plans, subdivision, etc.) in order to adequately address the provision of water, sanitary and stormwater services.

A Conceptual Servicing Study was completed in 2006 to provide a conceptual level servicing study for these lands, in conjunction with the Alsten Lands Outline Plan to the south-west. The conceptual study addressed water, sanitary and storm servicing issues within these areas. Although the study provides information relating to the plan area, more detailed technical reports will be required through subsequent stages of development (i.e. Outline Plans, subdivision, etc).

Water Servicing Concept

It is the City's intent through the Medium Industrial Off-Site Levy program to construct a combined 450mm and 350mm watermain to support this plan area, and meet fire flow demands in the Alsten Development. The 450mm watermain will follow a route along the north boundary of the Alsten Developments (just behind the Heartland Center) with the 350mm line to extend north and connect to the existing watermain at the intersection of 119th Street and 99th Avenue as per **Figure 7.0 – Conceptual Water Servicing**. It is the City's intent through the Medium Industrial Off-Site Levy program to construct a 450mm watermain to support this plan area, and meet fire flow demands in the Alsten Development. The 450mm watermain will follow a route along the north boundary of the Alsten Developments (just behind the Heartland Center) and loop back to Alsten at 118A Street.

The 2006 City of Fort Saskatchewan Conceptual Servicing Study Final Report determined that all of the Alsten Lands Outline Plan area and approximately one quarter section of land in this ASP can be serviced with the construction of the above noted 450mm water main. The October 2014 City of Fort Saskatchewan Josephburg Road North Industrial Water Modelling Study - Update Technical Memorandum identifies that the plan area west of Range Road 221 can be fully serviced. Approximately 189.44 ha within the plan area will be fully serviced with municipal water, which includes the approximately 63.0 ha Highway 15 Vicinity Overlay, as identified in **Figure 5.0 – Land Use Concept**. Capacity is limited for the development of the remaining lands; as a result, the majority of the ASP lands are to be serviced with a trickle water feed service to provide potable water only.

The servicing study determined that all of the Alsten Lands Outline Plan area and approximately one quarter section of land in this ASP can be serviced with the construction of the above noted 450mm water main. Should development of the ASP progress further north and east, capacity will no longer be available in this existing infrastructure to provide adequate water flow. The study outlines that a new water reservoir and pump house would be required in order to provide an acceptable level of water service to the remaining lands.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Figure 7.0 – Conceptual Water Servicing conceptually illustrates the possible water servicing concept for the ASP lands, based on the completion of infrastructure in the Alsten Lands Outline Plan.

Sanitary Servicing Concept

The Alberta Capital Region Wastewater Commission (ACRWC) pumping station and siphon is located over 2 km west of the ASP-plan area, along River Road and north of 104 Street. All sanitary flows in the City of Fort Saskatchewan are directed to this location.

The existing sanitary sewer system in the Eastgate Business Park is adequate to accommodate all of the light and medium industrial development within the Alsten Lands Outline Plan. These flows are directed to an existing 1200mm trunk sewer at the intersection of Highway 15 and 112 Street.

For development to commence in the Josephburg Road North Industrial ASP, it will be necessary to construct a lift station and a 450m forcemain connecting to the existing 1200mm pipe at 99 Avenue and 112 Street. The plan area will be served by a combined gravity and low pressure system. The gravity system will serve the area to be fully serviced with municipal water west of Range Road 221, while the low pressure system will service the remainder of the plan area east of Range Road 221.

Figure 8.0 – Conceptual Sanitary Servicing conceptually illustrates the possible sanitary sewer servicing concept for the ASP lands. All development must comply with the ACRWC principles and guidelines.

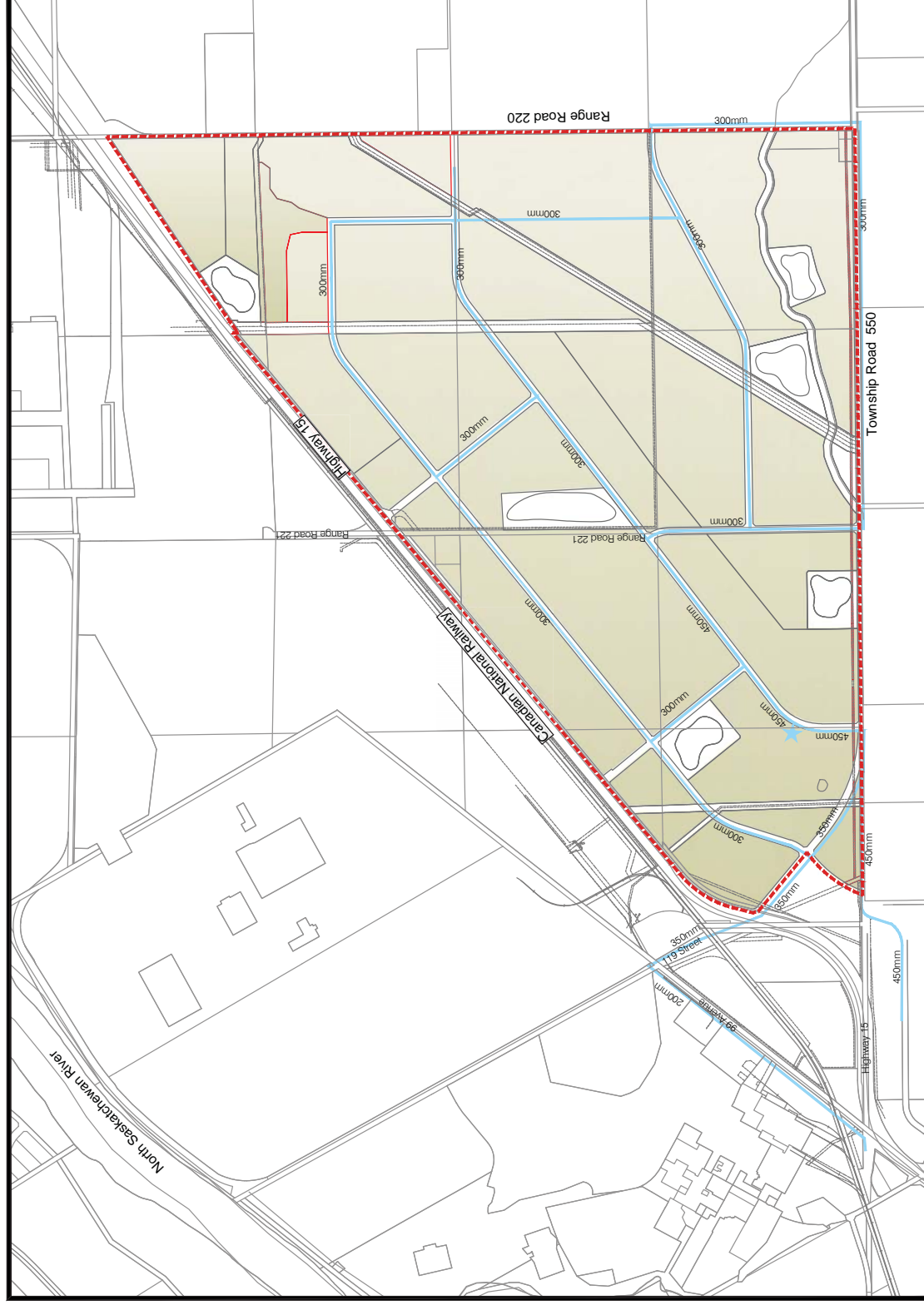
~~For development to commence in the Josephburg Road North Industrial ASP, a new, off-site sanitary trunk, ultimately connecting to the existing 1200mm pipe at 99 Avenue and 112 Street, must be constructed. Once completed, approximately one quarter section of the ASP area could then be serviced (in addition to all of the Alsten Lands Outline Plan), before substantial upgrades must be made to an existing portion of the sanitary trunk sewer along River Road. The existing system does not have adequate capacity to convey the design flows from the entire ASP area under full development conditions; resulting in some surcharging of the system along River Road, just before the ACRWC pump station and siphon.~~

~~When the ASP develops past the one quarter section limit and exceeds the existing sanitary sewer capacity, approximately 385m of the existing 900mm trunk sewer along River Road will either have to be upgraded to a 1200mm pipe, or twinned with another 900mm pipe in order to adequately handle the sanitary sewer flows anticipated for the entire ASP area.~~

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

Figure 7.0
Conceptual
Water Servicing



JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

~~Figure 8.0 – Conceptual Sanitary Servicing~~ conceptually illustrates the possible sanitary sewer servicing concept for the ASP lands. All development must comply with the ACRWC principles and guidelines.

Storm Servicing Concept

A heavily modified, minor tributary of Ross Creek (in the south-eastern part of the plan, crossing to the south side of Township Road 550) forms the principal drainage feature through the plan area. The east side of the ASP-plan area slopes from 630m in the west to 626m near the middle. The west half is very flat with an elevation of approximately 626m. Runoff in the west flows away from the high point in all directions whereas to the north runoff collects in a depression. The runoff to the east and south is ultimately collected in a ditch that runs along the south side of Township Road 550. This ditch carries flows east to a tributary of Ross Creek. The runoff on the east half of the ASP-plan area collects in the low lying areas, however under large rainfall events, the runoff would be directed to the ditch to the south and eventually into the tributary of Ross Creek.

Three wetland areas have been identified within the ASP-plan area. Although these wetland areas have been heavily modified over time, are deemed to have low ecological value and retain water only on an intermittent basis, their incorporation into the proposed development as stormwater management facilities may be a highly desirable means with which to retain the wetlands on the landscape, given the proposed industrial development. However, development would require regulatory approvals or compensation under the Water Act, Public Lands Act and Environmental Protection Act.

~~The proposed development concept identifies the general location and conceptual size of seven (7) storm water management facilities (SWMF) within the ASPplan area. These SWMF's will be sized in order to supply fire water for future development within the portion of the plan area serviced by a trickle water system and not fully serviced by municipal water services, as identified in Figure 7.0 – Conceptual Water Servicing. The SWMF will be outfitted with a dry hydrant system, acceptable to the City of Fort Saskatchewan. It is proposed that a piped outlet system will convey any discharges from each SWMF to Ross Creek. The proposed development concept identifies the general location and conceptual size of seven (7) storm water management facilities (SWMF) within the ASP area. It is proposed that a piped outlet system convey the discharges from each SWMF to Ross Creek.~~

The storm runoff from the developed ASP-plan area will discharge into Ross Creek through a proposed outfall near an existing storm outfall (near Highway 15 and 99 Avenue). During detailed design stage when more accurate information is available, the possibility of utilizing the existing outfall for these areas needs to be explored.

Consideration for alternative methods of overland storm water conveyance may be possible to reduce the cost of the storm sewer system by using drainage ditches and strategically placed public utility lots instead of underground infrastructure. A determination will be made upon the

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

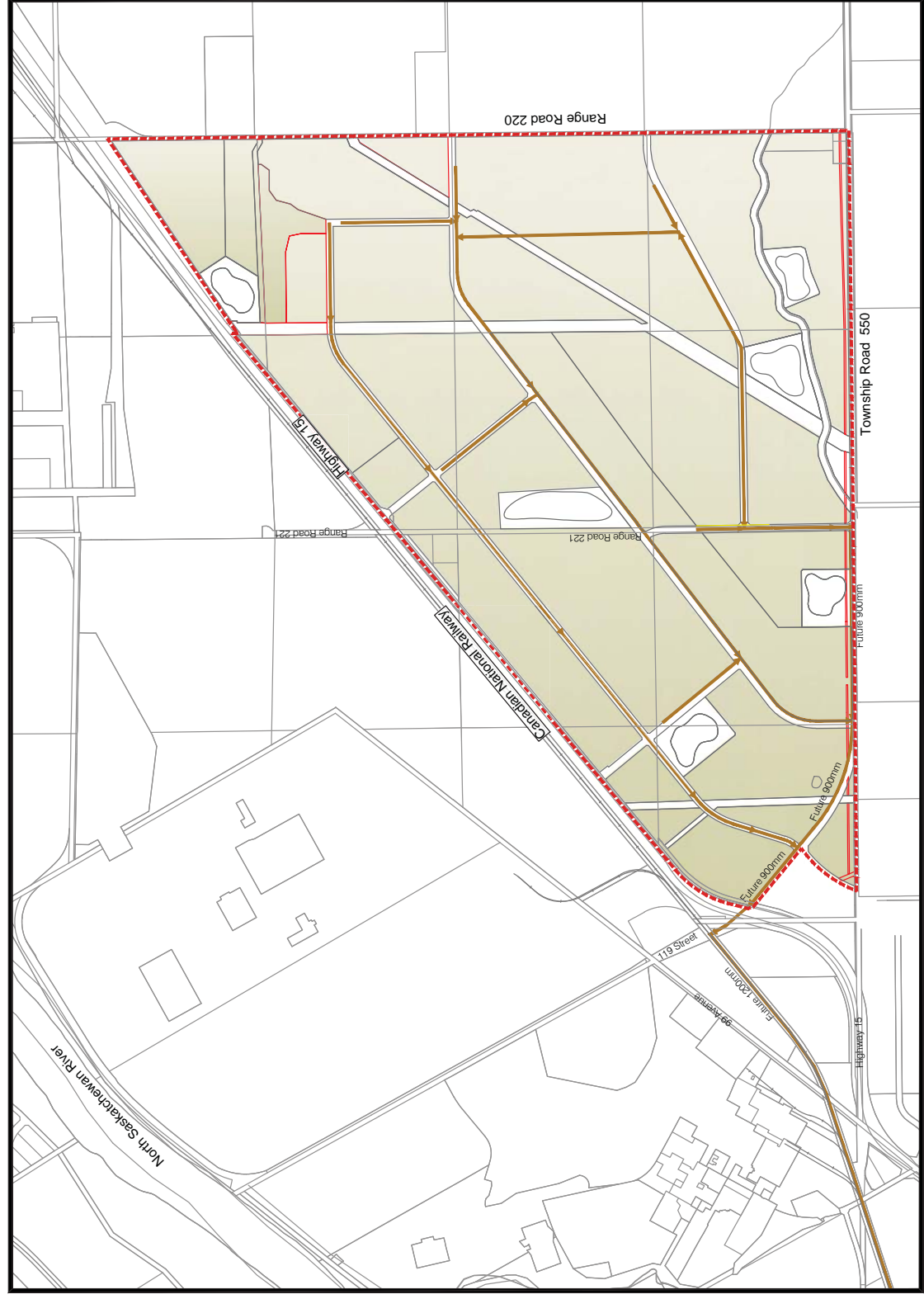
preparation and review of appropriate ~~engineering~~ design ~~reports~~ briefs by the landowners and at the discretion of the City of Fort Saskatchewan.

Figure 9.0 – Conceptual Storm Servicing conceptually illustrates the proposed storm sewer servicing concept for the ~~ASP~~ plan area.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

Figure 8.0
Conceptual
Sanitary Servicing



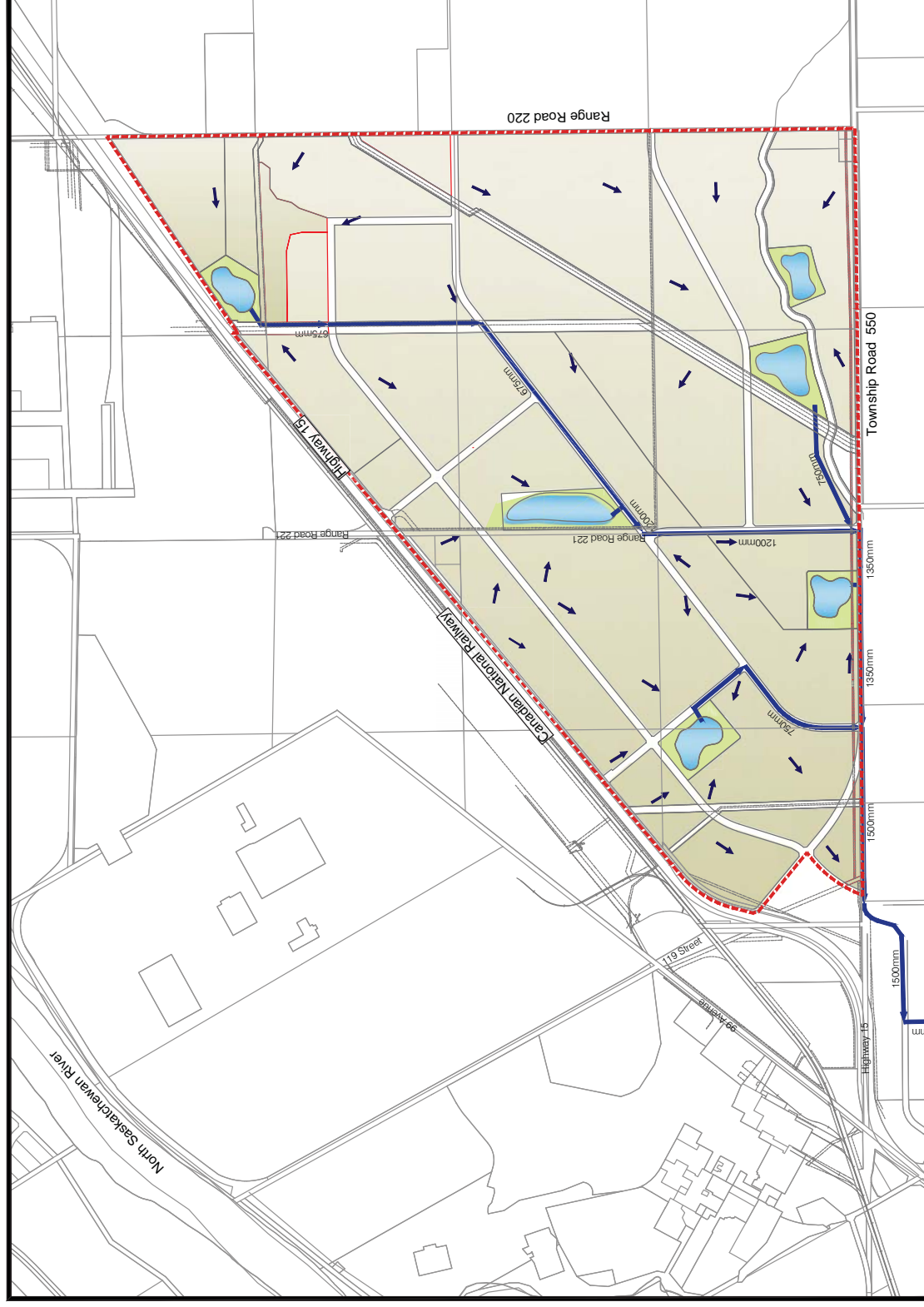
 Sanitary Trunk (conceptual alignment)
 ASP Boundary

Josephburg Road North Industrial Area Structure Plan

Bylaw C13-09

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Figure 9.0
Conceptual
Storm Servicing



Josephburg Road North Industrial

Area Structure Plan

Bylaw C13-09

- 1350mm  Proposed Storm Trunk (conceptual alignment)  General Direction of Stormwater Flow
-  Stormwater Management Facility  ASP Boundary

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Shallow Utilities (Power, Gas, Telecommunications)

Shallow utilities such as power, gas and telecommunications systems will be extended from existing locations, at the cost of the developer, and subject to approvals by the appropriate utility agencies.

Pipeline / Utility Corridors

In recognition of the proximity of the plan area to heavy industry and the existing pipeline and utility corridors within the plan area, utility companies shall be notified during the Outline Plan, subdivision application and development review process. Cooperation and communications with these companies will provide them with the ability to adequately plan their long term infrastructure and land right-of-way requirements, while integrating with the land uses proposed within the plan area.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.9 HIGHWAY 15 CORRIDOR DESIGN GUIDELINES

Goal: To control the appearance and quality of development along Highway 15, recognizing its importance as a gateway to the City of Fort Saskatchewan and the Alberta's Industrial Heartland.

Objective	ASP Policy	Implementation
4.9.1 Promote a high standard of design and aesthetics within the public and private lands abutting Highway 15.	4.9.1(a) <u>The City of Fort Saskatchewan should endeavor to develop and implement a statutory plan overlay in the Land Use Bylaw, consistent with Section 6.8 - Industrial, and more specifically policy 6.8.4 of the MDP, with respect to design guidelines for industrial uses along Highway 15 to ensure a higher standard of appearance for the major gateway into Fort Saskatchewan and the Alberta's Industrial Heartland.</u> The City of Fort Saskatchewan should endeavor to develop and implement a statutory plan overlay in the Land Use Bylaw, consistent with Section 7.5 Industrial Site Development of the MDP, with respect to design guidelines for industrial uses along Highway 15 to ensure a higher standard of appearance for the major gateway into Fort Saskatchewan and the Alberta's Industrial Heartland.	4.9.1(a) The statutory plan overlay design guidelines should address, but not be limited to, such considerations as building design, setbacks, location and screening of open storage areas, parking lots, landscaping, signage and lighting.
	4.9.1(b) All infrastructure required to distribute and service the development should be located underground.	4.9.1(b) Underground services will be required as a condition of subdivision and executed through associated servicing agreements.
		4.9.1(c) Roadway standards will be reviewed and included as

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
		conditions at the time of subdivision approval and executed through servicing agreements.

Discussion

Policy 6.8.4 of the Municipal Development Plan provides direction for the City to work with developers and landowners to develop and implement design guidelines regarding such items as architectural treatment of building, setbacks, landscaping, lighting, screening, storage, parking and signage for the Highway 15 corridor. ~~Section 7.5 of the Municipal Development Plan provides direction for the City to work with developers and landowners to develop and implement design guidelines regarding such items as architectural treatment of building, setbacks, landscaping, lighting, screening, storage, parking and signage for the Highway 15 corridor.~~ This would ensure that the siting, form, and unified architectural treatment of commercial and industrial development improves the visual quality of road(s) that it fronts and considers the environment in which it is located. Special attention will be given to development along Highways 15. Wetlands, stands of trees, and other natural habitat should be preserved where possible. Parking and loading areas should be paved and screened, have landscaped buffers, and provided with signage where appropriate.

The use of aerial (overhead) power servicing is discouraged as it is unattractive and may pose as an impediment to end users within the plan area. Roadway cross-sections within the Highway 15 Vicinity Overlay are to be urban in nature as it coincides with the level of servicing within the Overlay area, and is more aesthetically pleasing; thereby promoting that these lands be used for their highest and best use.

~~The use of aerial (overhead) power servicing and rural roadway cross-sections with ditches are un-attractive and may detract from the overall appearance of the plan area, and may in turn reduce the potential for these lands to be development and used to their highest and best use.~~

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.114.10 RISK MANAGEMENT

Goal: To adopt a Risk Management Framework which prohibits land uses within the **ASP plan** area that might be detrimentally impacted by hazardous land uses located on lands in adjacent industrial areas.

Objective	ASP Policy	Implementation
4.10.1 To restrict industrial uses and industrial activities associated with explosive or hazardous materials within the ASP plan area.	4.10.1 The IH Heavy Industrial district will not be applied to lands within the ASP plan area.	4.10.1 The Planning & Strategic Initiatives Department <u>Development Services</u> will refuse <u>support for</u> applications to rezone lands within the ASP plan area to the IH designation.
4.10.2 To implement risk management practices and procedures for limited highway commercial land uses.	4.10.2 Appropriate safety measures, such as in-place sheltering, shielding, public warning systems, evacuation programs and ventilation shut-off systems may be required for new buildings in the plan area.	4.10.2 A Risk Management Framework shall be developed at the Outline Plan and Design Brief stage by the developer/landowner, in consultation with the Planning & Strategic Initiatives and Community and Protective Service Departments <u>Emergency Services</u> and adjacent heavy industrial landowners.
<u>4.10.3</u> <u>To restrict public institution uses where people are dependant</u> dependent <u>upon others for evacuation such as hospitals, schools, or senior citizen homes.</u>	<u>4.10.3</u> <u>No public institution uses shall be approved with the ASP boundaries.</u>	<u>4.10.3</u> <u>The Development Officer will refuse any applications for any uses that, in the opinion of the development officer</u> Development Officer <u>, is deemed a public institutional use.</u>

Discussion

The Municipal Development Plan supports the continued growth and expansion of Heavy Industrial development in Fort Saskatchewan, and promotes a risk management approach to ensure that appropriate separation distances are defined between Heavy Industrial Development and less intensive industrial development or potentially non-compatible uses. The limited highway commercial land uses will not be able to accommodate day cares, institutional uses, recreation facilities, or uses where overnight accommodation may occur. The Municipal Development Plan provides that new heavy industries, which have or could, have a significant detrimental effect on the safety, use, amenity or enjoyment of adjacent or nearby sites due to appearance, noise, odour,

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

~~emission of contaminants, fire or explosive hazards or dangerous goods shall maintain a minimum reciprocal setback distance of 3.2 kilometres (2.0 miles) from lands designated for day care, residential, institutional or recreational purposes, unless a risk and environmental assessment and review demonstrates to the City's satisfaction that a lesser reciprocal setback is warranted. The inclusion of limited highway commercial land uses will not permit such uses in the plan area.~~

Section 4.3 recognizes the opportunities for limited highway commercial development along Highway 15. Reducing risk has been initiated through the completion of the Industrial Risk Assessment outlined in Section 3.7, whereby 1.5 km and 3.5 km setback distance from heavy industrial to various land uses are recommended. The opportunity for a limited amount of highway commercial uses within the 1.5 km setback distance may be acceptable provided that certain safety measures are implemented in the design and operation of the buildings and that site/development specific risk assessments demonstrate to the City's satisfaction that this lesser setback distance is warranted.

For example, new development may require designs (e.g. structural, mechanical, HVAC, ~~eteetc.~~) that reduce the calculated risk equal to or lower than the 1×10^{-5} risk criteria at the 1.5 km distance. The site/development-specific risk assessment could determine that although the site is at or closer than 1.5 km of industry, it actually functions as if it was located farther out because of the incorporation of additional safety measures.

Historically the City has required developers of new industrial subdivisions to provide public warning systems in accordance with City standards, and will continue to do so of future industrial subdivisions.

~~Section 11.5 – Public Warning Systems of the MDP also requires developers of new industrial subdivisions to provide public warning systems in accordance with City standards.~~

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

4.134.11 IMPLEMENTATION

Goal: To adopt and use the Area Structure Plan as a policy framework for development and to assist with the preparation of more detailed Outline Plans and design briefs for City approval prior to consideration of rezoning and subdivision applications.

Objective	ASP Policy	Implementation
4.11.1 <u>To ensure lands within the ASP boundaries are able to meet local and regional demands for industrial land. To ensure an orderly, economical and logical process of planning and development occurs in the ASP area.</u>	4.11.1(a) <u>Servicing of ASP lands shall be permitted to be flexible. Development which is not serviced to a full urban standard will not be permitted.</u>	4.11.1(a) <u>Reduced levels of service shall be permitted in the interim until such time as the owner of a parcel requires a higher level of service. The Development Officer will refuse applications for development on lands with less than full urban services.</u>
	4.11.1(b) To require more detailed servicing information at the Outline Plan and Design Brief stages.	4.11.1(b) Development and servicing shall proceed in accordance with Outline Plans and Design Briefs prepared for the associated lands.
	4.11.1(c) <u>Provide for a flexible servicing and phasing scheme having regard for the economical and efficient extension of City services and utilities. Provide a servicing and phasing scheme based on the economical and efficient extension of City services and utilities.</u>	4.11.1(c) <u>Development and servicing should occur sequentially, through the extension of storm, sanitary and water systems; flexibility in the sequence of development and servicing shall be accommodated</u> accommodated <u>where warranted by the proponent of a subdivision and supported by the City. Development and servicing shall occur sequentially, through the extension of existing storm, sanitary and water systems.</u>
	4.11.1(d) Require on-site and off-site costs associated with servicing new developments with roadways,	4.11.1(d) Development charges and levies will be in accordance with executed servicing agreements associated with each development.

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
	utilities and other infrastructure to be borne by the development.	
4.11.2 Undertake amendments to the Land Use Bylaw	4.11.2(a) A Land Use Bylaw amendment will recognize the addition of the Limited Highway Commercial Overlay designation within the plan area.	4.11.2(a) Administration shall draft a Limited Highway Commercial Overlay for Council consideration, prior to or concurrently with this plan.
	4.11.2(b) Individual landowners must apply for zoning prior to subdivision and/or development.	4.11.2(b) Administration will review all rezoning applications to ensure conformity with this plan.
	<u>4.11.2(c)</u> <u>The City of Fort Saskatchewan, at its discretion, may pre-zone the lands within the ASP to the IM - Medium Industrial District following approval of an Outline Plan.</u>	<u>4.11.2(c)</u> <u>Administration shall investigate the need to pre-zone the lands within the ASP plan area, and determine if it is in the best interest of the City.</u>
<u>4.11.3</u> <u>Ensure alignment and consistency between all municipal statutory documents.</u>	<u>4.11.3</u> <u>Amendments to The City of Fort Saskatchewan Alberta's Industrial Heartland ASP, Bylaw C19-00 will recognize amendments made to this Pplan.</u>	<u>4.11.3(a)</u> <u>The City shall amend Map 2 of The City of Fort Saskatchewan Alberta's Industrial Heartland ASP to reflect the Land Use Concept as found in this Pplan.</u> <u>4.11.3(a)</u> <u>The City shall amend The City of Fort Saskatchewan Alberta's Industrial Heartland ASP objective and policies to reflect those found in this Pplan.</u>
4.11. 43 To ensure the ASP remains relevant to the planning and	4.11. 43 The Planning & Strategic Initiatives Department Development Services	4.11. 43 The Planning & Strategic Initiatives Department Development Services

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Objective	ASP Policy	Implementation
development issues that may change over time.	will monitor the effectiveness of the ASP in an ongoing manner.	will monitor the ASP and undertake a review, and update if necessary, at 5 year intervals.
4.11.4 Facilitate open discussion with Strathcona County regarding access points from the plan area roads onto County roads.	4.11.4 Inter-municipal cooperation	4.11.4 The City of Fort Saskatchewan will enter into agreement with Strathcona County regarding identified access points from the plan area onto Range Road 220 and TWP RD 550 within one year of adoption of this plan and any access and/or County road improvement matters identified must be resolved to the satisfaction of both parties prior to any new development proposals being accepted within the plan area.

Discussion

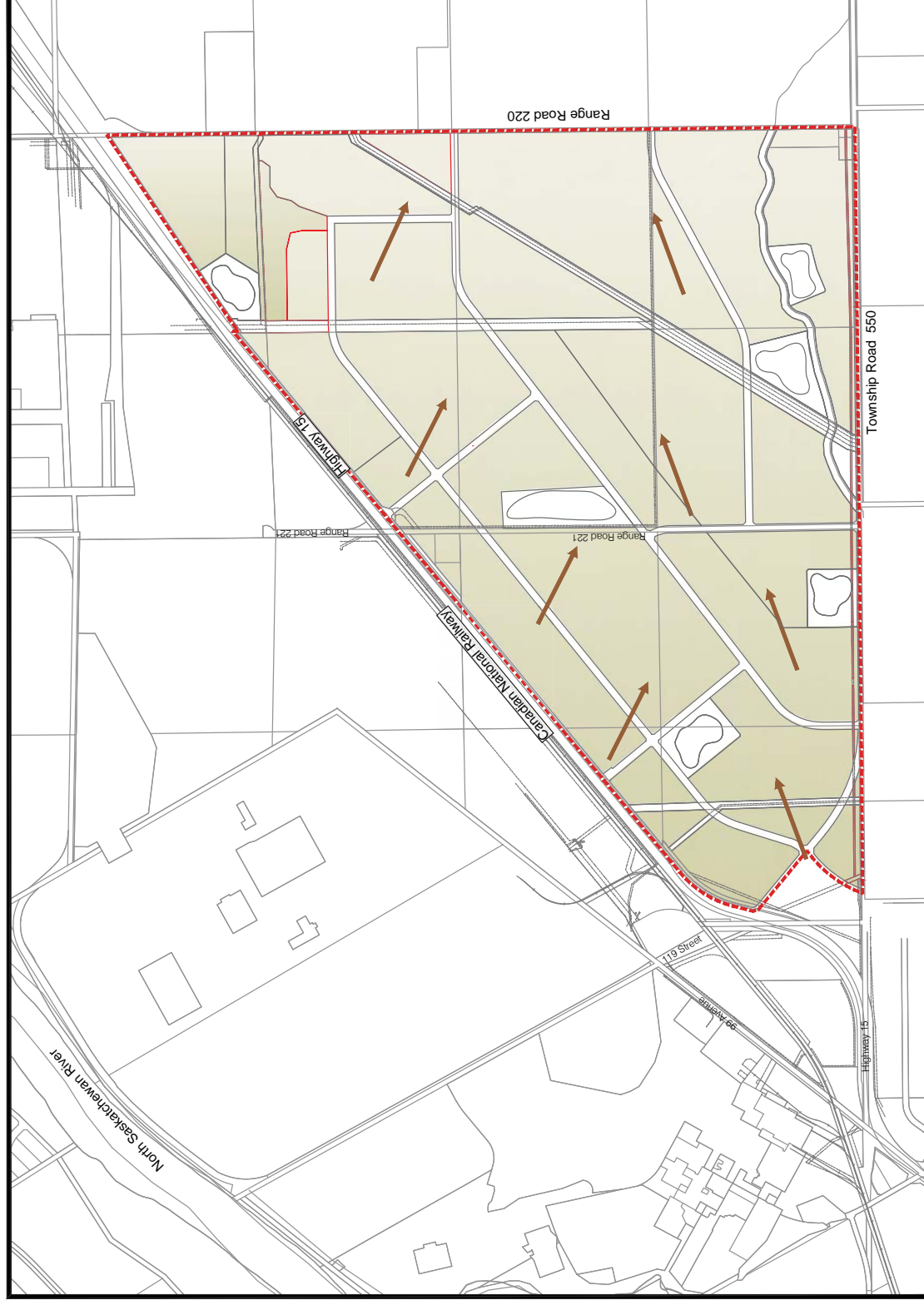
The ASP is meant to serve as a planning framework for development within the plan area, and should be reviewed periodically, updated and amended having regard to changes in economic, social or physical (i.e. man-made) conditions that may arise. Amendments are required to the Land Use Bylaw to ensure consistency with this plan and land uses proposed. Additional technical and supporting studies will be required regarding, but not limited to, transportation and servicing, within the plan area. The phasing of development is proposed based upon the logical extension of municipal infrastructure, transportation links and economics and is conceptually illustrated on **Figure 10.0 – Conceptual Development Phasing**.

Economic conditions, market demand and the logical and cost-effective extension of roadways, municipal services and other infrastructure will determine the phasing of Outline Plans, subdivision and development. If, as a result of proposed Outline Plans, subdivision or development proposals, the need to construct services and other municipal infrastructure in advance of the logical phasing of development is proposed, review will required by the City provided the developer finances the required costs.

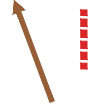
JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

This Page Left Intentionally Blank

Figure 10.0
Conceptual
Development Staging



General Direction of
Development
ASP Boundary



Josephburg Road North Industrial

Area Structure Plan

Bylaw C13-09

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Appendix 1 – Land Ownership

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

	Titled Owner	Legal Description	Area (ha)
1	Fort Business Park Ltd. Private Corporate	Lot 1, Block 1, Plan 082 0100	1.79
2	Private Corporate Fort Industrial Estates Ltd.	SW 1/4 2-55-22-W4M	54.93
3	Private Corporate Triple Five International Developments Ltd.	SE 1/4 2-55-22-W4M	65.195
4	Private Corporate Fort Industrial Estates Ltd.	SW 1/4 1-55-22-W4M	65.894
5	Private Corporate S & D Fort Saskatchewan Industrial Park Ltd	SE 1/4 1-55-22-W4M	40.436
6	Private Corporate IGW Properties GP Inc	Lot 1, Plan 952 2270	23.49
7	Timothy Mark and Denise Schoenleber Private Non-Corporate	SE 1/4 1-55-22-W4M	2.02
8	Private Non-Corporate Frank John and Louise Valdere-Meysey	SE 1/4 12-55-22-W4M	64.7
9	Private Non-Corporate Frank John and Louise Valdere-Meysey	NE 1/4 12-55-22-W4M	28.48
10	Private Corporate Dew Chemical Canada Inc	Pt of Lot 1, Plan 912 2507	0.82*
11	Private Corporate Fort Industrial Estates Ltd.	SW 1/4 12-55-22-W4M	44.7*
12	Private Corporate Fort Industrial Estates Ltd.	NW & NE 1/4 2-55-22-W4M	62.9*
13	Private Corporate Fort Industrial Estates Ltd.	NW 1/4 1-55-22-W4M	64.7
14	Private Non-Corporate Frank John and Louise Valdere-Meysey	NE 1/4 1-55-22-W4M	64.7
15	Private Corporate Altalink Management Ltd.	Power Line ROW OT, Plan 110RS	2.48
16	Private Non-Corporate Christopher W Theroux	SE 1/4 1-55-22-W4M	0.543
17	Private Non-Corporate Stephen John and Darlene Marie Galiwoda	SE 1/4 1-55-22-W4M	0.203
18	Private Corporate Dew Chemical Canada Inc.	Lot A, Plan 590KS	1.005
19	Private Corporate Dew Chemical Canada Inc.	Lot B, Plan 590KS	1.21
20	Private Corporate Dew Chemical Canada Inc.	Lot C, Plan 590KS	0.924
		TOTAL	591

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

** Represents area within the ASP, not as indicated on the certificate of title*

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Appendix 2 – Land Use Statistics

<u>JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN</u>		
	<u>Area (ha.)</u>	<u>% of GDA</u>
<u>Gross Area</u>	<u>590.3</u>	
<u>Existing Roadways and Widening (RR 221, Hwy 15)</u>	<u>9.2</u>	
<u>*Hwy 15 / Rge Rd 220 Interchange Right-of-Way</u>	<u>15.5</u>	
<u>Pipeline & Utility Rights-of-Way</u>	<u>27.3</u>	
<u>Public Utility Lot (Drainage Channel)</u>	<u>5.4</u>	
<u>Gross Developable Area</u>	<u>532.9</u>	<u>100%</u>
<u>Stormwater Management Facilities</u>	<u>32.4</u>	<u>6.1%</u>
<u>Industrial Collector Roadways</u>	<u>34.0</u>	<u>6.4%</u>
<u>**Municipal Reserve</u>		
<u>NE Treed Area</u>	<u>13.6</u>	<u>2.6%</u>
<u>Net Developable Area</u>	<u>452.9</u>	<u>85.0%</u>
(areas below are inclusive of future local industrial roads which will be determined through Outline Plans)		
<u>Medium Industrial</u>	<u>389.9</u>	<u>73.2%</u>
<u>Medium Industrial – Highway 15 Vicinity Overlay</u>	<u>63.0</u>	<u>11.8%</u>
*Area is approximate. Exact area and extent of land required for the interchange to be determined in conjunction with Alberta Transportation, Strathcona County and the City of Fort Saskatchewan prior to approval of an Outline Plan for the adjacent lands.		
** Exact MR area to be confirmed at time of detailed Outline Plan submission. Balance of MR owing to be provided as cash-cash in lieu, unless the City determines through the Outline Plan stages that smaller park areas are required.-		

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN		
	Area (ha.)	% of GDA
Gross Area	590.3	
Existing Roadways and Widening (RR 221, Hwy 15)	9.2	
*Hwy 15 / Rge Rd 220 Interchange Right of Way	15.5	
Pipeline & Utility Rights-of-Way	27.3	
Public Utility Lot (Drainage Channel)	5.4	
Gross Developable Area	532.9	100.0%
Stormwater Management Facilities	32.4	6.1%
Industrial Collector Roadways	34.0	6.4%
**Municipal Reserve NE Treed Area	13.6	2.6%
Net Developable Area	452.9	85.0%
(areas below are inclusive of future local industrial roads which will be determined through Outline Plans)		
Medium Industrial	233.0	43.7%
Medium Industrial - Highway 15 Vicinity Overlay	53.0	9.9%
Light Industrial	167.0	31.3%
*Area is approximate. Exact area and extent of land required for the interchange to be determined in conjunction with Alberta Transportation, Strathcona County and the City of Fort Saskatchewan prior to approval of an Outline Plan for the adjacent lands.		
**Exact MR area to be confirmed at time of detailed Outline Plan submission. Balance of MR owing to be provided as cash in lieu, unless the City determines through the Outline Plan stages that smaller park areas are required.		

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

Appendix 3 – References

The following studies were reference in support of the Josephburg Road North Industrial ASP:

- 1. City of Fort Saskatchewan Municipal Development Plan, 1999-2010, Bylaw C12-99 (Rescinded)**
- 2. City of Fort Saskatchewan Municipal Development Plan, 2010-2030, Bylaw C16-10**
- 3. City of Fort Saskatchewan Land Use Bylaw, Bylaw C6-08 (Rescinded)**
- 4. City of Fort Saskatchewan Land Use Bylaw, Bylaw C10-13**
- 3-5. The City of Fort Saskatchewan Alberta's Industrial Heartland ASP, Bylaw C19-00**
- 4-6. Strathcona Area Industrial Heartland Transportation Study Update – Final Report**
 - a. Prepared by Stantec Consulting Ltd, November 2007, for Strathcona County.
- 5-7. City of Fort Saskatchewan Conceptual Servicing Study Final Report**
 - a. Prepared by Stantec Consulting Ltd., October 2006
- 6-8. Ecological Assessment, Josephburg Road North Industrial ASP, Section 1-55-22-4 and portions of Sections 2-55-22-4 and 12-55-22-4, Fort Saskatchewan**
 - o Prepared by Stantec Consulting Ltd, August 15, 2008
- 7-9. Industrial Risk Assessment, Final Report**
 - o Prepared by Doug McCutcheon and Associates Consulting, March 2009
- 10. Josephburg ASP Amendment, Risk Review**
 - o Prepared by Doug McCutcheon and Associates Consulting, June 2009
- 8-11. Alsten Lands Outline Plan**
 - o Prepared by Durrance Projects Ltd; Trans America Group; Ecomark Ltd; Associated Engineering Ltd; & BK Hydrology Services, July 2007
- 9-12. City of Fort Saskatchewan Strategic Plan: 2020 Vision – Clarity for the Future**

JOSEPHBURG ROAD NORTH INDUSTRIAL AREA STRUCTURE PLAN

~~10.13.~~ **Strathcona County Land Use Bylaw, Bylaw 8-2001**

~~11.14.~~ **Heartland Transmission Project;**

- a. www.heartlandtransmissionarea.ca

~~12.15.~~ **Capital Region Integrated Growth Management Plan – Final Report on Core Infrastructure**

- Prepared by ISL Engineering and Land Services, November 2007

~~13.16.~~ **Josephburg Road North Industrial ASP – Transportation Impact Assessment**

- Prepared by Stantec Consulting Ltd., December 2009

17. Growing Forward – The Capital Region Growth Plan

- [Capital Region Board, March 2009](#)

18. The Capital Region Growth Plan Addendum

- [Capital Region Board, October 2009 & December 2009](#)

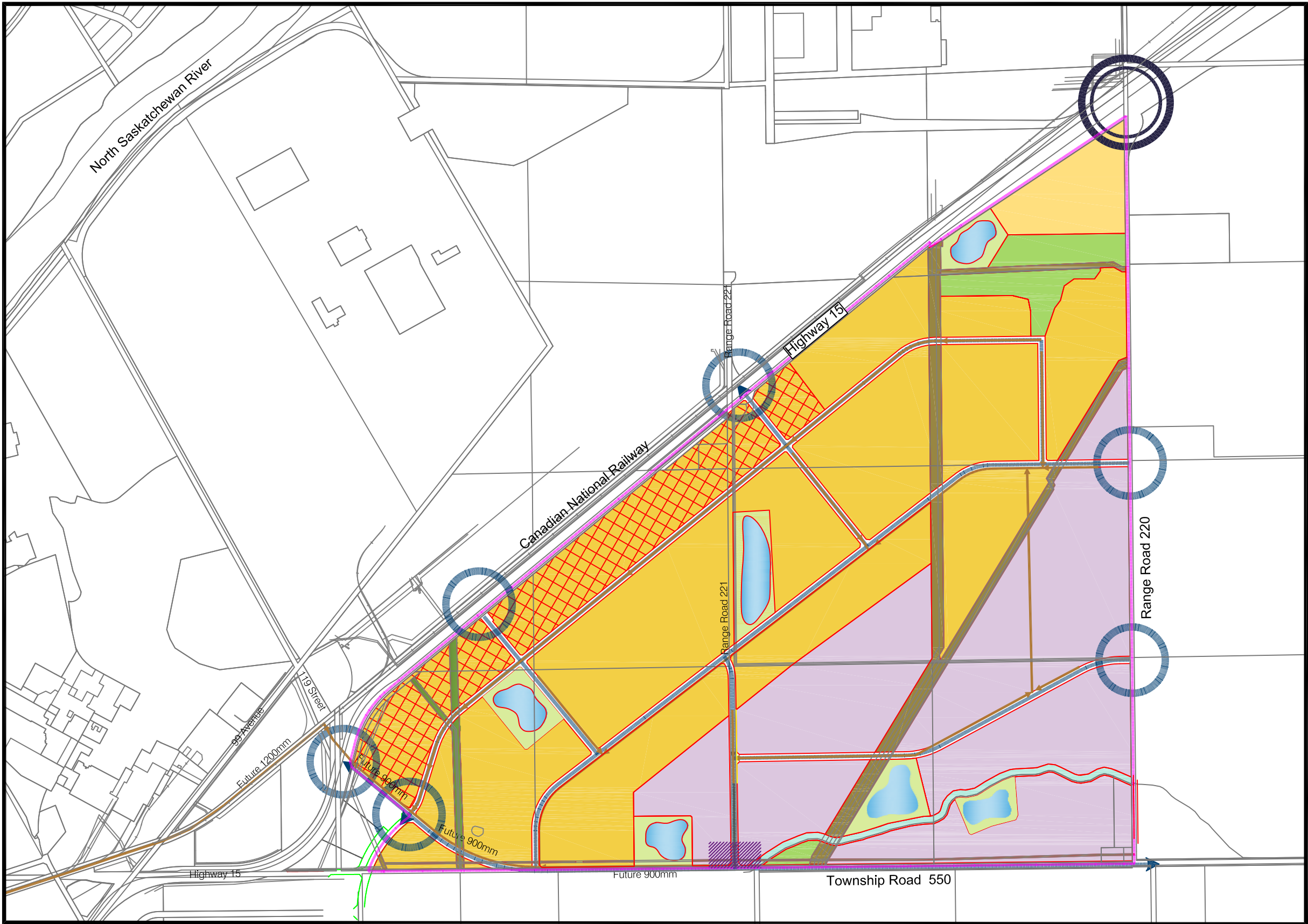
19. Integrated Regional Transportation Master Plan

- [Capital Region Board, September 2011](#)

20. Josephburg Road North Industrial Water Modelling Study - Update

- [Prepared by Associated Engineering, October 2014](#)

Figure 5.0
Land Use Concept



- Medium Industrial
- Medium Industrial - Highway 15 Vicinity Overlay
- Light Industrial
- Potential Service Centre Node
- Municipal Reserve
- Future Road Right-of-Way
- Pipeline/ Utility Corridor
- Public Utility Lot (Existing Drainage Channel)
- Stormwater Management Facilities
- Proposed All-Directional Access
- Potential Interchange
- Roadways
- ASP Boundary



Josephburg Road North Industrial
Area Structure Plan
Bylaw C13-09

Figure 5.0
Land Use Concept



SCALE 1: 20000



Josephburg Road North Industrial Area Structure Plan